



PARKING ADVISORY COMMITTEE AGENDA

Wednesday, June 21, 2023 - 6:00 PM

City Hall, Council Chambers, 169 SW Coast Hwy, Newport, OR 97365

All public meetings of the City of Newport will be held in the City Council Chambers of the Newport City Hall, 169 SW Coast Highway, Newport. The meeting location is accessible to persons with disabilities. A request for an interpreter, or for other accommodations, should be made at least 48 hours in advance of the meeting to Erik Glover, City Recorder at 541.574.0613, or e.glover@newportoregon.gov.

All meetings are live-streamed at <https://newportoregon.gov>, and broadcast on Charter Channel 190. Anyone wishing to provide written public comment should send the comment to publiccomment@newportoregon.gov. Public comment must be received four hours prior to a scheduled meeting. For example, if a meeting is to be held at 3:00 P.M., the deadline to submit written comment is 11:00 A.M. If a meeting is scheduled to occur before noon, the written comment must be submitted by 5:00 P.M. the previous day. To provide virtual public comment during a city meeting, a request must be made to the meeting staff at least 24 hours prior to the start of the meeting. This provision applies only to public comment and presenters outside the area and/or unable to physically attend an in person meeting.

The agenda may be amended during the meeting to add or delete items, change the order of agenda items, or discuss any other business deemed necessary at the time of the meeting.

1. WELCOME AND INTRODUCTIONS

1.1 Memorandum.

[Staff Memorandum](#)

2. ROLL CALL

3. APPROVAL OF MINUTES

- 3.1 **May 17, 2023 Parking Advisory Committee Meeting.**
[Draft Parking Advisory Comm Mtg Minutes 05-17-2023](#)

4. DISCUSSION ITEMS

- 4.1 **Review Bayfront Parking Lot Refurbishment and Sign Installation Bid Packages.**
- 4.2 **Review Draft Meter Permit Municipal Code Changes.**
- 4.3 **Confirm Updates to Regulatory Signs.**
- 4.4 **Confirm Permit Availability Caps by Zone.**
- 4.5 **Updated Implementation Schedule.**

5. PUBLIC COMMENT

This is an opportunity for members of the audience to bring to the Work Group's attention any item not listed on the agenda. Comments will be limited to three (3) minutes per person with a maximum of 15 minutes for all items. Speakers may not yield their time to others.

6. ADJOURNMENT


HANDOUTS

Meeting Materials:

- [Unsigned Drawings for Bayfront Parking Lot Refurbishments](#)
- [Unsigned Drawings Bayfront Parking Sign Pole Installations](#)
- [Meter- Permit Code Changes](#)
- [Updated Regulatory Sign Concepts](#)

Bayfront Parking Map with Permit Availability by Zone
Implementation Schedule

Memorandum

To: Parking Advisory Committee
From: Derrick I. Tokos, AICP, Community Development Director 
Date: June 16, 2023
Re: Topics for June 21st Parking Advisory Committee Meeting

For this meeting we would like to review with the Committee the bid packages for the Bayfront parking lot refurbishments and the installation and/or replacement of sign poles. Chris Beatty, P.E. with the Public Works Department will be in attendance to discuss the details. Advertisements have been posted for both projects, with bids due July 10, 2023. This will position the projects such that the work can be initiated after the Labor Day Weekend and before an October launch date for the metering/permit program. The bid packages are a close to final set of drawings; however, changes can still be made to them if needed.

The next item on the agenda is a review of the code changes the City will need to make in order for the meter/permit program to be operable. At your last meeting, we discussed potential changes to the City's off-street parking requirements. This discussion will focus more on the non-land use changes that relate to the operation and enforcement of a meter and electronic permit program.

Our Public Works Department updated the regulatory sign concepts following your April discussion and we are looking for confirmation from the group that the format is acceptable so that the new signs can be ordered. A one page illustrated summary of the regulatory sign concepts is included in the packet. Following that discussion, I have updated the Bayfront e-permitting zone maps to include the number of permits that will be available by zone. The limit for Tier 1 permits is 225, which represents 100% of the available stalls. The Tier 2 permit cap would be set at 400 permits, which represents 120% of the available spaces. Once the Committee confirms that this is an acceptable approach, then we can provide this information to T2 Systems, Inc. to include in the Mobile Pay setup.

Lastly, attached for reference is the implementation schedule. City staff is working with T2 Systems, Inc. on parking system setup and training, which will extend through the summer. One area that we haven't discussed is collections. Our Police Department is initiating the process with Oregon DMV to allow T2 Systems, Inc. to serve as the City's agent to pursue payment of stale parking tickets. If this works out, then the City would look to amend its contract with T2 Systems to include the service. Costs for the service would be paid for with a surcharge to the parking ticket fee (i.e. a delinquent payment charge). This is a topic that we will be discussing with the City Council at its June 20th work session.

Attachments

Unsigned Drawings for Bayfront Parking Lot Refurbishments
Unsigned Drawings for Bayfront Sign Pole Installations
Meter – Permit Code Changes
Updated Regulatory Sign Concepts
Bayfront Parking Map with Permit Availability by Zone
Implementation Schedule

Draft MINUTES
Parking Advisory Committee
Meeting #11
Newport City Hall Council Chambers
May 17, 2023

Committee Members Present: Janell Goplen (*by video*), Bill Branigan (*by video*), Aracelly Guevara (*by video*), Aaron Bretz, Doretta Smith, Jan Kaplan, and Robert Emond.

Committee Members Absent: Gary Ripka.

City Staff Present: Community Development Director, Derrick Tokos; and Executive Assistant, Sherri Marineau.

1. **Call to Order & Roll Call.** Meeting started at 6:05 p.m.
2. **Approval of Minutes.** Bill Branigan reported minor corrections to the minutes.

MOTION was made by Aaron Bretz, seconded by Doretta Smith, to approve the April 19, 2023 Parking Advisory Committee meeting minutes with minor corrections. The motion carried unanimously in a voice vote.

3. **E-Permit Pricing and Availability.** Tokos reviewed the staff memorandum and noted there would be a kickoff meeting happening with T2 Systems and city staff that Friday. He reviewed the ePermitting pricing and talked through some of the options with the group. Tokos covered the Bayfront parking permit zone options for Zones A through D and the number of parking spaces available in each. He noted interest on the permits would be different on the east and west sides of the Bayfront. He thought that Zone A would sell more permits than the number of spaces that were available. It was a common practice for more permits to be sold than spaces were available, because they never assumed 100 percent utilization at any given time. Tokos suggested they over subscribe the number of Tier 2 timed permits and thought 120 percent would be appropriate. If they took the approach of 100 percent of spaces in Tier 1 areas (Zones A and B) then 120 percent of available parking stalls for Tier 2 (Zones C and D) there would be around 630 permits available.

Emond asked if 120 percent overflow would be enough and questioned if it should be more than 120. Tokos noted that the 120-140 range was typically what they would see. Smith asked what other cities did. Tokos reported they were all over the place on this. He thought that Newport would want to make sure in the metered permit zones that there as a healthy number of spaces available for people who were arriving and wanted pay at the meters. This way they wouldn't get too high on the numbers. Tokos thought they could be comfortable changing it to 140 percent in the Tier 2 areas. Emond pointed out they could always changes this the next year. Bretz noted the people that purchased the permits would be revolving. The people who wanted them in the summer would be different from those that wanted them in the winter. Bretz thought that because the permit was for one month, it would be sufficient to set it at 120 percent on Tier 2. Emond asked if that included both the 72 hour permits and the 12 hour permits. Tokos noted the 72 hour permits would be done by Port invite only in either Zones B or D. He noted this was part of the thinking to break it up by west and east ends. This wasn't an issue on the west end at all. Tokos noted it would be 100 percent on the Tier 1 pricing zones, which were Zones A and B, and 120 percent on the Tier 2 zones, which would be Zones C and D. The commercial fishing would be by email invitation, which was the 72 hour period and limited to zones B and D, which would be the east end zones. Smith thought that a commercial fishermen who paid \$45 a month for a permit wouldn't be happy if permits were oversold and they had no parking spots.

Bretz thought it was hard to gauge this because the Port's permits were so inexpensive. Tokos reminded that the people who would be engaged in commercial fishing permits would also have the Port parking available for them. Bretz reiterated that it was hard to tell how many permits they would need. They had somewhere around 260 Port property permits. It was hard to compare both because they were a different price and product. Tokos reminded that all of the permits had no guarantee of parking spaces.

Goplen asked how often they could change the rules once things were set. Tokos thought they would have flexibility and if they saw something wasn't working they could make changes. There would be reports to the Committee about permit uptake. Goplen thought the Tier 1 pricing was too low. Emond suggested picking a number and sending out invites. Then, when they saw the response they would know how many permits to provide. Guevara thought they should start low and increase the numbers later. Smith thought they were going to have PR issues when implementing the system, and didn't want to start upsetting people right off the bat. She thought they should start off low. Tokos suggested they start with 100 percent in the Tier 1 and 120 percent in Tier 2, then give it three to six months to see what happened.

Goplen asked if this would be implemented in October, when would the meters go in. Tokos reported the installs would be done in late September or early October. This would be an advantage because they wouldn't be implemented in the busy season. Tokos noted the article Goplen shared about what Newport, Rhode Island did was something they should look to do for the next summer season.

Branigan asked if businesses should be allowed to buy a bunch of the permits for their employees. He also asked if they would allow employees to have first crack at getting permits before opening them up to the general public. Tokos noted they didn't have any limitations on who got the permits, so if an employer wanted to cover the cost for their employee's permits they could do that if the permits were available. He thought they should do one launch date instead of multiple dates. The commercial fishing permits would be done by an invite only and they would coordinate this with the Port. Smith questioned who else, besides an employee would want a monthly permit. Branigan thought employees would want the permits the most, but there would be others who came to Newport multiple times that would want them. Goplen thought an Uber or an Airbnb might want them as well.

Emond asked how the daily lodging permits and charter fishing permits would work. Tokos said they would be separate from what they were talking about here. The charters and hotels would have a number of permits that they would hand out. Emond thought it would be nice to be able to track this. Tokos reminded they would have the data from the system that would show how many were handed out.

Tokos asked if the Committee was generally okay with the zone break out. Goplen thought Tier 1 should be more expensive. Bretz wanted the commercial fishing permits to be 96 hours instead of 72 hours. Emond was concerned that people who worked on the Bayfront would be upset when they saw the fishermen had more time to park. Tokos didn't have a problem with setting the commercial fishing permit to 96 hours if that was functionally what they needed. He didn't think a retail person should pay the same as them and asked if \$65 was better for extended stay. Bretz thought if they were going to do that they should leave it at 72 hours. Then if they got blowback on the 72 hours they could say that we can add more time, but it would be more expensive. Emond thought it should be \$55 for fishermen so it was a nominal price difference. Kaplan agreed but noted they didn't want to make this too complicated. He thought it got confusing when there were too many different options. Smith didn't think it would be because the commercial fishermen would be concentrating on their own price. Tokos noted he was inclined to go with \$65 for 96 hours. Bretz reminded that fishermen still had the option to go with the Port parking if they were going to be out for a fourth day. He thought \$65 might be at

the point that a lot of the fishermen wouldn't purchase them. Goplen reminded they could always change this, but they wouldn't want to post a lower price upfront and then say price was being bumped up. Bretz thought if they were going to be bumping it up with a new product it would be okay. Smith was concerned that retail staff would be paying \$45 for 12 hours a day then fishermen were paying \$45 a month for 72 hours. Tokos reminded that on the flip side of this, it met both needs because why would someone who worked retail need more than 12 hours. Tokos said what he was hearing was the Committee thought this was a reasonable framework for this currently, and then look to adjust the fees based on the feedback they received.

Tokos reported that they held the meter cost at a \$1 an hour. Some jurisdictions were bumping this up and he asked if \$1.25 an hour made more sense. Emond and Goplen thought \$1 was too cheap. Bretz reminded this was saying the cost would be \$1 an hour and he didn't think it would price anyone out. Smith questioned if locals would say it was unfair to charge them and mean that cause them to choose not to shop local. Goplen reminded they had discussed allowing businesses to do parking validation for customers. Tokos confirmed they would have the coupon codes for this. Bretz noted the current people parking weren't paying for anyone to maintain the parking areas. He thought it was reasonable to have local people pay for a couple of hours which would go into maintenance and add to turnover.

- 4. Updating Special Parking Area Requirements for the Bayfront.** Tokos reviewed the updates to NMC 14.14.100 for the special area parking requirements. He noted that when the City Council adopted the Parking Study into the Comprehensive Plan, they agreed that when metering was implemented there would be a reduction or elimination of off-street parking requirements. The draft of these revisions would achieve this and would become effective at the same time that metering was live.

Tokos reviewed three options of revisions, starting with Option B.1 that would eliminate off-street parking requirements. This would only apply to the Bayfront because it would be the only area where the city required payment for parking. Smith asked if the purpose of B.1 would just be for new development. Tokos said it applied to both new development and redevelopment. Kaplan noted Nye Beach was mostly residential and asked if this would apply to Nye Beach once the metering was implemented there. Tokos said it would, and pointed out B.1 would be the easiest of the option to implement because it was straightforward and there were no off street parking requirements.

Tokos covered Option B.2. that would allow developers to pay a onetime fee in lieu of providing the off street parking required in special areas where payment was required for the use of public parking. Goplen asked if they could build housing on the Bayfront. Tokos explained they could put housing on anything other than street grade, but they wouldn't expect to realistically see anything meaningful come in because of terrain constraints. Option B.2 gave a fee in lieu which gave progressively expensive options for the additional parking demand they placed. Tokos reviewed the examples of how much it would cost for eliminating parking space requirements. Goplen pointed out the retail use example that was listed on the document wasn't currently being used for retail. Tokos said this would have a credit for the previous use. The parking code said that the city was to give credit for the old use, but it didn't say for how long. They may have to put a fixed timeframe on the credits with this policy change, and he guessed that policy makers would want that number to be that same as System Development Charges, which was 10 years. Goplen asked if they had the number of spaces currently necessary for all the businesses in Newport. Tokos could get it but thought it was around 60-90. Goplen thought if they removed this and let people build, it would change how people viewed the meter conversation and all the work the Committee was doing. Tokos noted there was an equity issue when they had some businesses providing zero parking spaces and others providing some. It would become a question on why someone would be obligated to dedicate what was a pretty expensive chunk of real estate for off street parking, when others are doing nothing. Tokos thought a fairness piece would be

to eliminate the parking requirement, and then not allow major development and keep development to a smaller scale.

Emond asked if it was possible that this ordinance wouldn't apply to all three districts because he thought each district had radically different needs. Tokos thought that was fair but expected they would have further adjustments once they had solutions set for Nye Beach. Since they didn't know what the solutions would be for Nye Beach, there was no reason to sort out what the special parking area would be for it. Kaplan reminded that the special district areas already had different requirements. Tokos agreed and noted that the existing requirements would stay in place for 90 days and codified after the last changes were adopted. Emond asked if these code changes would be drafted and adopted by the City Council after public input. Tokos confirmed they would. Emond thought this made it harder to make changes. He noted this was why he questioned whether or not they wanted to put down rules that would be harder to change in the future. Tokos said this was something they would expect when dealing with when rolling out a new demand management program for areas like this. He reminded there would be residential permits in Nye Beach that weren't in the Bayfront, which would have to be dealt with it at that time.

Tokos reviewed Option B.3 that would give a hard wire a limit on the demand a project could place on the limited supply of available public parking before off-street parking must be provided. He covered the examples of what this would look like. Goplen asked if the five examples were current projects that were waiting on the changes to go through. Tokos said they didn't have any specific projects that were waiting. Emond thought this option made more sense for retail and restaurants, but made less sense for fish plants or industrial use. He asked if they could limit this to retail and restaurants, and have B.2 apply to industrial. Tokos thought this might create an equity issue. He thought they could go with B.3 and scale it with fees. They could also hybridize it with B.2 and B.3. Emond thought because retail and restaurants had a higher turnover, this would be fine for them. He had a problem with uses that required their employees to be there all the time, who created congestion. Edmond thought this was contrary to what they set the goal for turnover at.

Tokos asked for the Committee's thoughts on the three options. Smith asked which option Tokos leaned toward. Tokos favored B.2, or a combo of B.2 and B.3. He also liked the simplicity of B.1 but was concerned it would lead to some unintended consequences. Emond liked basing it on the number of parking spots and the building use type because different uses had different customers. Tokos thought they could look at the ratios which would be citywide. The ratios were development based and typically done by the types of uses. Tokos asked if there were any other options that the Committee had in mind to loosen up the rules. Emond thought they should do B.3 at a smaller number and B.2 as an overage. They should set things at a number of spaces, then if demand was over that they could then charge a fee for additional spaces. Tokos asked if what he was saying was to pair B.2 and B.3 and then do something smaller than 25 spaces. Emond thought that was fine, but thought 25 would work on the Bayfront but wouldn't work in other areas. Goplen needed some time to think about it. Tokos asked the Committee to send him a note on their thoughts.

Kaplan asked if B.3 was saying a business was not required to put in parking. Tokos explained was for redevelopment in a manner that was more intense than what the use was currently, or for new development. Tokos gave examples of the properties on the Bayfront that might develop with these changes. Bretz pointed out that it wasn't economical to put parking spaces over the water. If someone was looking to put in a processing plant, they would want a parking lot across the street. Bretz asked if having parking across the street would be allowed. Tokos confirmed it could be allowed as satellite parking. Bretz liked a mixture of B.2 and B.3. Before they establish the fees they look at what it was that they were looking to be incentivize and discourage. They needed to determine if they wanted the added fees to be difficult to be absorbed by the developer or not. Then they could answer what the city

would want to do with the funds. Tokos thought they would look for it to go into the parking fund to supplement the meter revenues so they had money to make a go at constructing a parking structure at some point.

Tokos reviewed the addition of Section C which would make it clear that the few businesses currently providing off-street parking in a meter or meter/permit area would no longer be bound to do so, meaning they could develop these properties. Kaplan reminded that they were trying to manage parking demand. If they decreased the supply it would be contrary to what they were trying to do. If they didn't have a way to obligate additional parking, they would be adding more demand without resources. Emond thought this went back to how B.2 and B.3 worked best in Nye Beach, and B.2 worked only on the Bayfront. Tokos stated he wouldn't expect a gold rush of people eliminating their off street parking that was privately held. In most cases, they were there for a reason. Tokos noted how other jurisdictions who had eliminated off street parking requirements was working for them because they wouldn't generally have businesses coming in that were going to be a massive traffic generator. Smith asked if they adopted B.2 would there be any forgiveness for housing on the Bayfront. Tokos said the direction the state was going was to have a full blanket prohibition on requiring off street parking for residential development because of the housing crisis. Smith thought this would make residents have to use street parking and would set up a whole new dilemma. Tokos thought that this got to the City Center area, where he didn't see a lot of residential opportunity because they couldn't add additional stories to the buildings given their conditions. He also didn't see any real estate available for multifamily projects. Tokos reminded that the residential demand for parking was different than commercial use. Kaplan was in favor of more housing than more commercial. Emond noted this was why he like B.3 because it didn't eliminate all of the parking requirements, just a few. Tokos asked for feedback on the examples from the Committee. Emond thought the examples were good. Bretz thought that for over the water they should look at how far away the parking should be. Tokos explained that not all of the Bayfront was eligible for housing. The areas that were water dependent were not allowed to have houses in the water.

5. **Timing and Location for Outreach with Affected Stakeholders.** Tokos reviewed the implementation schedule, and the list of public engagement groups they would like to engage. Bretz thought that having commercial fishing group meeting in August would work best with the help of the Port. He thought it would work better for businesses if they were directly contacted them directly to get them involved in the engagement instead of sending a blanketed notice.

Tokos reported they would be working on the license plate recognition to get it linked up and the officers trained. He reviewed the parking lot improvements timeline; the sign pole installation; the regulatory sign install timelines; and the pay station install timeline. Tokos noted this schedule was subject to change.

Tokos asked if there was anything the Committee wanted him to bring forward to the next meeting. Smith wanted to see information about the general public outreach that would be done. Goplen thought the city could be more proactive how the information came out. Smith reminded that everyone on the Committee had connections to different groups who they could present to. Emond asked if they were set on the pricing. Tokos thought they figured out pretty much where people wanted to start with.

6. **Public Comment.** None were heard.
7. **Adjournment.** Having no further business, the meeting adjourned at 7:53 p.m.

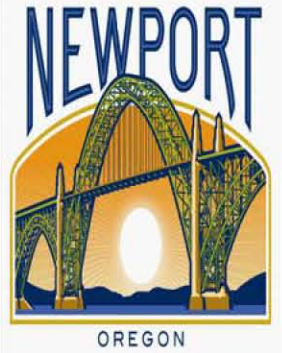
Respectfully submitted,

Sherri Marineau
Executive Assistant

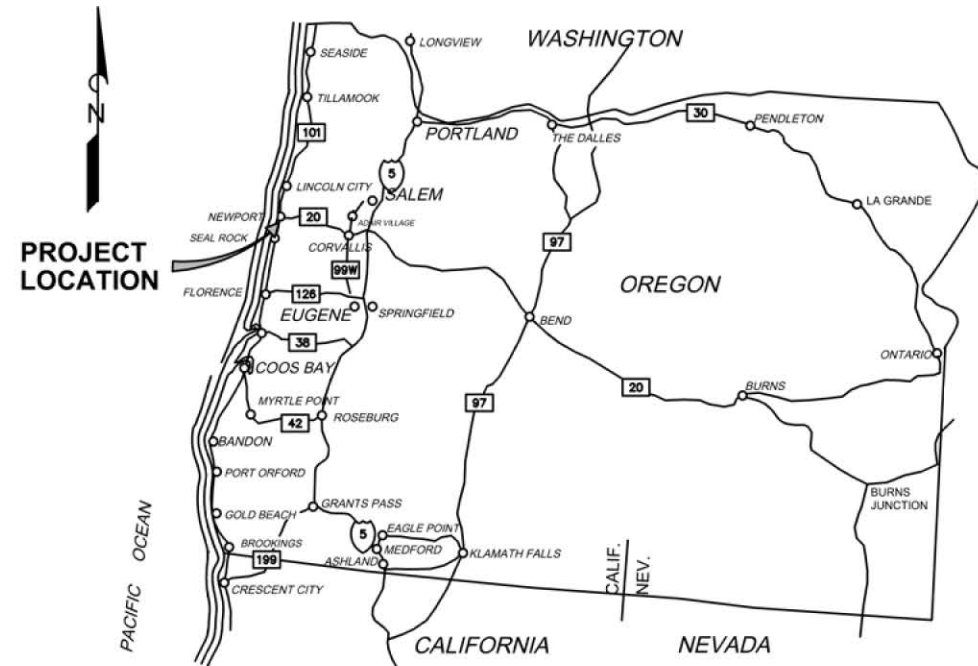
CITY OF NEWPORT, OREGON

BAYFRONT PARKING LOT REFURBISHMENT

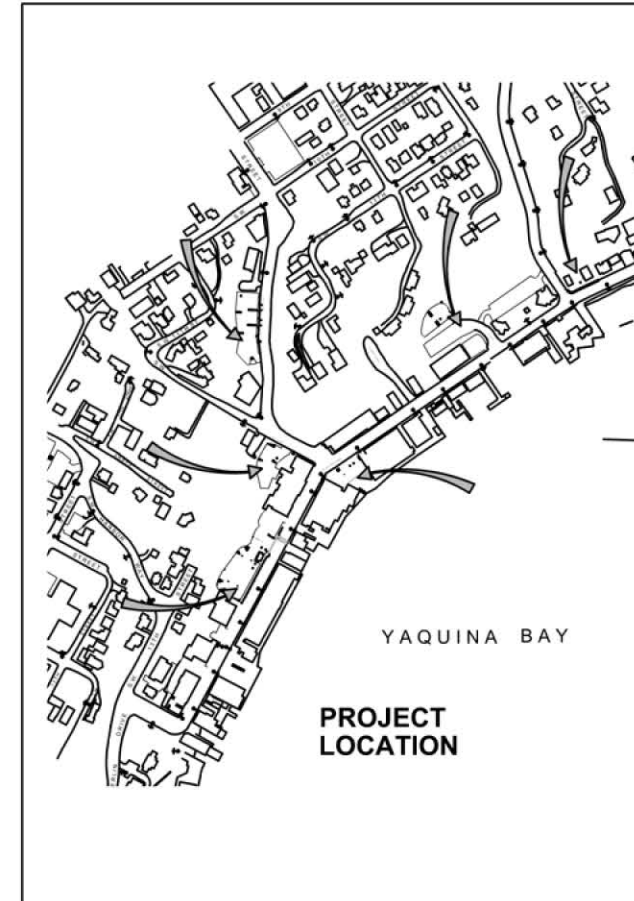
PROJECT NO. 2021-045



169 SW Coast Hwy
Newport, Oregon 97365
541-574-3376
www.newportoregon.gov



LOCATION MAP



VICINITY MAP
NOT TO SCALE



EXPIRES: 12/31/23

REV.	DATE	DESCRIPTION	BY

Designed By: CAB	Drawn By: MUR	Checked By: CAB
Project No: 2021-045		

CITY OF NEWPORT, OREGON
BAY BOULEVARD, NEWPORT, OR 97365

BAYFRONT PARKING LOT REFURBISHMENT

COVER SHEET

Date: Sheet No: **G1**
JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg

GENERAL NOTES

1. ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH 952-001-0090. YOU MAY OBTAIN A COPY OF THE RULES BY CALLING THE CENTER.

NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION ADMINISTRATION CENTER: (503) 232-1987.

STAT. AUTH.: ORS 757.542 THROUGH ORS 757.562 AND ORS 757.993.
2. THE CONTRACTOR SHALL CONTACT 'ONE CALL' FOR UTILITY LOCATES PRIOR TO EXCAVATION. (1-800-332-2344)
3. PROPERTY AND RIGHT OF WAY LINES SHOWN IN THIS PLAN SET ARE APPROXIMATE AND BASED ON BEST AVAILABLE INFORMATION. CONTRACTOR SHALL OBTAIN TEMPORARY CONSTRUCTION ACCESS OR PERMISSION FROM PRIVATE LAND OWNERS PRIOR TO ENTERING PRIVATE PROPERTY.
4. ALL WORK SHALL COMPLY WITH THE OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), LATEST EDITION.
5. SURVEY FOR STRIPING LAYOUT IS THE RESPONSIBILITY OF CONTRACTOR. OWNER WILL ASSIST CONTRACTOR WITH LAYOUT OF LANDSCAPE ISLANDS.
6. TEMPORARY DRAINAGE FACILITIES INCLUDING, BUT NOT LIMITED TO, FLOW DIVERSION, IS THE RESPONSIBILITY OF THE CONTRACTOR WHERE WEEP AREAS FROM ADJACENT HILLSIDES ARE SHOWN ON THE PLANS.
7. ALL TESTS MUST BE COMPLETED WITH CITY REPRESENTATIVE PRESENT.


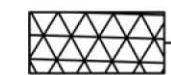
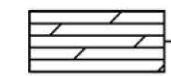
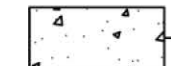
SHEET INDEX

SHEET #	SHEET NAME	SHEET #	SHEET NAME
GENERAL			
G1	- COVER	GENERAL	
G2	- GENERAL NOTES, ABBREVIATIONS, LEGEND, & SHEET INDEX	LEE ST PARKING LOT - EXHIBIT E	
G3	- PROJECT OVERVIEW	C13	- STRIPING & SITE PLAN
ABBEY STREET PARKING LOT - EXHIBIT A			
C1	- STRIPING & SITE PLAN	C14	- DEMOLITION & SURFACING PLAN
C2	- STRIPING & SITE PLAN	C15	- DEMOLITION & SURFACING PLAN
C3	- PART 1 DEMOLITION AND SURFACING PLAN	HATFIELD PARKING LOT - EXHIBIT F	
C4	- PART 2 DEMOLITION AND SURFACING PLAN	C16	- STRIPING & SITE PLAN
FALL STREET PARKING LOT - EXHIBIT B			
C5	- STRIPING & SITE PLAN	C17	- DEMOLITION & SURFACING PLAN
C6	- DEMOLITION & SURFACING PLAN	DETAIL	
C7	- DEMOLITION & SURFACING PLAN	D1	- ROAD DETAILS
BAY BLVD PARKING LOT - EXHIBIT C			
C8	- STRIPING & SITE PLAN	D2	- SIDEWALK & CURB DETAILS
C9	- DEMOLITION & SURFACING PLAN	D2A	- CONCRETE ADA PANEL DETAIL
CANYON WAY PARKING LOT - EXHIBIT D			
C10	- STRIPING & SITE PLAN	D3	- SIGN & CROSSWALK DETAILS
C11	- DEMOLITION & SURFACING PLAN	D4	- LANDSCAPE & PARKING STOP DETAILS
C12	- DEMOLITION & SURFACING PLAN	D5	- STRIPING DETAILS
		D6	- STRIPING DETAILS
		D7	- EROSION CONTROL DETAILS
		D8	- EROSION CONTROL DETAILS
		D9	- BARRICADE DETAIL

GENERAL ABBREVIATIONS

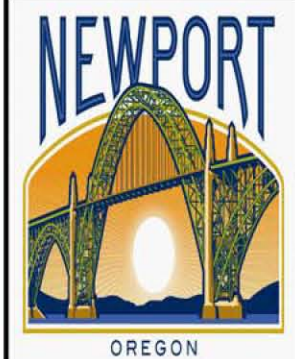
AC	PAVEMENT
BOW	BACK OF WALK
CB	CATCH BASIN
D	DRAIN
EOP	EDGE OF PAVEMENT
EX	EXISTING
MH	MANHOLE
ROW	RIGHT OF WAY
SD	STORM DRAIN
SW	SIDEWALK

HATCH LEGEND

	SLURRY SEAL
	DIG OUT
	MILL AND OVERLAY
	NEW CONCRETE



EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

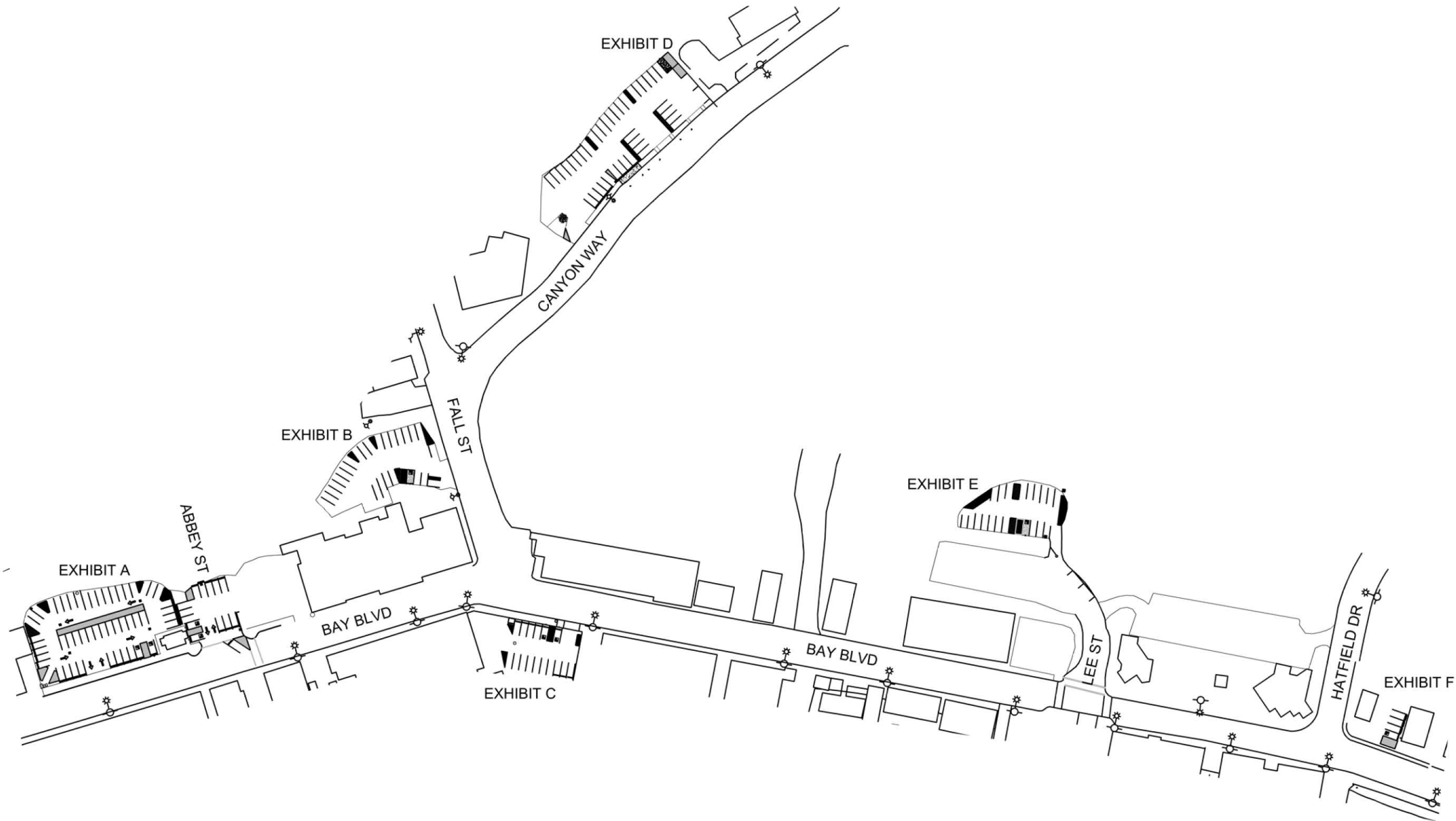
Designed By: CAB
Drawn By: MUR
Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
GENERAL NOTES, ABBREVIATIONS,
LEGEND, & SHEET INDEX

G2

JUNE 2023

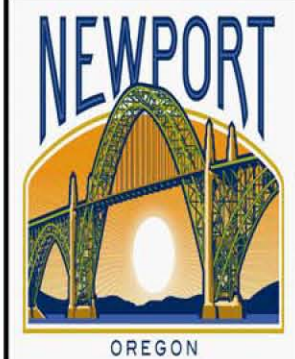
DATE: 6/15/23 FILE: Y:\pki\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg



NTS



EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

Designed By: CAB	Drawn By: MUR	Checked By: CAB
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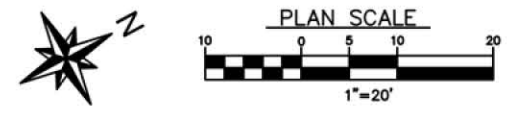
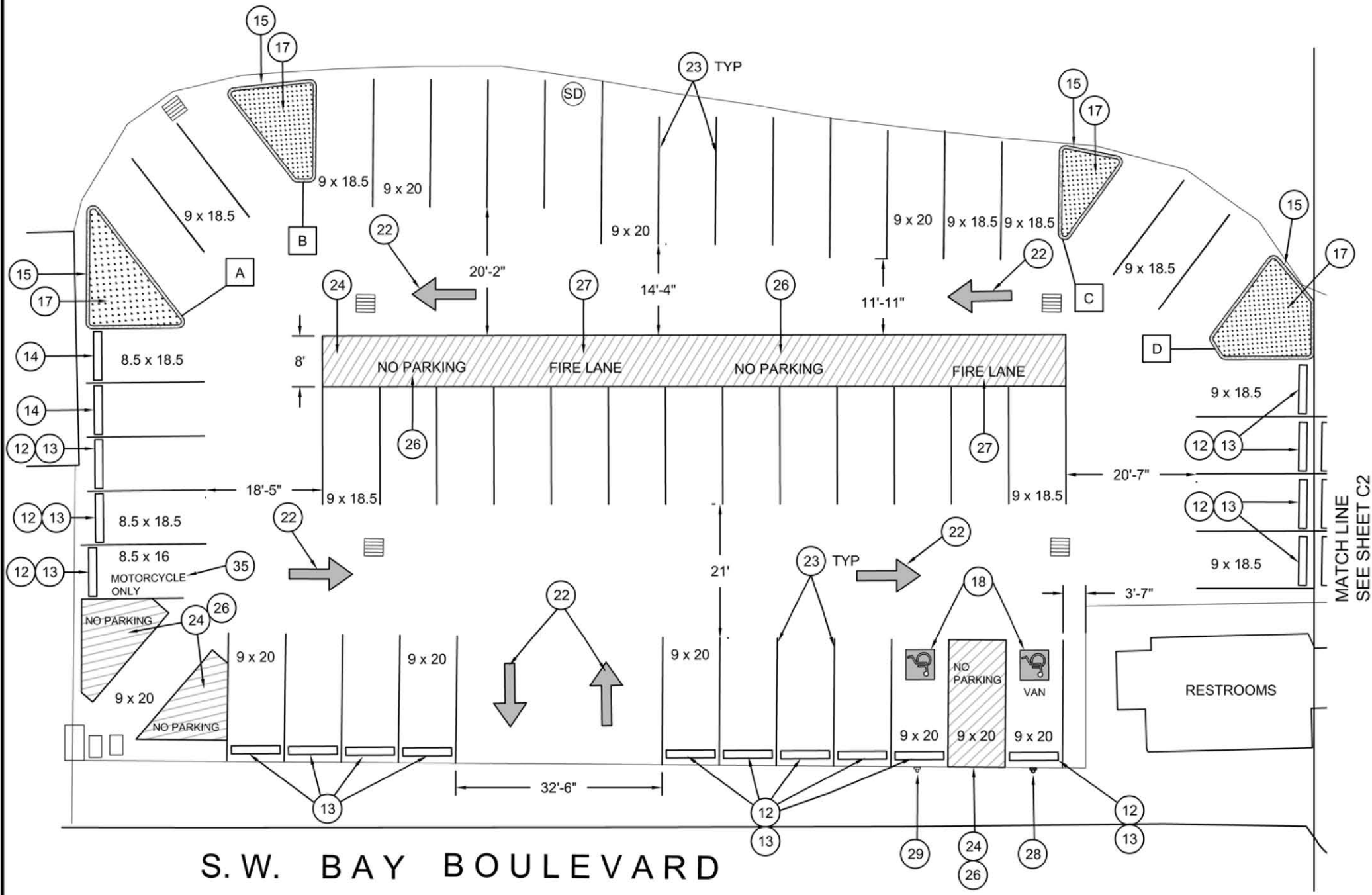
CITY OF NEWPORT, OREGON

BAYFRONT PARKING LOT REFURBISHMENT

PROJECT OVERVIEW

Date: Sheet No: **G3**

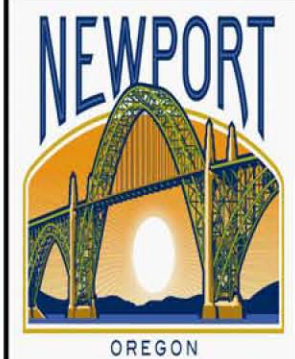
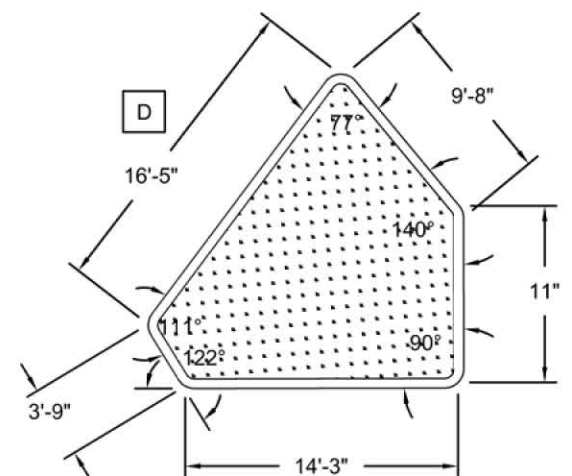
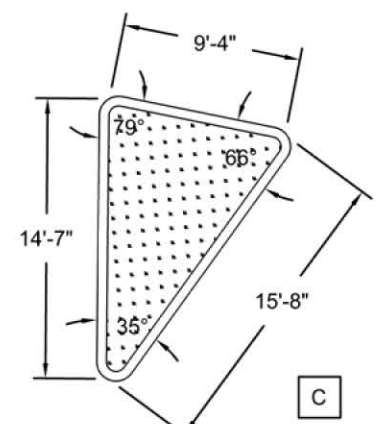
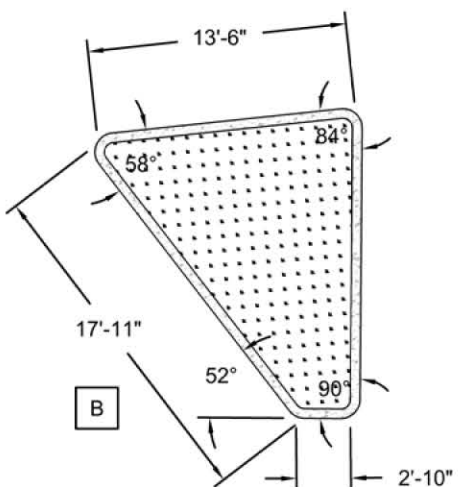
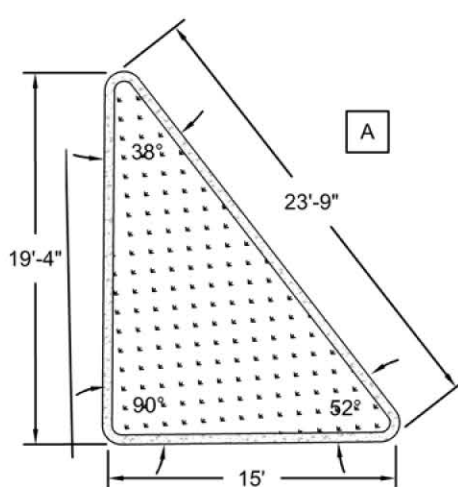
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- CONSTRUCTION NOTES**
- 12 REMOVE AND SALVAGE CONCRETE WHEELSTOP FOR RE-INSTALLATION.
 - 13 INSTALL SALVAGED CONCRETE WHEEL STOP.
 - 14 FURNISH AND INSTALL CONCRETE WHEEL STOP PER DETAIL T-503 SHEET D4.
 - 15 CONSTRUCT TYPE 'C' CONCRETE CURB PER DETAIL #T-301 ON SHEET D2.
 - 17 FURNISH AND INSTALL 4" OF 3"-0" CLEAN RIVER ROCK WITH BOULDERS PER XERISCAPE ISLAND DETAIL ON SHEET D4.
 - 18 INSTALL DISABLED PARKING PAVEMENT LEGEND (HC) PER DETAIL TM500 ON SHEET D5.
 - 19 INSTALL DISABLED PARKING SIGN PER MUTCD DETAIL R7-08P ON NEW SIGN POST PER DETAIL ON SHEET D3.
 - 20 INSTALL DISABLED PARKING WITH VAN ACCESSIBLE SIGN PER MUTCD DETAIL R7-08P ON NEW SIGN POST PER DETAIL ON SHEET D3.
 - 22 INSTALL DIRECTIONAL ARROW (SA) PER DETAIL TM501 ON SHEET D6.
 - 23 INSTALL 4" WHITE LINE (W) PER DETAIL TM500 ON SHEET D5.
 - 24 INSTALL 4" WHITE CROSSHATCH PAVEMENT STRIPING PER DETAIL TM500 ON SHEET D5.
 - 26 INSTALL "NO PARKING" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.
 - 27 INSTALL "FIRE LANE" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.
 - 28 INSTALL VAN ACCESSIBLE SIGN ON EXISTING POST PER MUTCD DETAIL R7-08P.
 - 29 PROTECT EXISTING SIGN(S) AND POST.
 - 35 INSTALL "MOTORCYCLE ONLY" STENCIL IN PAINTED WHITE LETTERS, 4" HIGH.

NOTE:
LANDSCAPE ISLANDS RUN PARALLEL TO PARKING STALLS; EDGE OF ASPHALT MAY NOT BE EVEN

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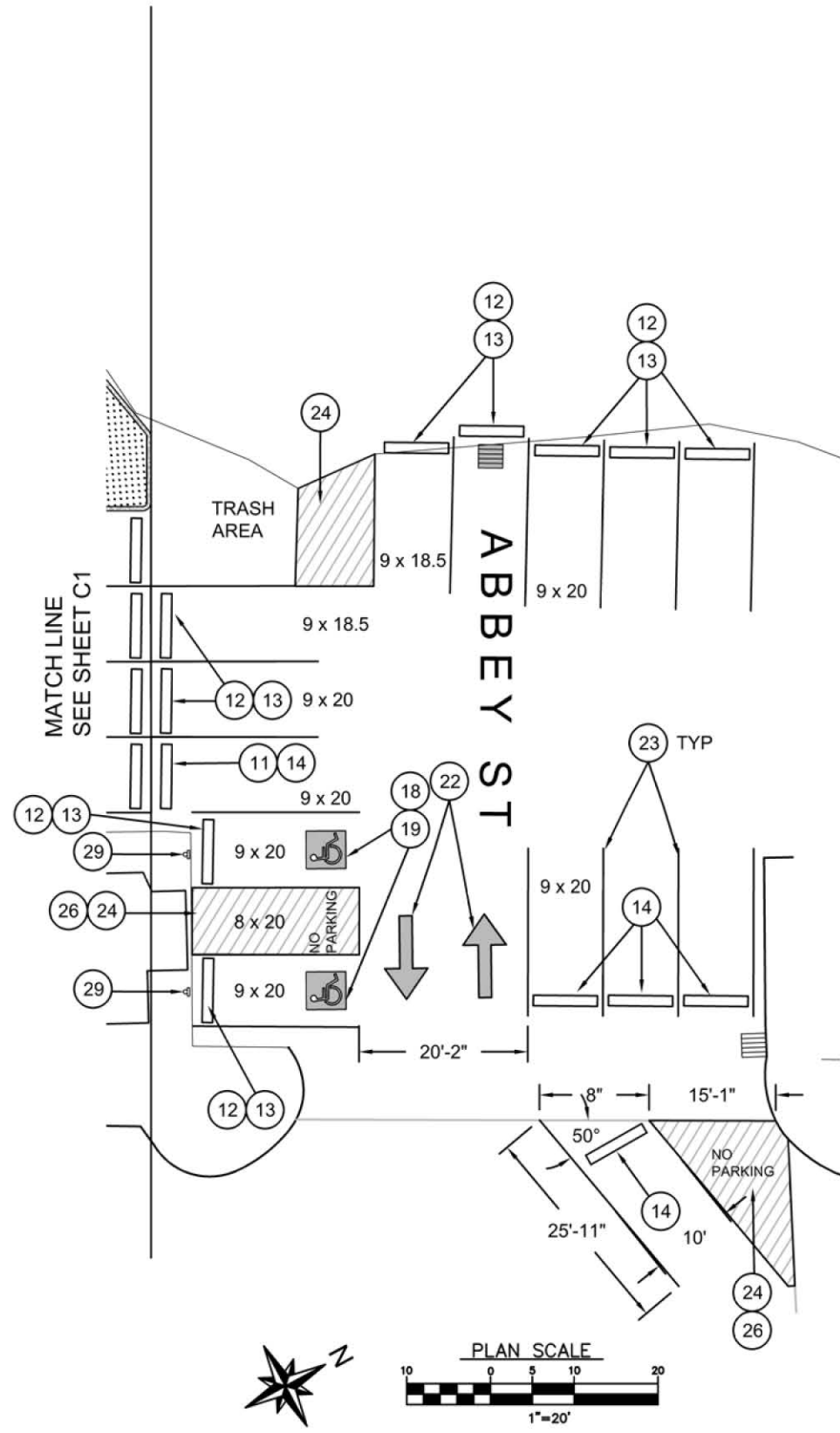
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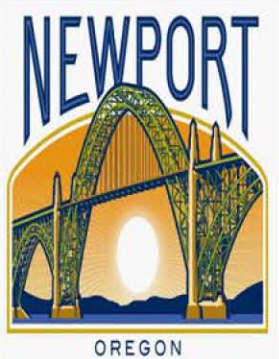
CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT A
ABBAY ST PARKING LOT STRIPING & SITE PLAN

Sheet No: **C1**
Date: **JUNE 2023**

DATE: 6/15/23 FILE: Y:\pkl\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg



- CONSTRUCTION NOTES**
- 11 REMOVE AND DISPOSE OF EXISTING CONCRETE WHEEL STOP.
 - 12 REMOVE AND SALVAGE CONCRETE WHEELSTOP FOR RE-INSTALLATION.
 - 13 INSTALL SALVAGED CONCRETE WHEEL STOP.
 - 14 FURNISH AND INSTALL CONCRETE WHEEL STOP PER DETAIL T-503 SHEET D4.
 - 18 INSTALL DISABLED PARKING PAVEMENT LEGEND (HC) PER DETAIL TM500 ON SHEET D5.
 - 22 INSTALL DIRECTIONAL ARROW (SA) PER DETAIL TM501 ON SHEET D6.
 - 23 INSTALL 4" WHITE LINE (W) PER DETAIL TM500 ON SHEET D5.
 - 24 INSTALL 4" WHITE CROSSHATCH PAVEMENT STRIPING PER DETAIL TM500 ON SHEET D5.
 - 26 INSTALL "NO PARKING" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.
 - 29 PROTECT EXISTING SIGN(S) AND POST.



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CITY OF NEWPORT, OREGON

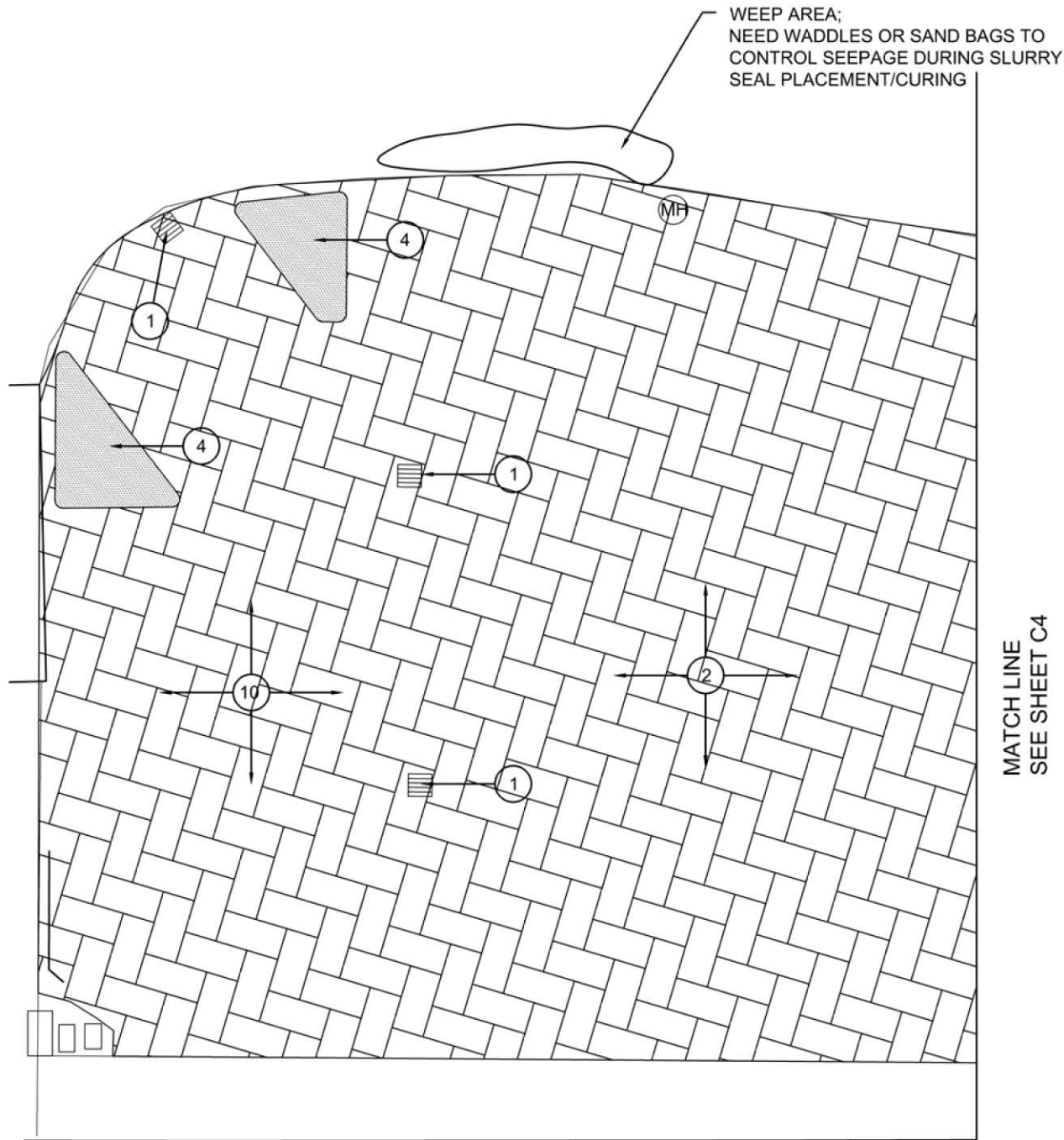
BAYFRONT PARKING LOT REFURBISHMENT

EXHIBIT A
 ABBEY ST PARKING LOT
 STRIPING & SITE PLAN



Date: Sheet No: **C2**
JUNE 2023

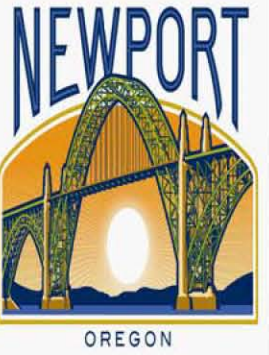
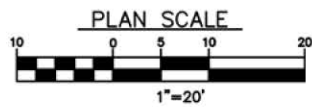
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- CONSTRUCTION NOTES
- 1 INSTALL INLET PROTECTION, TYPE 3, PER DETAIL RD1010 ON SHEET D7.
 - 2 FURNISH AND APPLY LEVEL 1 EMULSION SLURRY SEAL.
 - 4 SAWCUT AND REMOVE EXISTING AC PAVEMENT FOR NEW CURB AND LANDSCAPE PLANTER CONSTRUCTION.
 - 10 CRACK SEAL EXISTING ASPHALT PAVEMENT.

MATCH LINE
SEE SHEET C4



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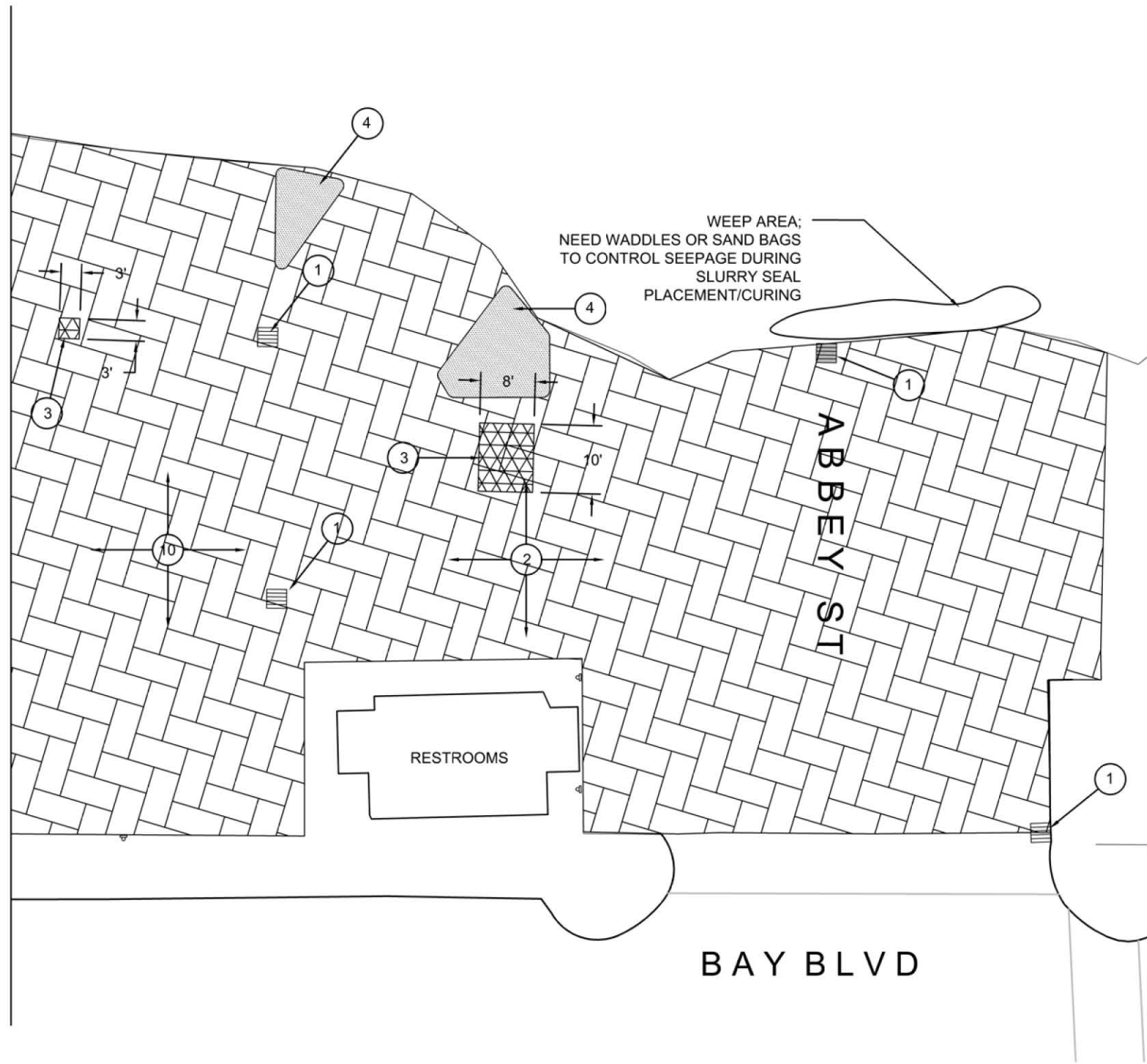
BAYFRONT PARKING LOT REFURBISHMENT

EXHIBIT A
ABBEY ST PARKING LOT
DEMOLITION & SURFACING PLAN

Date: Sheet No: **C3**
JUNE 2023

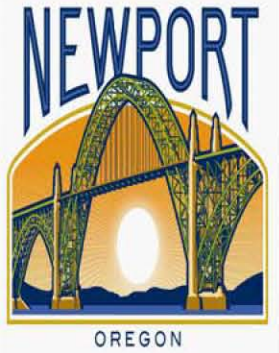
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MATCH LINE
SEE SHEET C3



CONSTRUCTION NOTES

- 1 INSTALL INLET PROTECTION, TYPE 3, PER DETAIL RD1010 ON SHEET D7.
- 2 FURNISH AND APPLY LEVEL 1 EMULSION SLURRY SEAL.
- 3 PAVEMENT DIGOUT INCLUDING SAWCUT, EXCAVATION, 4" OF LEVEL 2 - 1/2" DENSE ASPHALT CONCRETE PAVEMENT IN TWO LIFTS OVER 8" OF 1"-0" CRUSHED ROCK. APPROXIMATE LIMITS SHOWN ON THE PLAN.
- 4 SAWCUT AND REMOVE EXISTING AC PAVEMENT FOR NEW CURB AND LANDSCAPE PLANTER CONSTRUCTION.
- 10 CRACK SEAL EXISTING ASPHALT PAVEMENT.



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Project No: 2021-045		

CITY OF NEWPORT, OREGON

BAYFRONT PARKING LOT REFURBISHMENT

EXHIBIT A

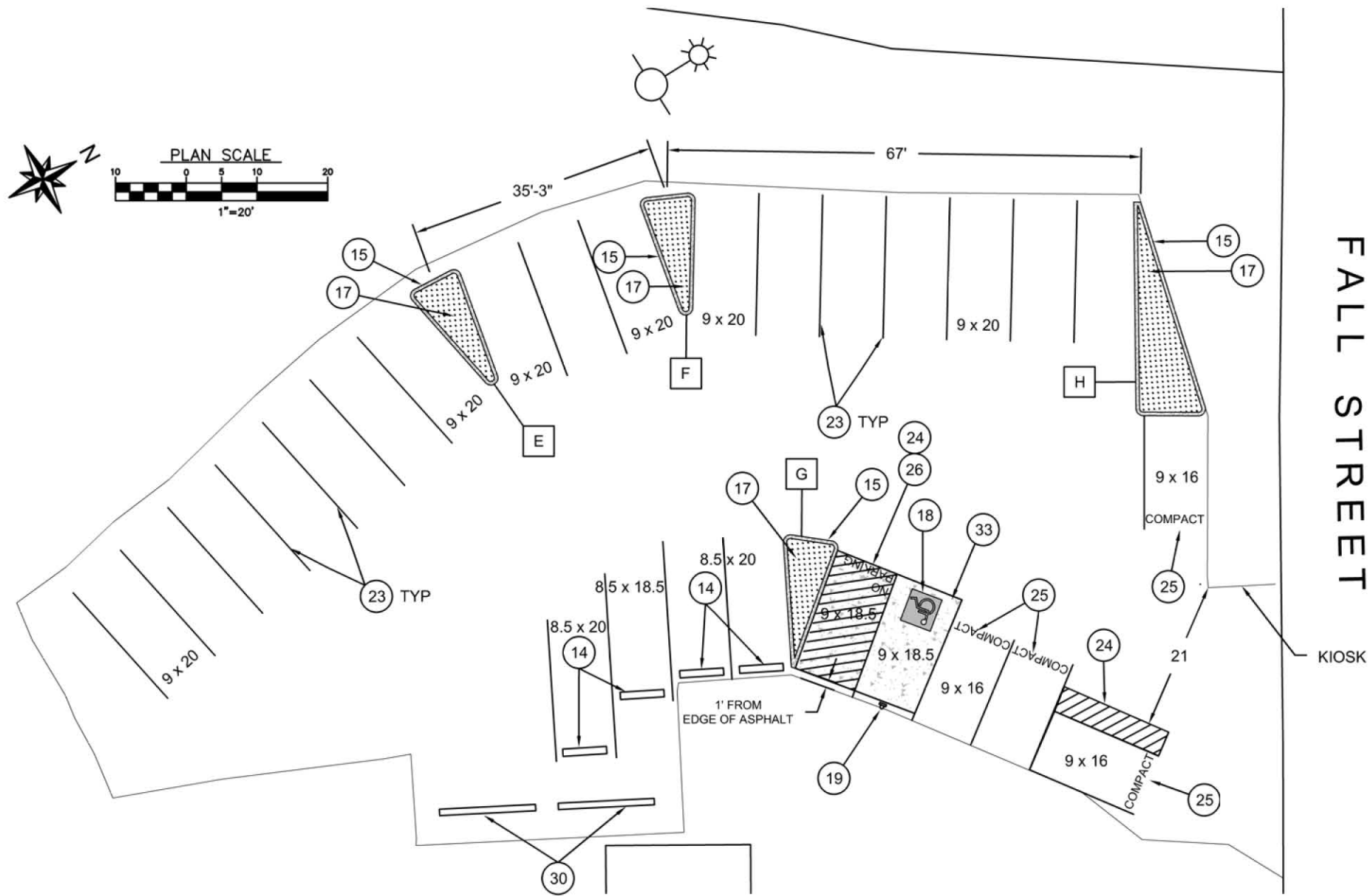
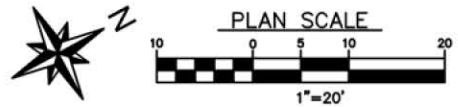
ABBEY ST PARKING LOT
DEMOLITION & SURFACING PLAN



Date: Sheet No: **C4**

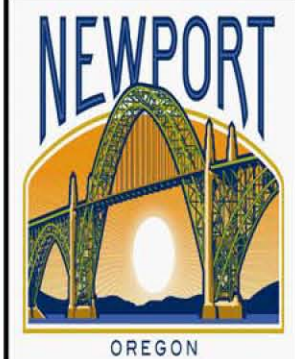
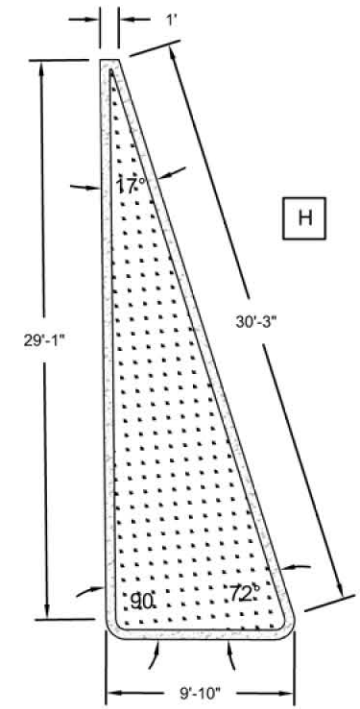
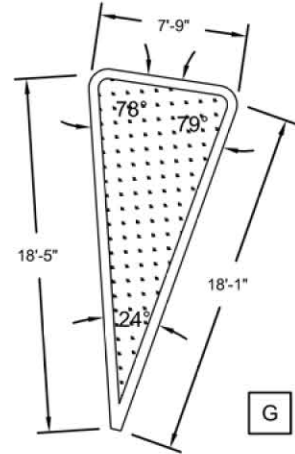
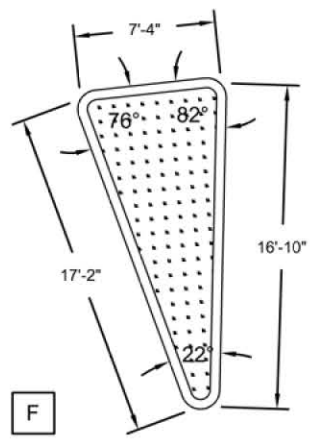
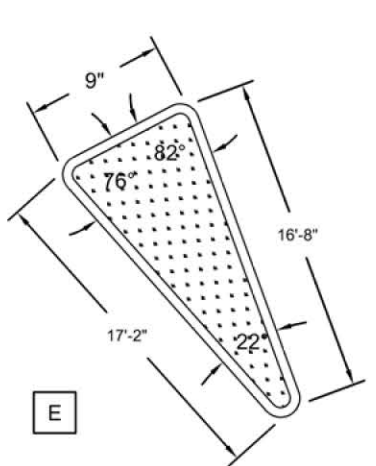
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- CONSTRUCTION NOTES**
- 14 FURNISH AND INSTALL CONCRETE WHEEL STOP PER DETAIL T-503 SHEET D4.
 - 15 CONSTRUCT TYPE 'C' CONCRETE CURB PER DETAIL #T-301 ON SHEET D2.
 - 17 FURNISH AND INSTALL 4" OF 3'-0" CLEAN RIVER ROCK WITH BOULDERS PER XERISCAPE ISLAND DETAIL ON SHEET D4.
 - 18 INSTALL DISABLED PARKING PAVEMENT LEGEND (HC) PER DETAIL TM500 ON SHEET D5.
 - 19 INSTALL DISABLED PARKING SIGN PER MUTCD DETAIL R7-08P ON NEW SIGN POST PER DETAIL ON SHEET D3.
 - 23 INSTALL 4" WHITE LINE (W) PER DETAIL TM500 ON SHEET D5.
 - 24 INSTALL 4" WHITE CROSSHATCH PAVEMENT STRIPING PER DETAIL TM500 ON SHEET D5.
 - 25 INSTALL "COMPACT" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.
 - 26 INSTALL "NO PARKING" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.
 - 30 INSTALL TYPE III BARRICADE 8' LONG PER DETAIL T-501 SHEET D9.
 - 33 INSTALL CONCRETE ADA PARKING SECTION; ADJUST SLOPE ELEVATION TO 2% MAX. EACH WAY; SEE DETAIL T-050B ON SHEET D2A.

NOTES
 LANDSCAPE ISLANDS RUN PARALLEL TO PARKING STALLS; EDGE OF ASPHALT MAY NOT BE EVEN.



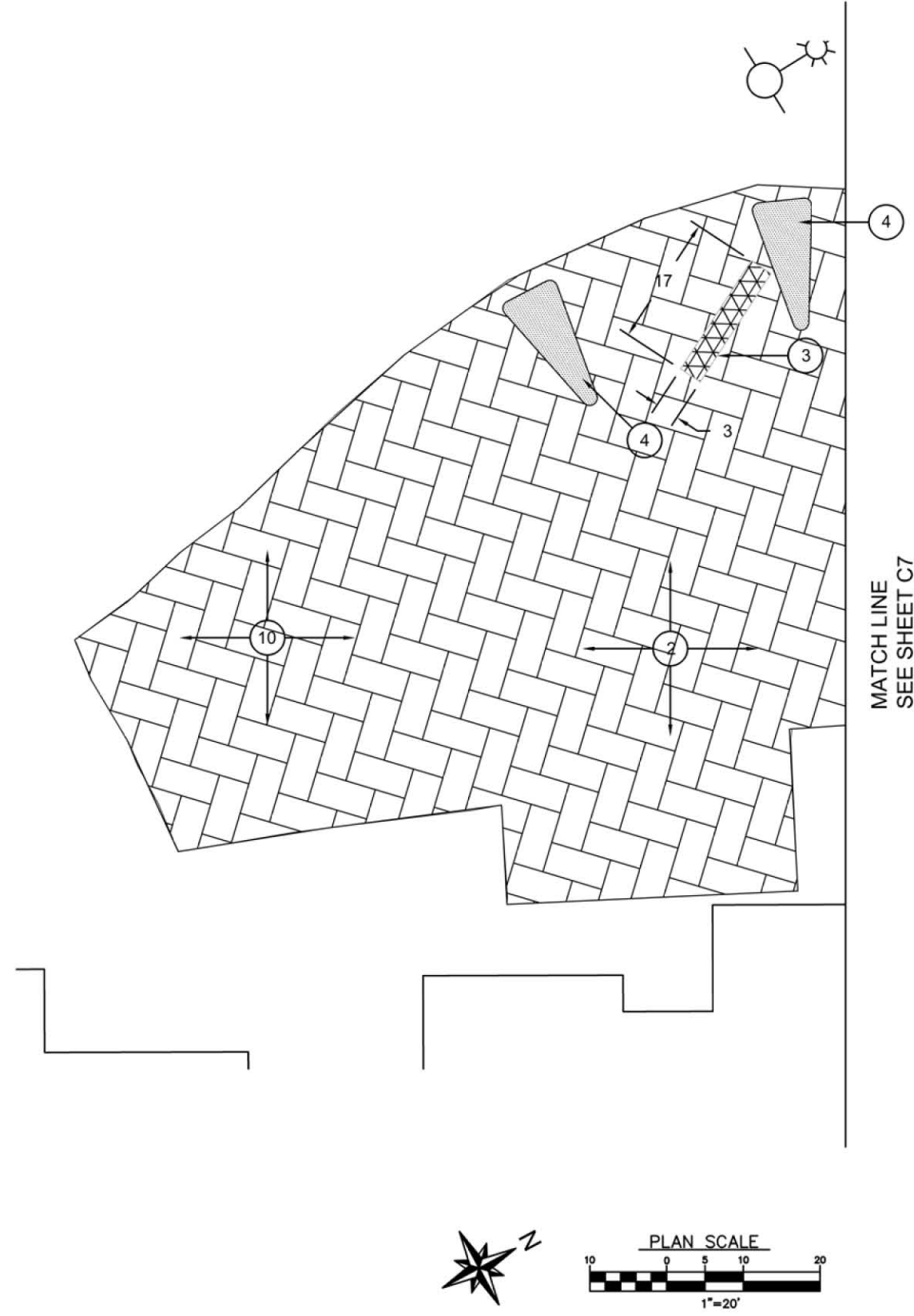
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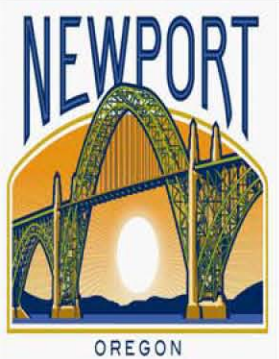
CITY OF NEWPORT, OREGON
 BAYFRONT PARKING LOT REFURBISHMENT
 EXHIBIT B
 FALL ST PARKING LOT STRIPING & SITE PLAN

Date: Sheet No: **C5**
 JUNE 2023



MATCH LINE
SEE SHEET C7

- CONSTRUCTION NOTES**
- 2 FURNISH AND APPLY LEVEL 1 EMULSION SLURRY SEAL.
 - 3 PAVEMENT DIGOUT INCLUDING SAWCUT, EXCAVATION, 4" OF LEVEL 2 - 1/2" DENSE ASPHALT CONCRETE PAVEMENT IN TWO LIFTS OVER 8" OF 1"-0" CRUSHED ROCK. APPROXIMATE LIMITS SHOWN ON THE PLAN.
 - 4 SAWCUT AND REMOVE EXISTING AC PAVEMENT FOR NEW CURB AND LANDSCAPE PLANTER CONSTRUCTION.
 - 10 CRACK SEAL EXISTING ASPHALT PAVEMENT.



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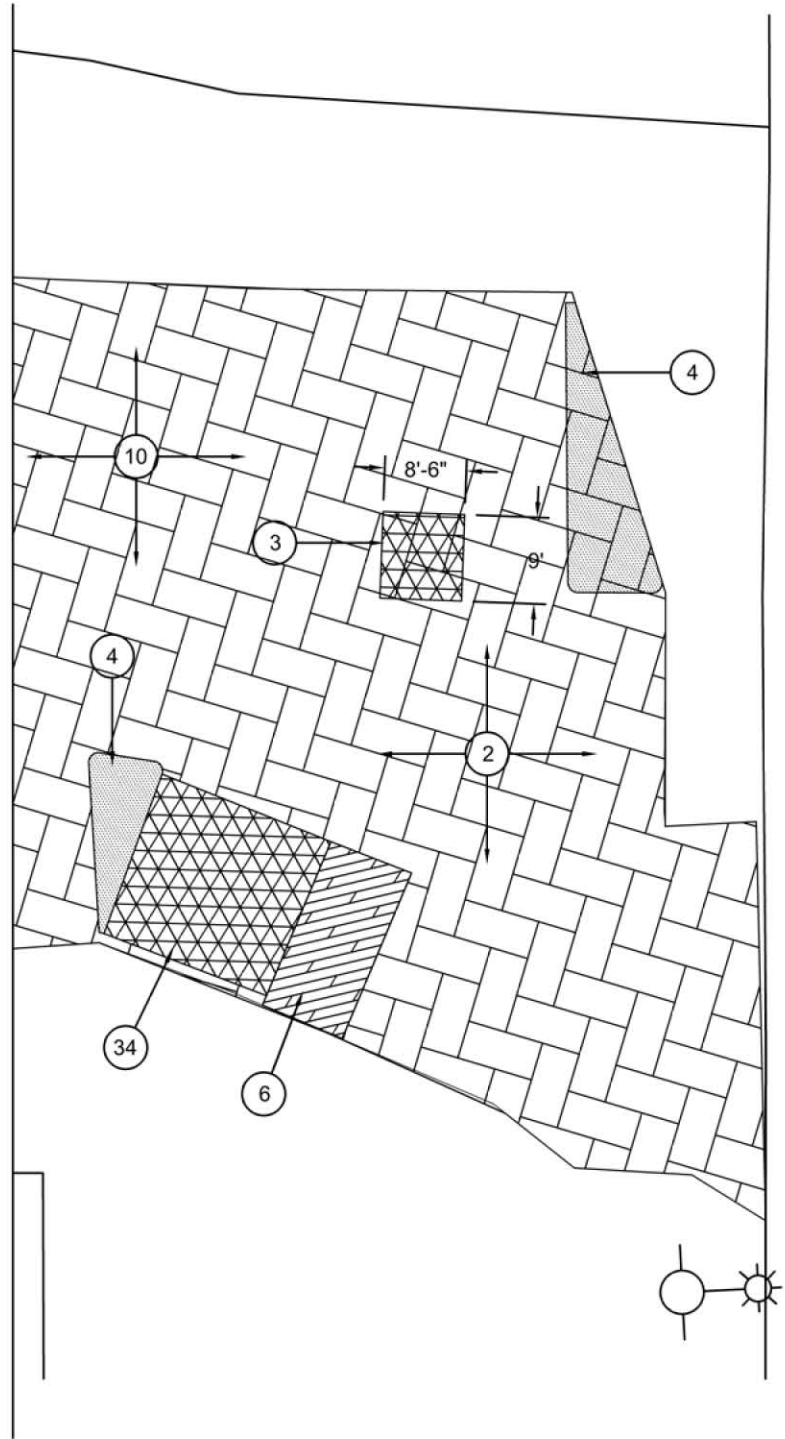
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Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT B
FALL ST PARKING LOT
DEMOLITION & SURFACING PLAN

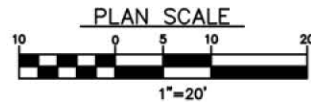
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MATCH LINE
SEE SHEET C6

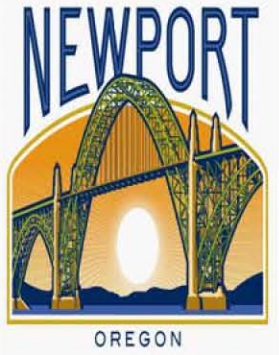


FALL STREET



CONSTRUCTION NOTES

- 2 FURNISH AND APPLY LEVEL 1 EMULSION SLURRY SEAL.
- 3 PAVEMENT DIGOUT INCLUDING SAWCUT, EXCAVATION, 4" OF LEVEL 2 - 1/2" DENSE ASPHALT CONCRETE PAVEMENT IN TWO LIFTS OVER 8" OF 1"-0" CRUSHED ROCK. APPROXIMATE LIMITS SHOWN ON THE PLAN.
- 4 SAWCUT AND REMOVE EXISTING AC PAVEMENT FOR NEW CURB AND LANDSCAPE PLANTER CONSTRUCTION.
- 6 2" COLD PLANE WITH 2" OF LEVEL 2 - 1/2" DENSE ASPHALT CONCRETE OVERLAY.
- 10 CRACK SEAL EXISTING ASPHALT PAVEMENT.
- 34 12" PAVEMENT DIGOUT FOR CONCRETE ADA SPACES AND LOADING ZONE; SEE DETAIL T-050B ON SHEET D2A.



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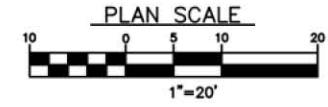
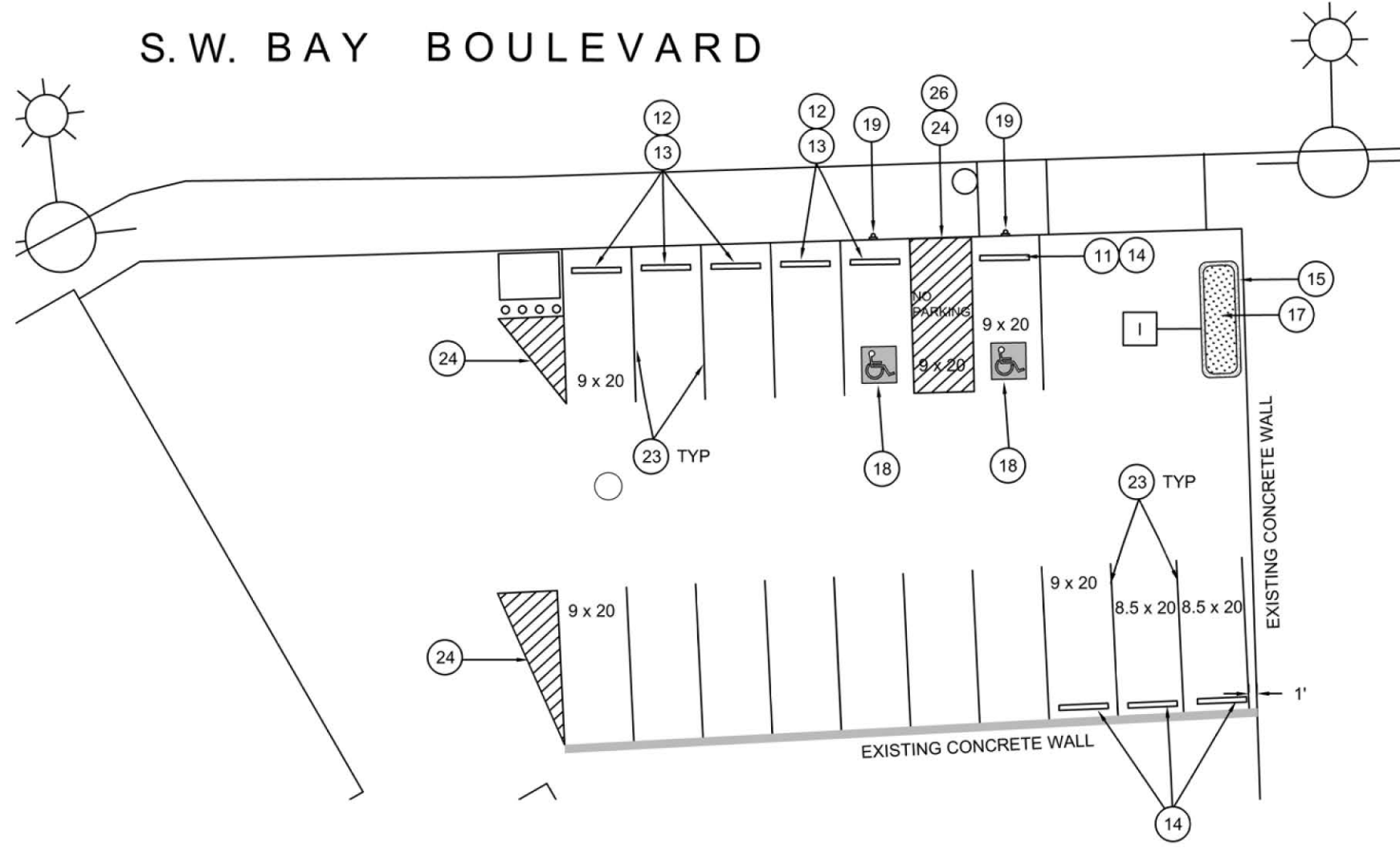
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Project No: 2021-045

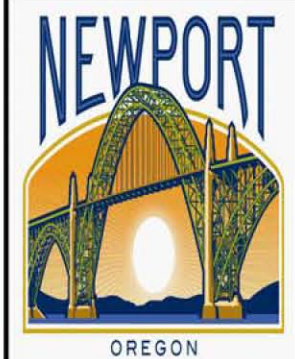
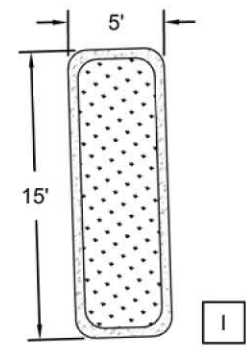
CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT B
FALL ST PARKING LOT
DEMOLITION & SURFACING PLAN

Date: Sheet No: **C7**
JUNE 2023

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- CONSTRUCTION NOTES**
- 11 REMOVE AND DISPOSE OF EXISTING CONCRETE WHEEL STOP.
 - 12 REMOVE AND SALVAGE CONCRETE WHEELSTOP FOR RE-INSTALLATION.
 - 13 INSTALL SALVAGED CONCRETE WHEEL STOP.
 - 14 FURNISH AND INSTALL CONCRETE WHEEL STOP PER DETAIL T-503 SHEET D4.
 - 15 CONSTRUCT TYPE 'C' CONCRETE CURB PER DETAIL #T-301 ON SHEET D2.
 - 17 FURNISH AND INSTALL 4" OF 3"-0" CLEAN RIVER ROCK WITH BOULDERS PER XERISCAPE ISLAND DETAIL ON SHEET D4. GENERAL LANDSCAPE DETAIL SHALL BE FIELD MODIFIED BY CITY REPRESENTATIVE FOR ISLAND I TO INCLUDE 2 TO 3 ADDITIONAL BOULDERS.
 - 18 INSTALL DISABLED PARKING PAVEMENT LEGEND (HC) PER DETAIL TM500 ON SHEET D5.
 - 19 INSTALL DISABLED PARKING SIGN PER MUTCD DETAIL R7-08P ON NEW SIGN POST PER DETAIL ON SHEET D3.
 - 23 INSTALL 4" WHITE LINE (W) PER DETAIL TM500 ON SHEET D5.
 - 24 INSTALL 4" WHITE CROSSHATCH PAVEMENT STRIPING PER DETAIL TM500 ON SHEET D5.
 - 26 INSTALL "NO PARKING" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.



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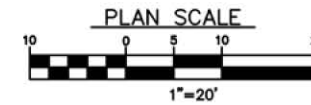
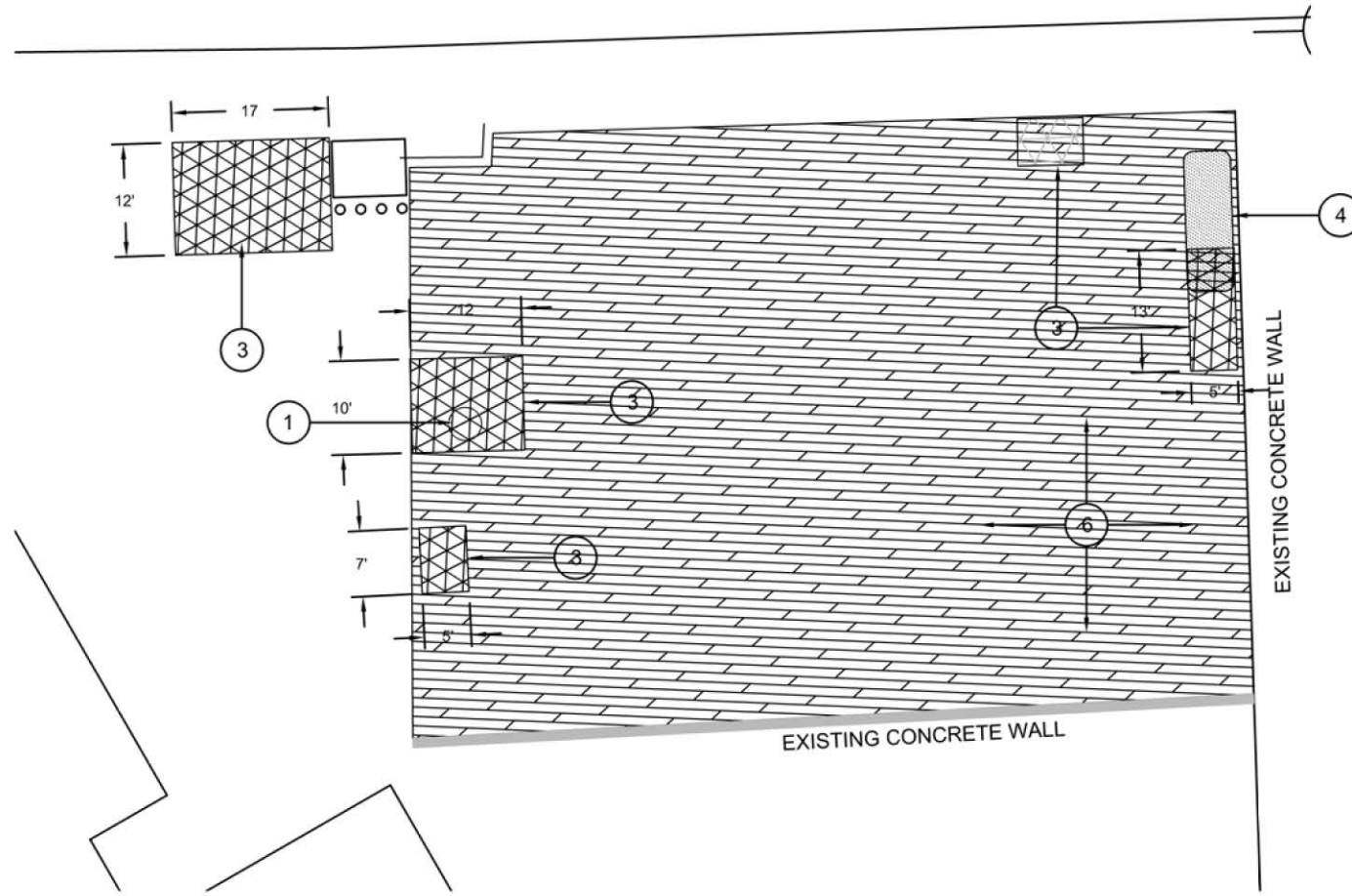
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Drawn By: MUR
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Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT C
BAY BLVD PARKING LOT STRIPING & SITE PLAN

C8

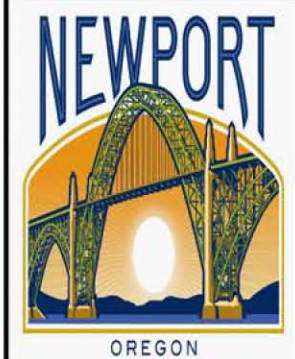
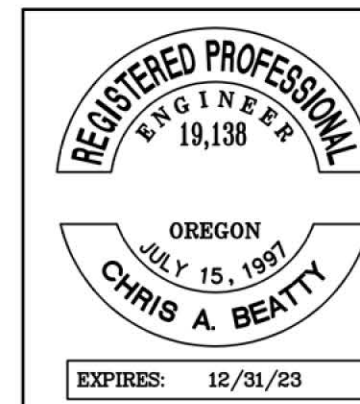
JUNE 2023

BAY BOULEVARD



CONSTRUCTION NOTES

- 1 INSTALL INLET PROTECTION, TYPE 3, PER DETAIL RD1010 ON SHEET D8.
- 3 PAVEMENT DIGOUT INCLUDING SAWCUT, EXCAVATION, 4" OF LEVEL 2 - 1/2" DENSE ASPHALT CONCRETE PAVEMENT IN TWO LIFTS OVER 8" OF 1"-0" CRUSHED ROCK. APPROXIMATE LIMITS SHOWN ON THE PLAN.
- 4 SAWCUT AND REMOVE EXISTING AC PAVEMENT FOR NEW CURB AND LANDSCAPE PLANTER CONSTRUCTION.
- 6 2" COLD PLANE WITH 2" OF LEVEL 2 - 1/2" DENSE ASPHALT CONCRETE OVERLAY.



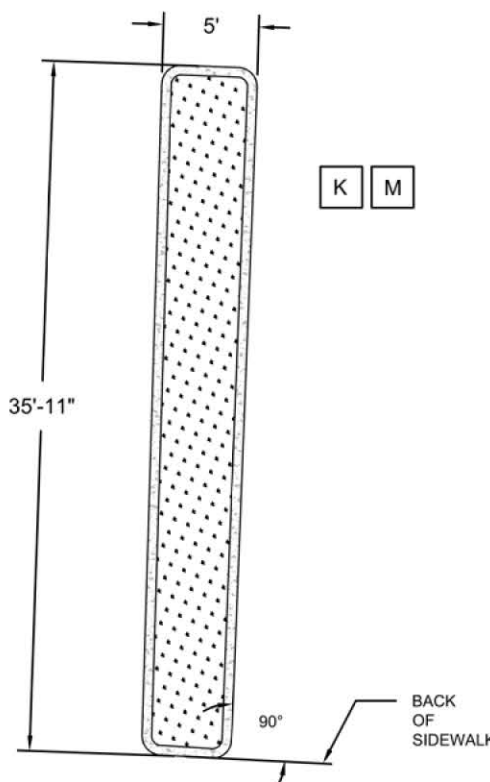
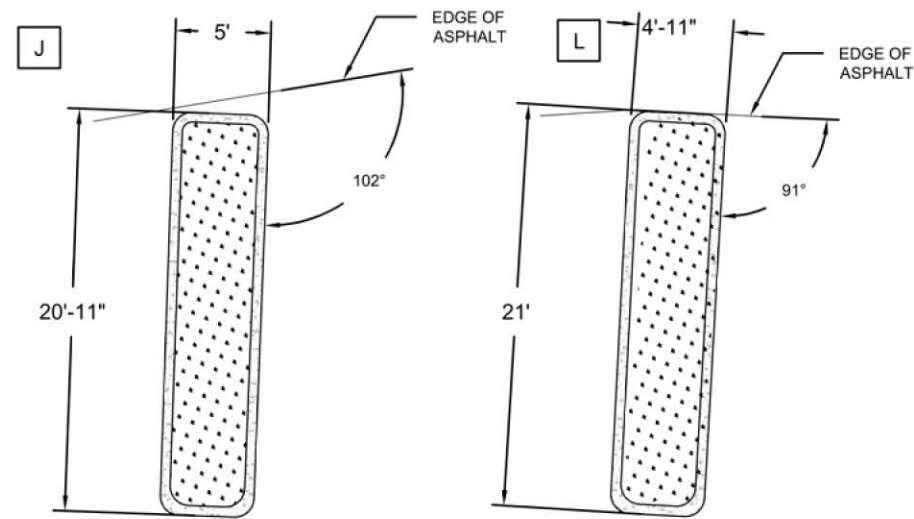
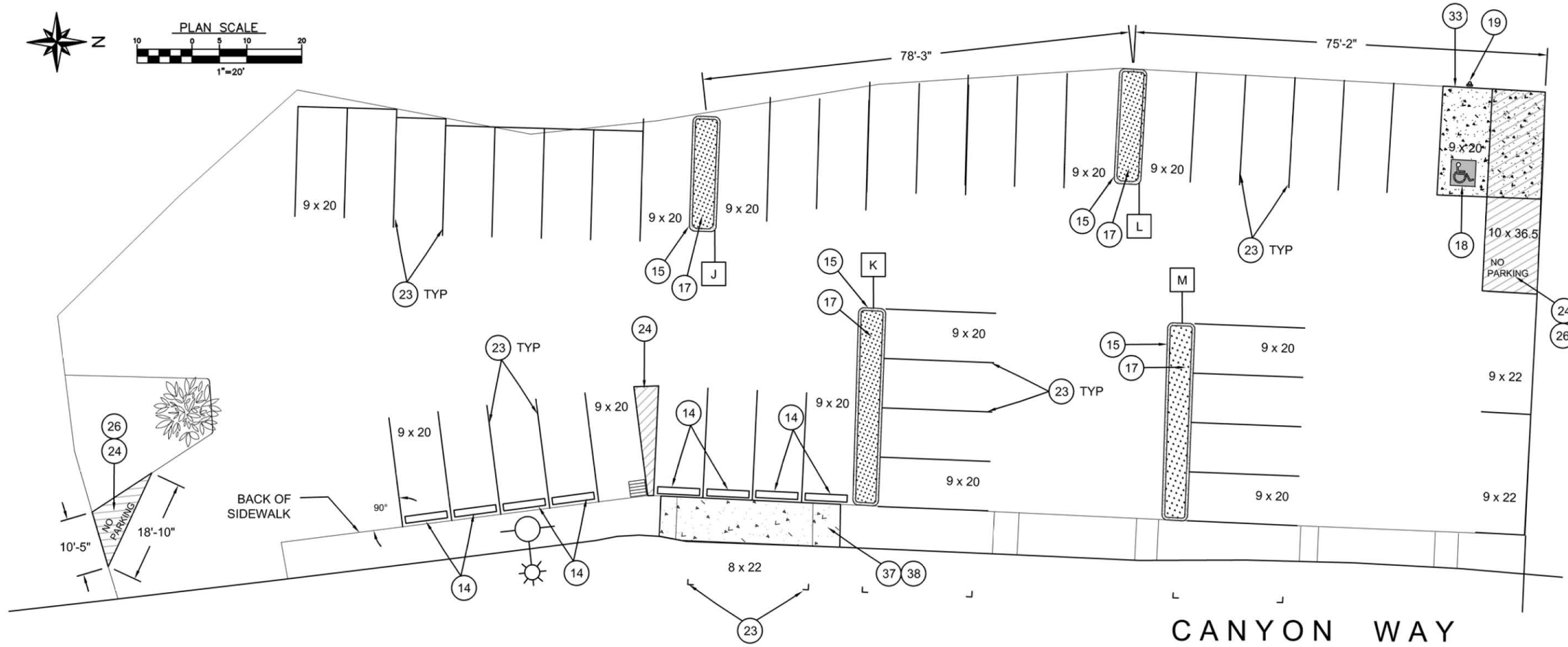
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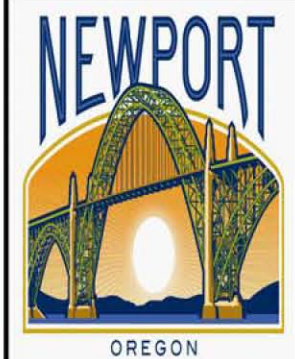
CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT C
BAY BLVD PARKING LOT
DEMOLITION & SURFACING PLAN

Date: Sheet No: **C9**
JUNE 2023



- CONSTRUCTION NOTES**
- 14 FURNISH AND INSTALL CONCRETE WHEEL STOP; SEE DETAIL SHEET D4.
 - 15 CONSTRUCT TYPE 'C' CONCRETE CURB PER DETAIL #T-301 ON SHEET D2.
 - 17 FURNISH AND INSTALL 4" OF 3'-0" CLEAN RIVER ROCK WITH BOULDERS PER XERISCAPE ISLAND DETAIL ON SHEET D4. GENERAL LANDSCAPE DETAIL SHALL BE FIELD MODIFIED BY CITY REPRESENTATIVE FOR ISLANDS K & M TO INCLUDE 2 TO 3 ADDITIONAL BOULDERS.
 - 18 INSTALL DISABLED PARKING PAVEMENT LEGEND (HC) PER DETAIL TM500 ON SHEET D5.
 - 19 INSTALL DISABLED PARKING SIGN PER MUTCD DETAIL R7-08P ON NEW SIGN POST PER DETAIL ON SHEET D3.
 - 23 INSTALL 4" WHITE LINE (W) PER DETAIL TM500 ON SHEET D5.
 - 24 INSTALL 4" WHITE CROSSHATCH PAVEMENT STRIPING PER DETAIL TM500 ON SHEET D5.
 - 26 INSTALL "NO PARKING" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.
 - 33 INSTALL CONCRETE ADA PARKING SECTION; ADJUST SLOPE ELEVATION TO 2% MAX. EACH WAY; SEE DETAIL T-050B ON SHEET D2A.
 - 37 INSTALL STANDARD "C" CURB PER DETAIL T-301 ON SHEET D2.
 - 38 INSTALL 4" SIDEWALK PER DETAIL T-210 ON SHEET D2.

NOTES
 LANDSCAPE ISLANDS RUN PARALLEL TO PARKING STALLS; EDGE OF ASPHALT MAY NOT BE EVEN.



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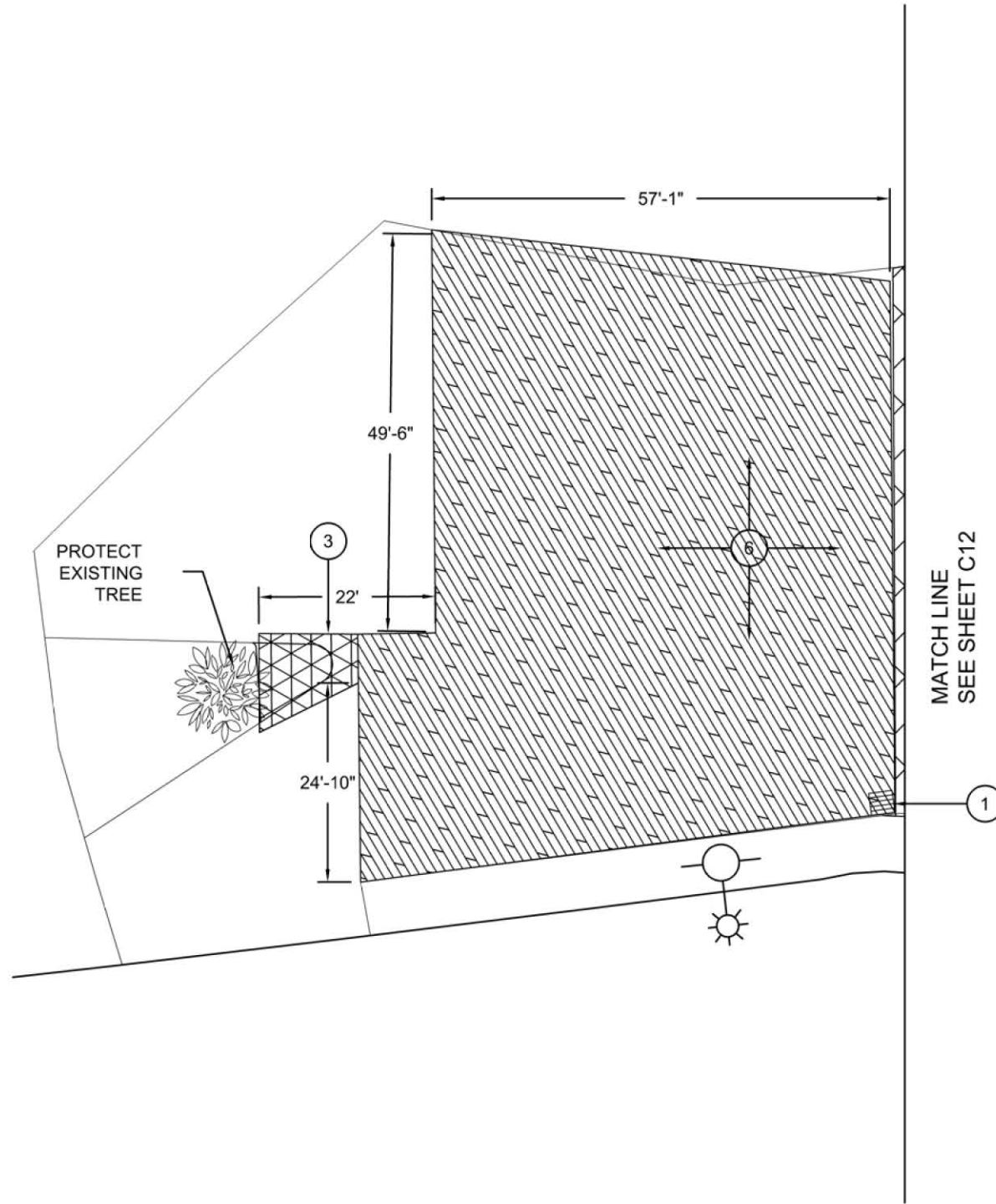
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 Drawn By: MUR
 Checked By: CAB
 Project No: 2021-045

CITY OF NEWPORT, OREGON
 BAYFRONT PARKING LOT REFURBISHMENT
 EXHIBIT D
 CANYON WAY PARKING LOT STRIPING & SITE PLAN

Date: Sheet No: **C10**
JUNE 2023

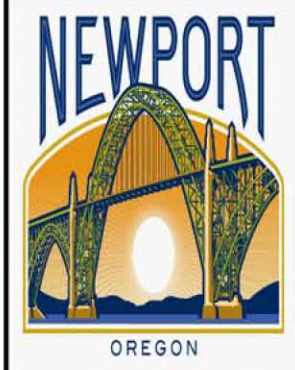
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DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg



- CONSTRUCTION NOTES**
- 1 INSTALL INLET PROTECTION, TYPE 3, PER DETAIL RD1010 ON SHEET D7.
 - 3 PAVEMENT DIGOUT INCLUDING SAWCUT, EXCAVATION, 4" OF LEVEL 2 - 1/2" DENSE ASPHALT CONCRETE PAVEMENT IN TWO LIFTS OVER 8" OF 1"-0" CRUSHED ROCK. APPROXIMATE LIMITS SHOWN ON THE PLAN.
 - 6 2" COLD PLANE WITH 2" OF LEVEL 2 -1/2" DENSE ASPHALT CONCRETE OVERLAY.

MATCH LINE
SEE SHEET C12



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REV.	DATE	DESCRIPTION	BY

Designed By: CAB	Drawn By: MUR	Checked By: CAB
Project No: 2021-045		

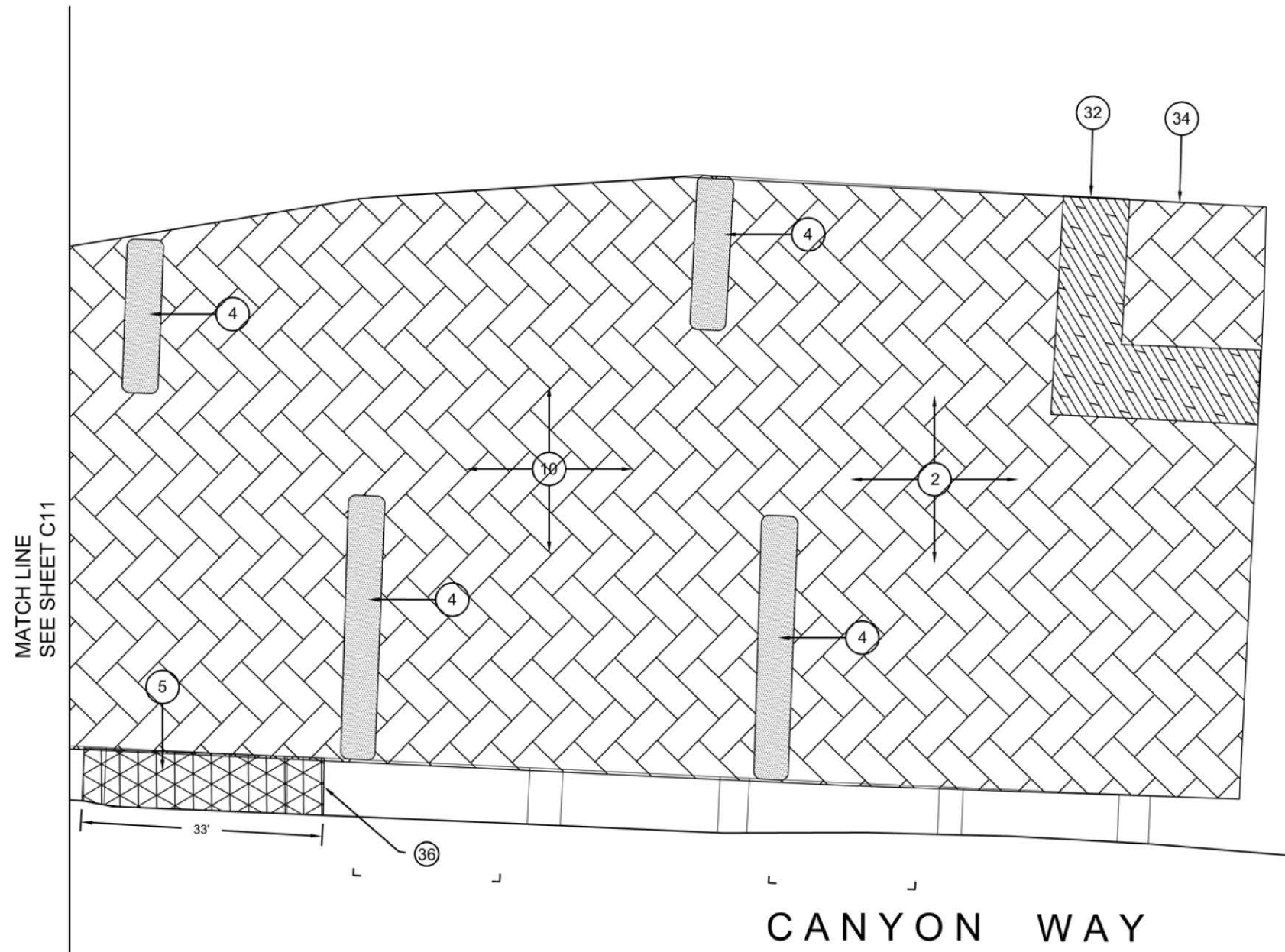
CITY OF NEWPORT, OREGON

BAYFRONT PARKING LOT REFURBISHMENT

EXHIBIT D
CANYON WAY PARKING LOT
DEMOLITION & SURFACING PLAN

Date: Sheet No: **C11**
JUNE 2023

DATE: 6/15/23 FILE: Y:\pavk\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg

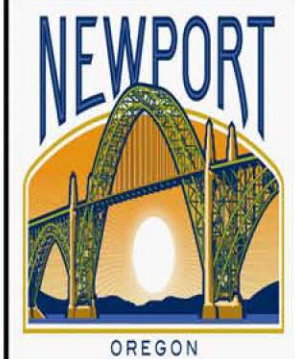


CONSTRUCTION NOTES

- 2 FURNISH AND APPLY LEVEL 1 EMULSION SLURRY SEAL.
- 4 SAWCUT AND REMOVE EXISTING AC PAVEMENT FOR NEW CURB AND LANDSCAPE PLANTER CONSTRUCTION.
- 5 SAWCUT AND REMOVE EXISTING CONCRETE DRIVEWAY FOR NEW CURB AND SIDEWALK CONSTRUCTION.
- 10 CRACK SEAL EXISTING ASPHALT PAVEMENT.
- 32 1"-2" COLD PLANE WITH 2"-3" OF LEVEL 2-1/2" DENSE ASPHALT CONCRETE OVERLAY. FINISH GRADE TO BE 2% MAX, IN ALL DIRECTIONS FOR DISABLED PARKING AND LOADING SPACE
- 34 12" PAVEMENT DIGOUT FOR CONCRETE ADA SPACES; SEE DETAIL T-152 ON SHEET D2A.
- 36 REMOVE EXISTING DRIVEWAY, SIDEWALK, AND CURB PER DRAWING LIMITS.



EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

Designed By: CAB Drawn By: MUR Checked By: CAB
Project No: 2021-045

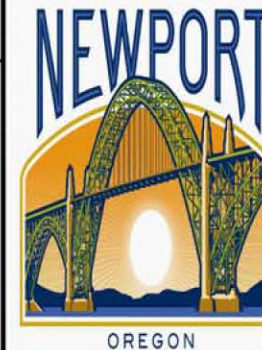
CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT D
CANYON WAY PARKING LOT
DEMOLITION & SURFACING PLAN

Date: Sheet No: **C12**
JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg



EXPIRES: 12/31/23



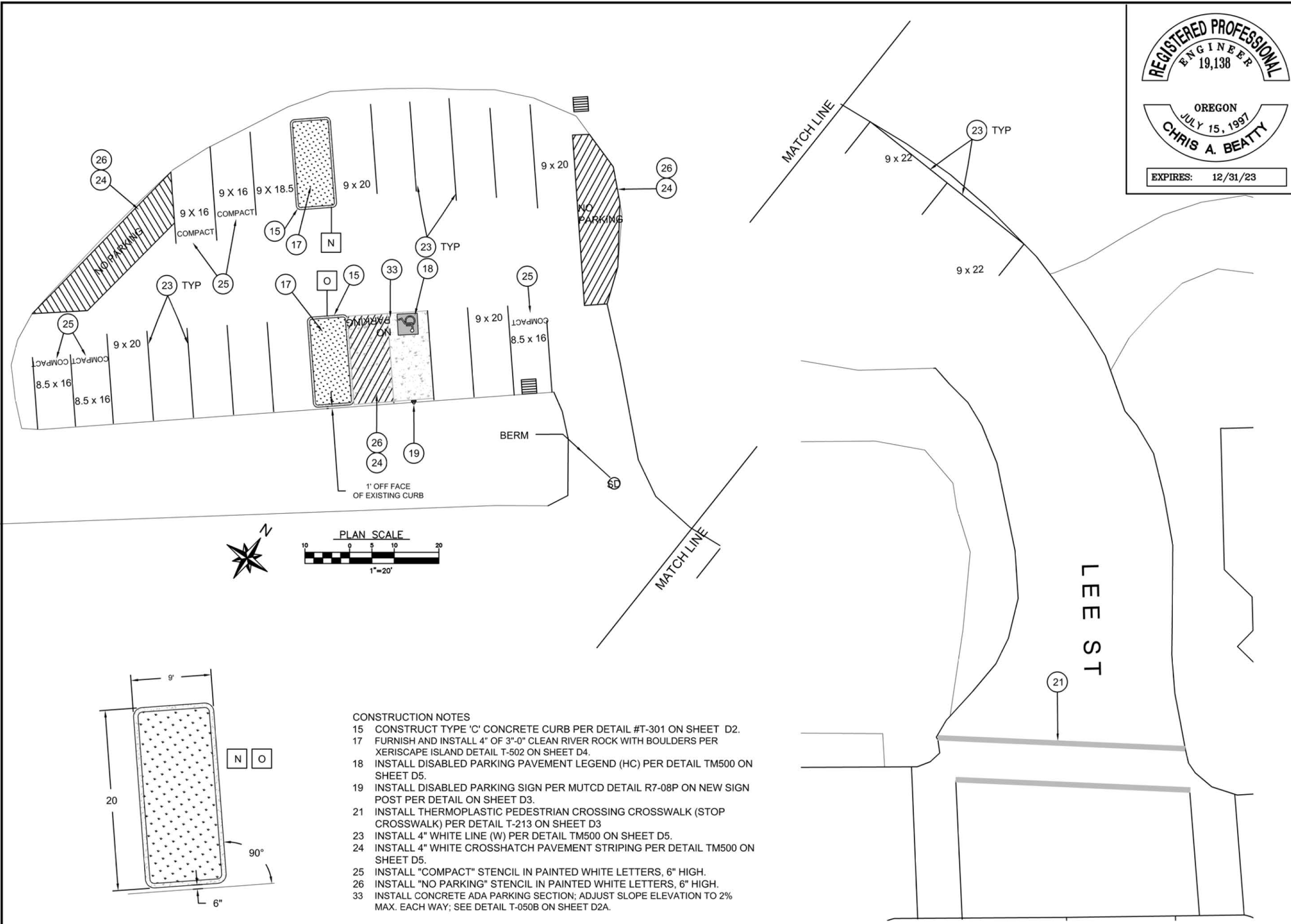
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REV.	DATE	DESCRIPTION	BY

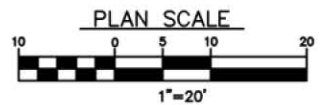
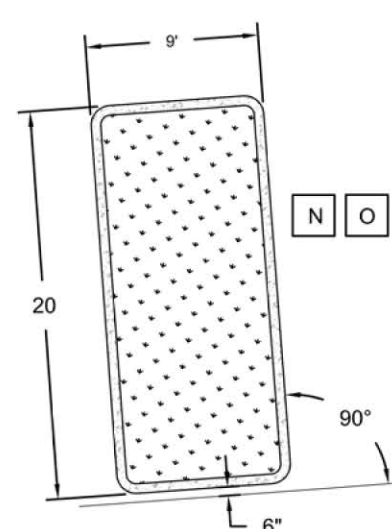
Designed By: CAB	Drawn By: MUR	Checked By: CAB
Project No: 2021-045		

CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT E
LEE ST PARKING LOT
STRIPING & SITE PLAN

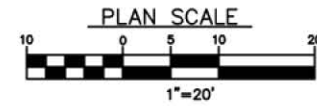
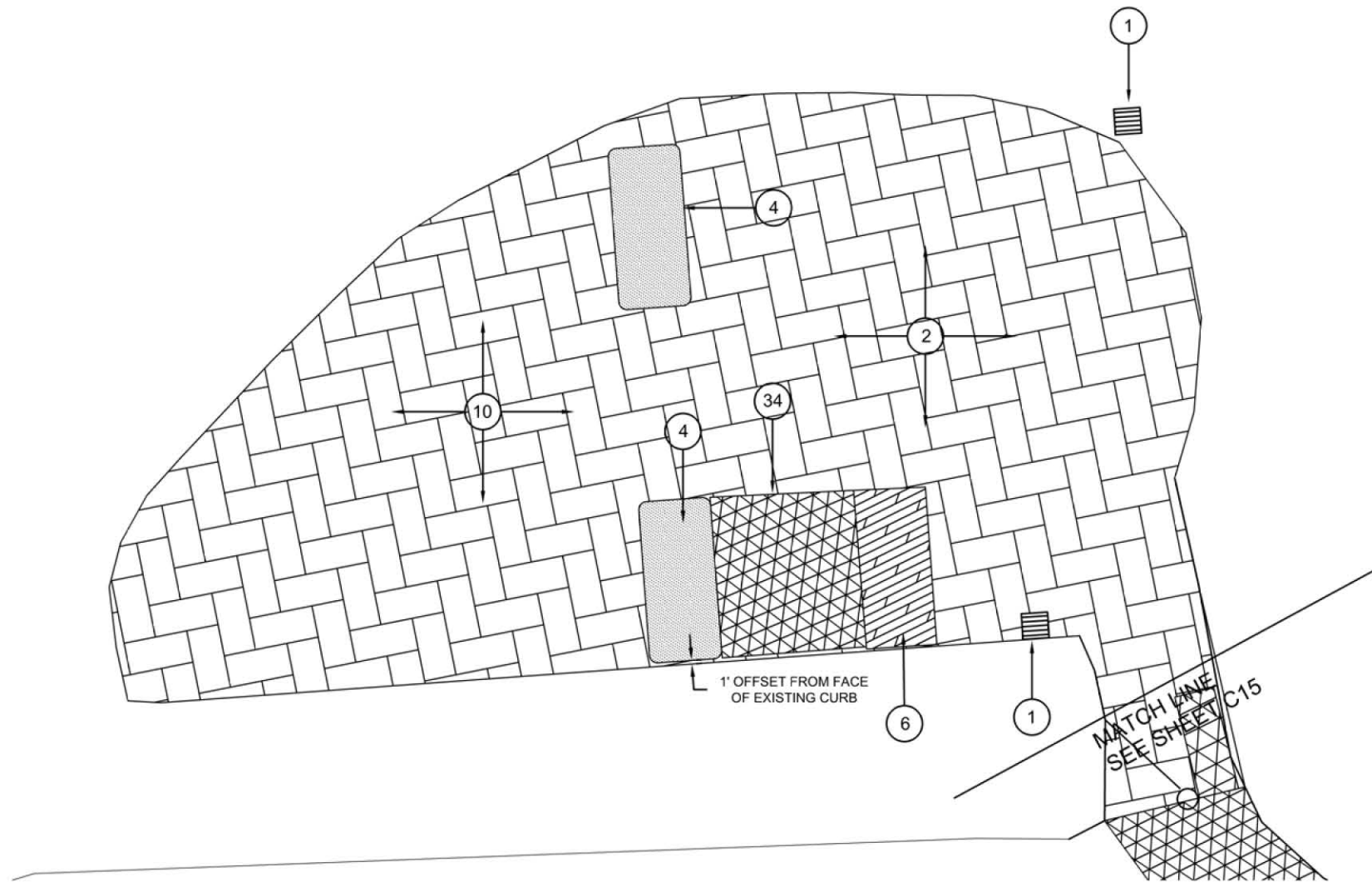
Sheet No: **C13**
Date: **JUNE 2023**



- CONSTRUCTION NOTES**
- CONSTRUCT TYPE 'C' CONCRETE CURB PER DETAIL #T-301 ON SHEET D2.
 - FURNISH AND INSTALL 4" OF 3"-0" CLEAN RIVER ROCK WITH BOULDERS PER XERISCAPE ISLAND DETAIL T-502 ON SHEET D4.
 - INSTALL DISABLED PARKING PAVEMENT LEGEND (HC) PER DETAIL TM500 ON SHEET D5.
 - INSTALL DISABLED PARKING SIGN PER MUTCD DETAIL R7-08P ON NEW SIGN POST PER DETAIL ON SHEET D3.
 - INSTALL THERMOPLASTIC PEDESTRIAN CROSSING CROSSWALK (STOP CROSSWALK) PER DETAIL T-213 ON SHEET D3
 - INSTALL 4" WHITE LINE (W) PER DETAIL TM500 ON SHEET D5.
 - INSTALL 4" WHITE CROSSHATCH PAVEMENT STRIPING PER DETAIL TM500 ON SHEET D5.
 - INSTALL "COMPACT" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.
 - INSTALL "NO PARKING" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.
 - INSTALL CONCRETE ADA PARKING SECTION; ADJUST SLOPE ELEVATION TO 2% MAX. EACH WAY; SEE DETAIL T-050B ON SHEET D2A.



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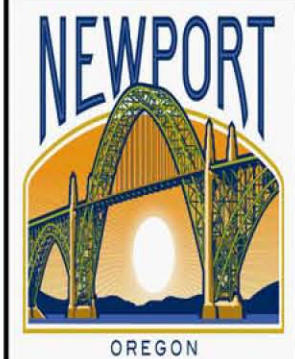


CONSTRUCTION NOTES

- 1 INSTALL INLET PROTECTION, TYPE 3, PER DETAIL RD1010 ON SHEET D7.
- 2 FURNISH AND APPLY LEVEL 1 EMULSION SLURRY SEAL.
- 4 SAWCUT AND REMOVE EXISTING AC PAVEMENT FOR NEW CURB AND LANDSCAPE PLANTER CONSTRUCTION.
- 6 2" COLD PLANE WITH 2" OF LEVEL 2 -1/2" DENSE ASPHALT CONCRETE OVERLAY.
- 10 CRACK SEAL EXISTING ASPHALT PAVEMENT.
- 34 12" PAVEMENT DIGOUT FOR CONCRETE ADA SPACES AND LOADING ZONE; SEE DETAIL T-050B ON SHEET D2A.



EXPIRES: 12/31/23



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Project No: 2021-045		

CITY OF NEWPORT, OREGON

BAYFRONT PARKING LOT REFURBISHMENT

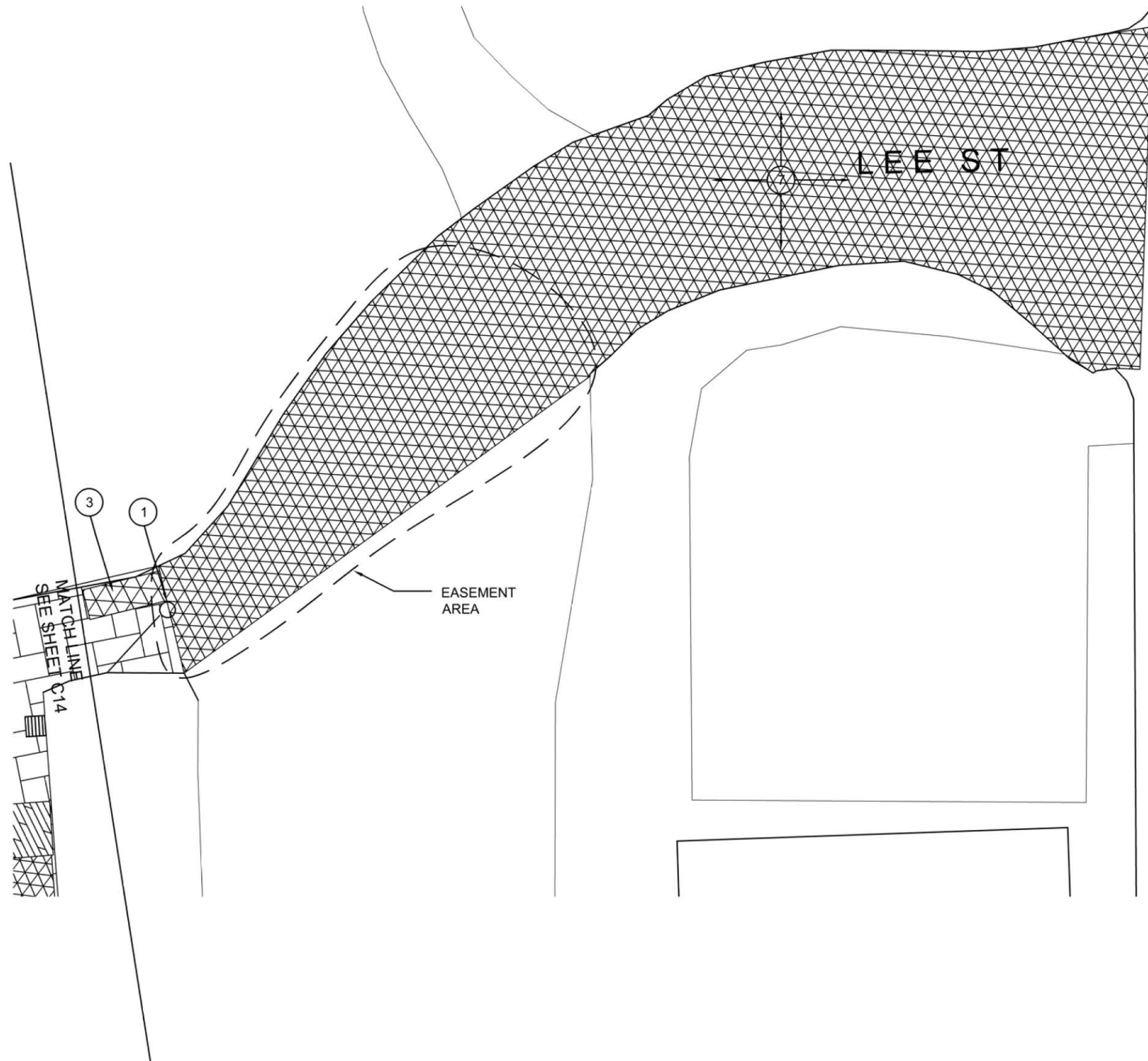
EXHIBIT E

LEE ST PARKING LOT DEMOLITION & SURFACING PLAN

Date: Sheet No: **C14**

JUNE 2023

DATE: 6/15/23 FILE: Y:\pav\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg

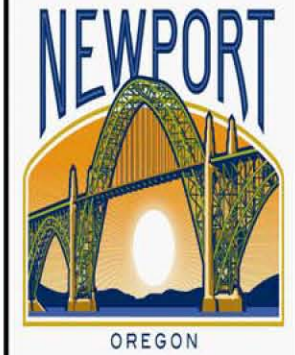


CONSTRUCTION NOTES

- 1 INSTALL INLET PROTECTION, TYPE 3, PER DETAIL RD1010 ON SHEET D7.
- 2 PAVEMENT DIGOUT INCLUDING SAWCUT, EXCAVATION, 4" OF LEVEL 2 - 1/2" DENSE ASPHALT CONCRETE PAVEMENT IN TWO LIFTS OVER 8" OF 1"-0" CRUSHED ROCK. APPROXIMATE LIMITS SHOWN ON THE PLAN.
- 3 SAWCUT AND REMOVE EXISTING ASPHALT PAVEMENT FOR NEW ASPHALT PAVEMENT SECTION PER DETAIL ON SHEET D1.



EXPIRES: 12/31/23



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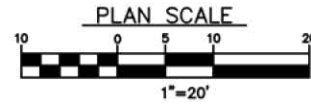
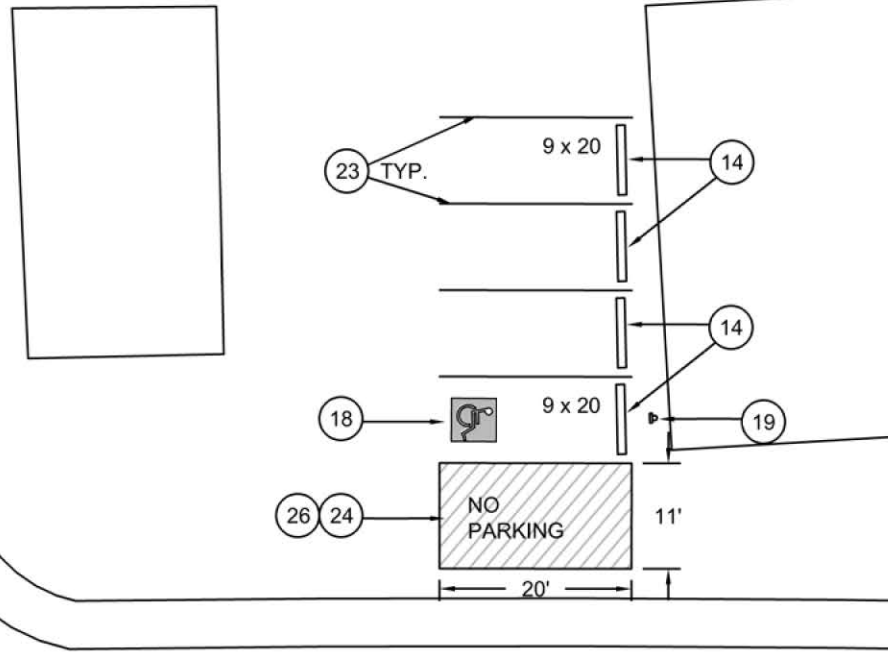
Designed By: CAB Drawn By: MUR Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT E
LEE ST PARKING LOT
DEMOLITION & SURFACING PLAN

Date: Sheet No: **C15**
JUNE 2023

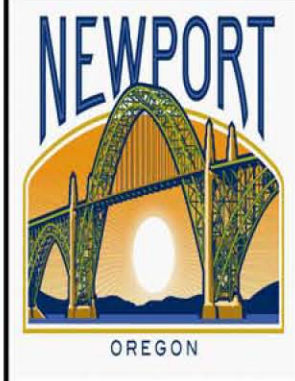
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HATFIELD DRIVE



CONSTRUCTION NOTES

- 14 FURNISH AND INSTALL CONCRETE WHEEL STOP PER DETAIL T-503 SHEET D4.
- 18 INSTALL DISABLED PARKING PAVEMENT LEGEND (HC) PER DETAIL TM500 ON SHEET D5.
- 19 INSTALL DISABLED PARKING SIGN PER MUTCD DETAIL R7-08P ON NEW SIGN POST PER DETAIL ON SHEET D3.
- 23 INSTALL 4" WHITE LINE (W) PER DETAIL TM500 ON SHEET D5.
- 24 INSTALL 4" WHITE CROSSHATCH PAVEMENT STRIPING PER DETAIL TM500 ON SHEET D5.
- 26 INSTALL "NO PARKING" STENCIL IN PAINTED WHITE LETTERS, 6" HIGH.



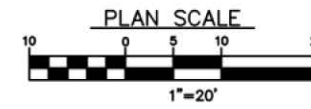
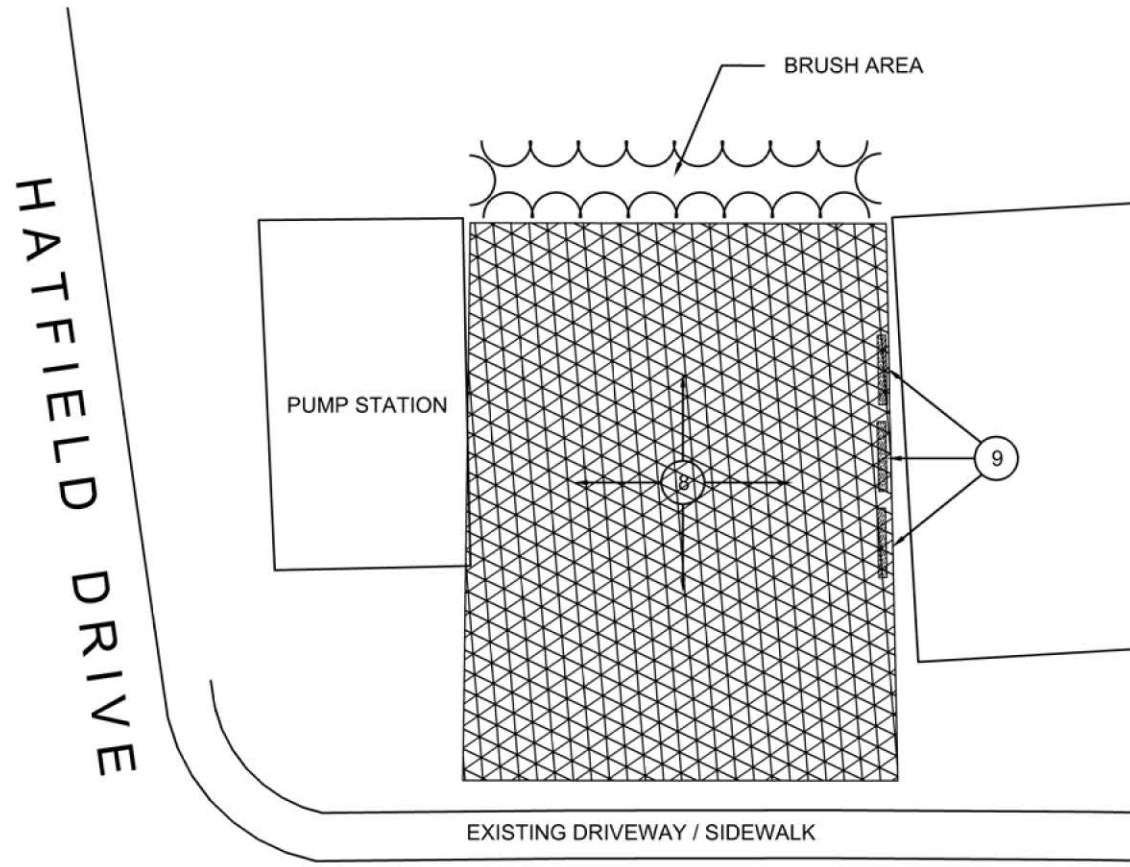
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Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT F
HATFIELD DR PARKING LOT STRIPING & SITE PLAN

Date: Sheet No: **C16**
JUNE 2023

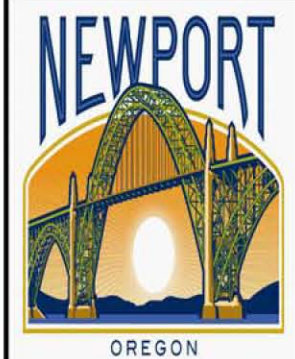


CONSTRUCTION NOTES

- 8 REMOVE EXISTING GRAVEL FOR NEW ASPHALT PAVEMENT SECTION PER DETAIL ON SHEET D1.
- 9 REMOVE EXISTING WOOD PARKING STOPS.



EXPIRES: 12/31/23



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Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING LOT REFURBISHMENT
EXHIBIT F
HATFIELD DR PARKING LOT
DEMOLITION & SURFACING PLAN

Date: Sheet No: **C17**
JUNE 2023

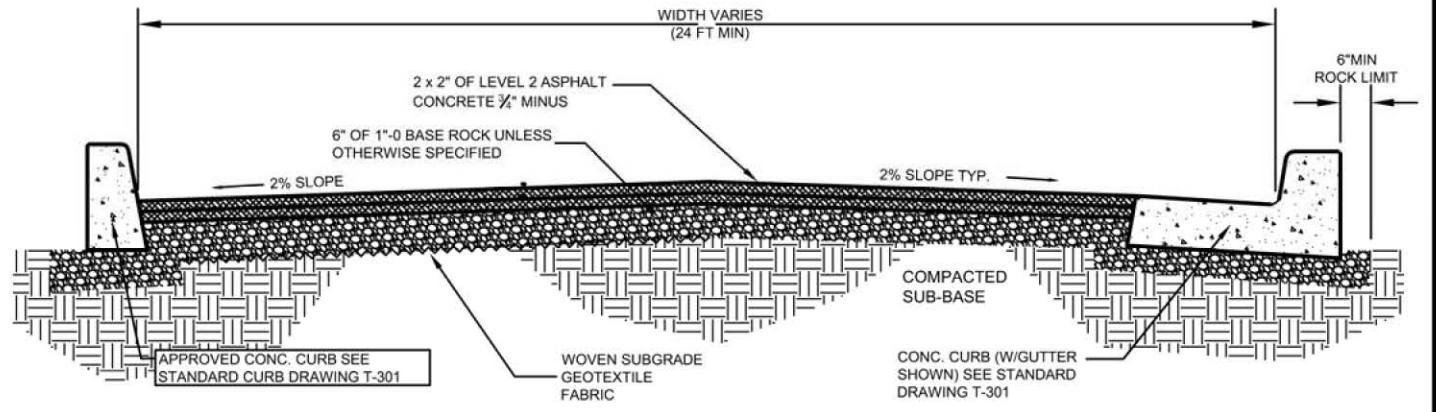


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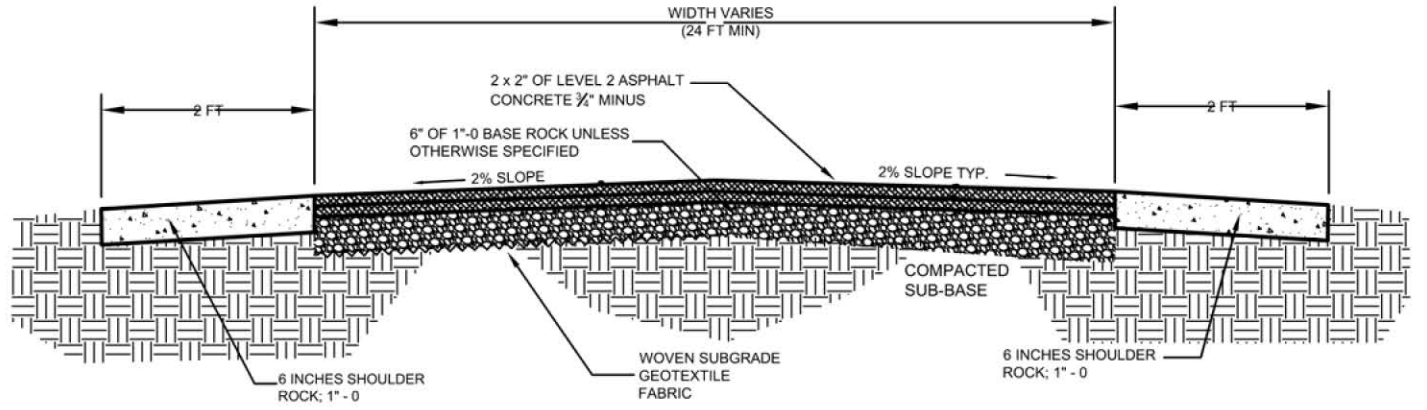
STANDARD PAVED STREET

DETAIL NO.
T-050

5/12/2020



STANDARD PAVED STREET WITH CURB



STANDARD PAVED STREET WITH GRAVEL SHOULDERS

NOTES:

1. ASPHALT CONCRETE PAVEMENT SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00744
2. SEE ODOT STANDARD DRAWING RD610 FOR SAW CUT SPECIFICATIONS
3. NEW ASPHALT CONCRETE SHALL NOT HAVE GREATER THAN A 1/4" VERTICAL TRANSITION WHEN MEETING EXISTING ASPHALT CONCRETE
4. GEOTEXTILE FABRIC SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 02320
5. EARTHWORK AND SUBGRADE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00330
6. AGGREGATE BASE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00641
7. ALL ACP AND JOINTS SHALL BE SANDED AND SEALED WITH TACK.



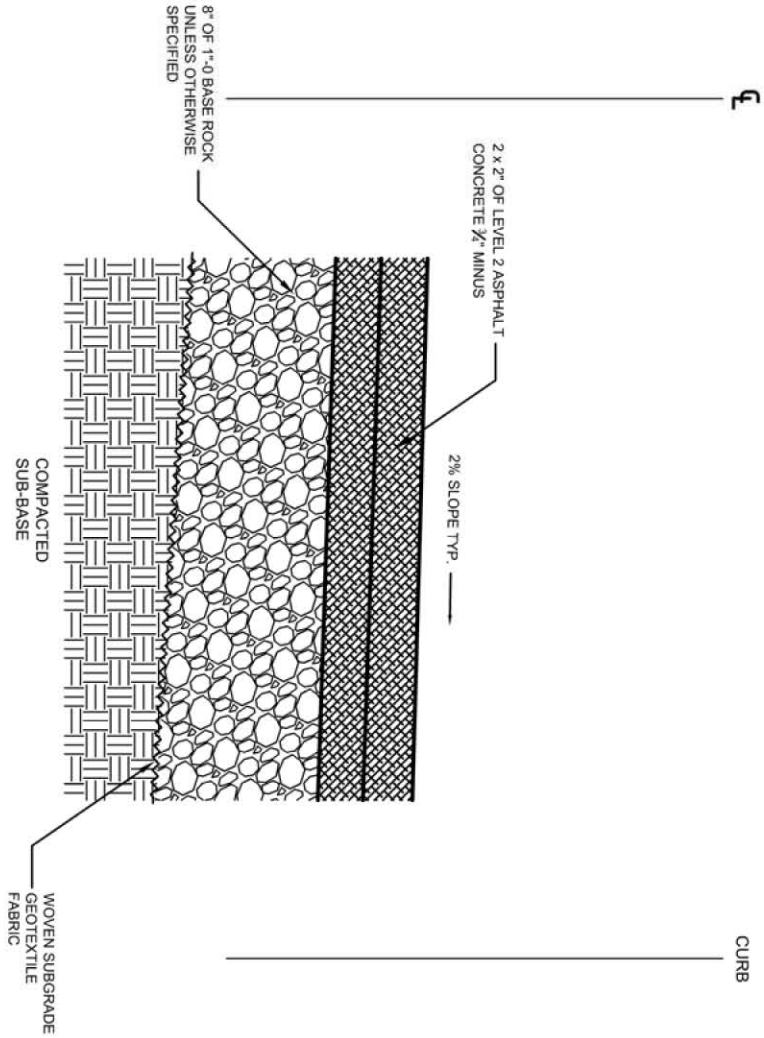
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TYPICAL STREET SECTION

DETAIL NO.
T-050A

5/15/2023

- NOTES:
1. ASPHALT CONCRETE PAVEMENT SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00744
 2. SEE ODOT STANDARD DRAWING RD610 FOR SAW CUT SPECIFICATIONS
 3. NEW ASPHALT CONCRETE SHALL NOT HAVE GREATER THAN A 1/4" VERTICAL TRANSITION WHEN MEETING EXISTING ASPHALT CONCRETE
 4. EARTHWORK AND SUBGRADE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00330
 5. AGGREGATE BASE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00641
 6. ALL ACP AND JOINTS SHALL BE SANDED AND SEALED WITH TACK.



REGISTERED PROFESSIONAL ENGINEER
19,138
OREGON
JULY 15, 1991
CHRIS A. BEATTY
EXPIRES: 12/31/23

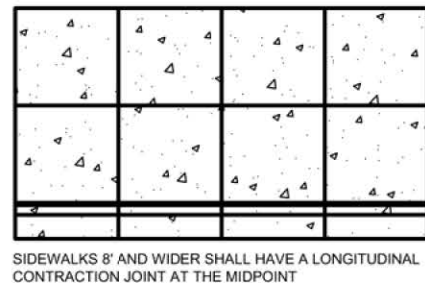
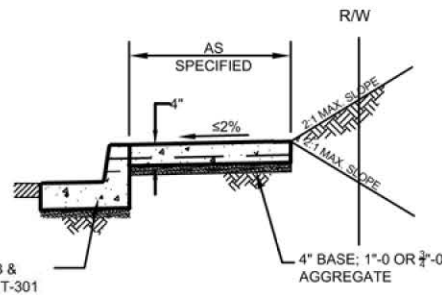
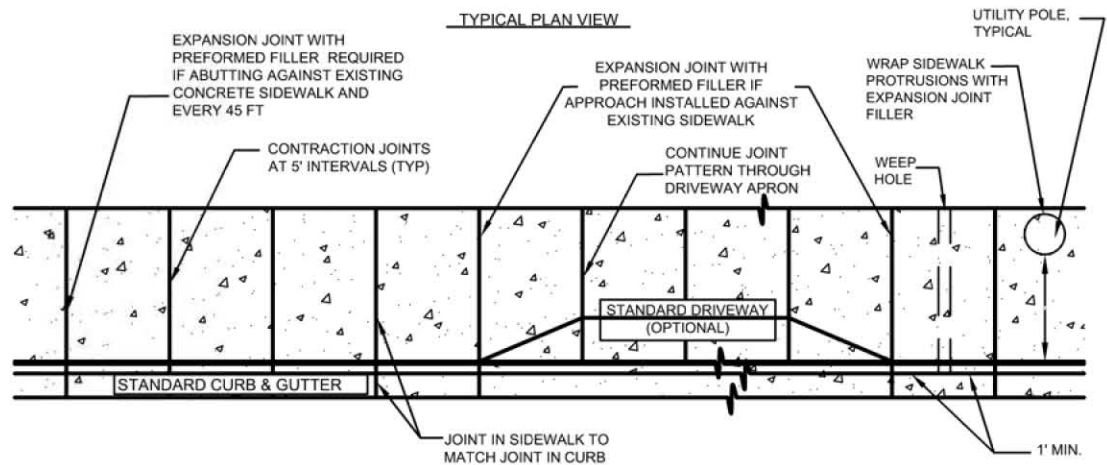
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Date	Sheet No.	CITY OF NEWPORT, OREGON	Designed By:	Drawn By:	Checked By:	
			CAB	MJR	CAB	
Date	Sheet No.	CITY OF NEWPORT, OREGON	Project No:	2021-045		
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JUNE 2023

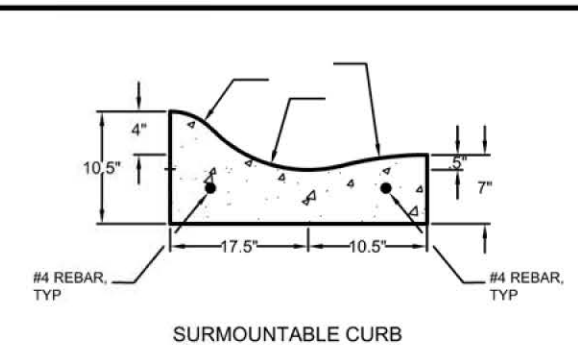
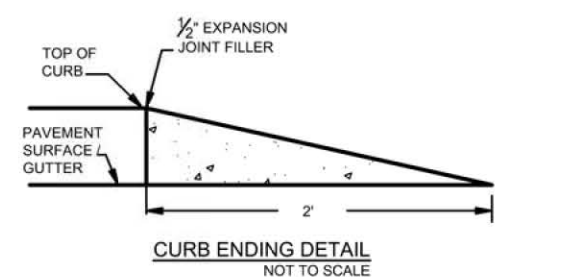
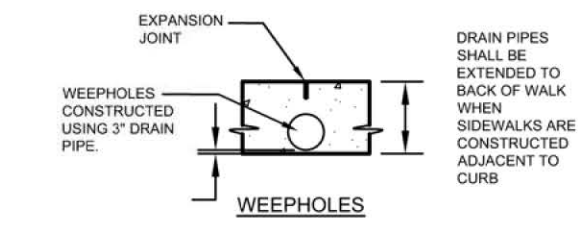
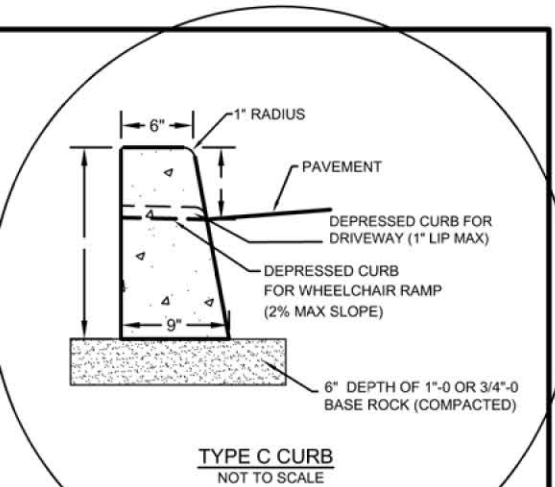
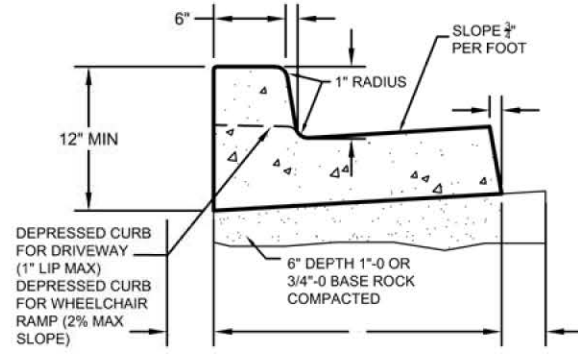
D1

ROAD

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc - Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg



- NOTES:**
- A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR.
 - STRUCTURES SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
 - STANDARD SIDEWALK CROSS SLOPE SHALL BE 2% TOWARD THE STREET. WHEN THE ADJACENT PROPERTY IS LOWER THAN THE TOP OF THE CURB, SIDEWALK MAY SLOPE AWAY FROM STREET.
 - DRAIN BLOCKOUTS IN THE CURB SHALL BE EXTENDED TO THE BACK OF THE SIDEWALK WITH A 3" DIA. PLASTIC PIPE AT A 2% SLOPE. A CONTRACTION JOINT SHALL BE PLACED OVER THE PIPE. NO COUPLINGS UNDER SIDEWALK. OUTFALL AT FACE OF CURB EQUALS GUTTER PAN ELEVATION.
 - SEE STANDARD WHEELCHAIR/BICYCLE RAMP DETAILS ODOT STANDARD DRAWING RD744, CITY STANDARD DRAWING T-212.
 - SEE STANDARD DETAILS FOR DRIVEWAYS: T-150, T-151, AND T-152.
 - WRAP PROTRUDING STRUCTURES IN SIDEWALKS WITH PREFORMED EXPANSION JOINT FILLER.

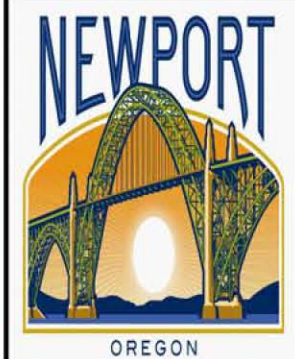


- NOTES:**
- A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR.
 - CURB STRUCTURES SHALL CONFORM TO THE OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
 - CONTRACTION JOINTS
 - TO BE PROVIDED
 - AT EACH POINT OF TANGENCY
 - AT EACH SIDE OF INLET STRUCTURES
 - AT BOTH SIDES OF AN APPROACH
 - SPACING TO BE NOT MORE THAN 15 FEET
 - DEPTH OF JOINT SHALL BE AT LEAST 1/3 THICKNESS OF CONCRETE
 - EXPANSION JOINTS
 - TO BE PROVIDED
 - AT EACH COLD JOINT
 - AT EACH JOINT BETWEEN NEW AND EXISTING CONCRETE
 - EXPANSION JOINTS IN CURB & GUTTER SHALL BE PLACED AT MAX 45' INTERVALS
 - DEPTH OF JOINT SHALL BE EQUAL TO THICKNESS OF CONCRETE
 - EXPANSION JOINTS SHALL USE PREFORMED EXPANSION JOINT FILLER
 - WEEPHOLES
 - DRAINAGE ACCESS THROUGH EXISTING CURBS SHALL BE DONE BY:
 - CORE DRILLING, OR
 - VERTICAL SAWCUT OF CURB 18" EACH SIDE OF DRAIN AND RE-POURED (WITH DOWELS BOTH SIDES INTO EXISTING CUT) TO FULL DEPTH OF CURB OR CURB & GUTTER
 - STAMP TOP OF CURB WITH "W" AT WATER SERVICE CROSSING AND "S" AT SANITARY LATERAL CROSSING AS SPECIFIED.
 - SEE STANDARD CURB CUT DETAIL FOR DRIVEWAYS: T-150, T-151, AND T-152.
 - TYPE C CURB MAY BE USED FOR REPLACEMENT OF EXISTING TYPE C CURBS ONLY UPON APPROVAL BY CITY ENGINEER.
 - STANDARD CURB AND GUTTER MUST BE ROTATED ON SHED ROADS TO DRAIN TOWARD STREET.

CURB AND GUTTER DETAILS

DETAIL NO. T-301

5/12/2020



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Checked By: CAB
Project No: 2021-045

NEWPORT

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STANDARD SIDEWALK DETAILS

DETAIL NO. T-210

5/12/2020

NEWPORT

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CURB AND GUTTER DETAILS

DETAIL NO. T-301

5/12/2020



CITY OF NEWPORT, OREGON

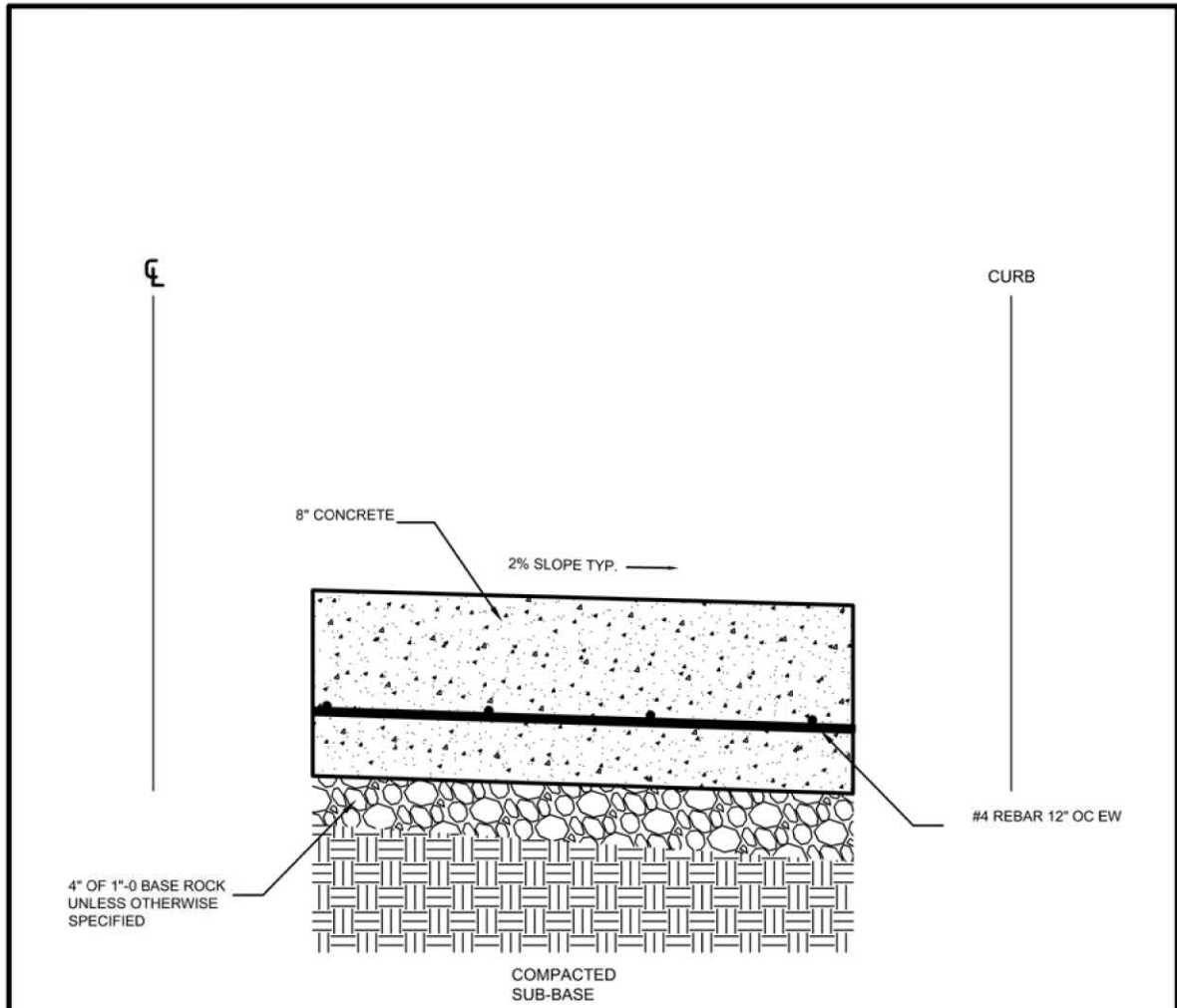
DETAILS

SIDEWALK & CURB

Sheet No: **D2**

Date: **JUNE 2023**

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg



NOTES:

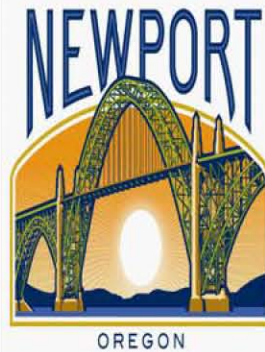
1. CONCRETE PAVEMENT SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00756
2. SEE ODOT STANDARD DRAWING RD610 FOR SAW CUT SPECIFICATIONS
3. EARTHWORK AND SUBGRADE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00330
4. AGGREGATE BASE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00641



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TYPICAL CONCRETE SECTION

DETAIL NO.
T-050B
 6/14/2023



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REV.	DATE	DESCRIPTION	BY

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 Drawn By: MJR
 Checked By: CAB
 Project No: 2021-045

CITY OF NEWPORT, OREGON

DETAILS

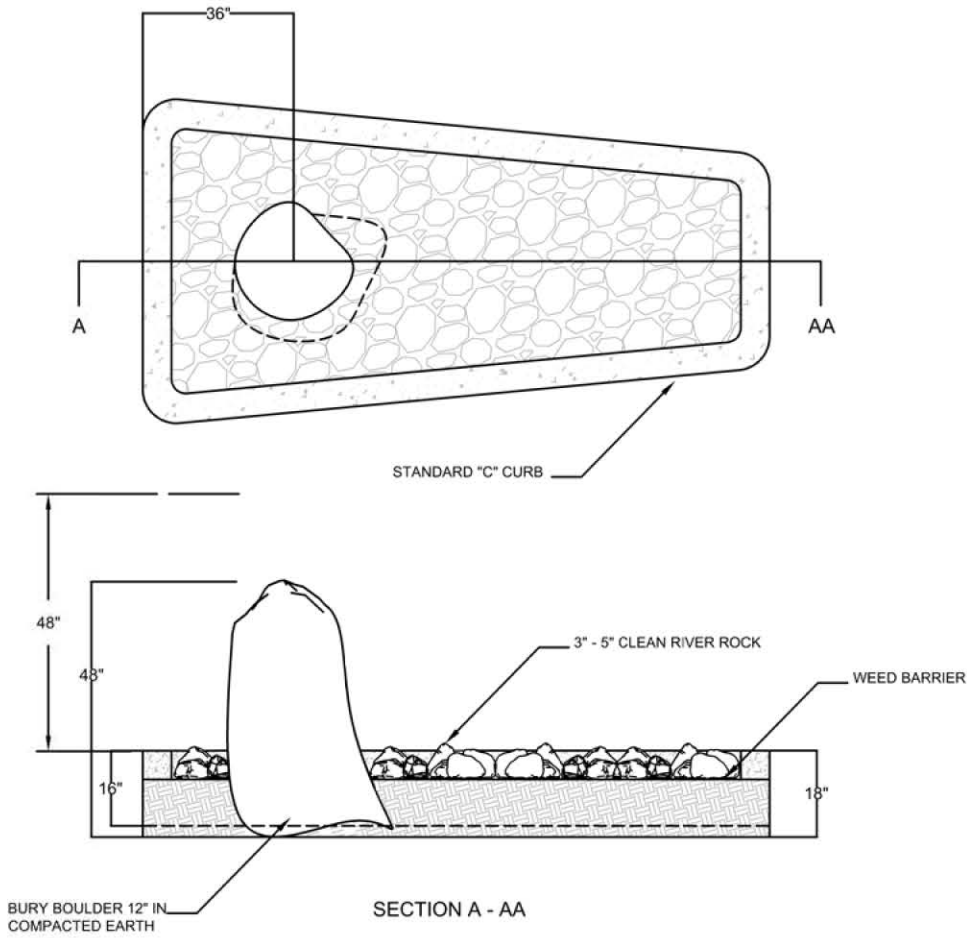
SIDEWALK & CURB



EXPIRES: 12/31/23

Date: Sheet No:
D2A
JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc - Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg



- NOTES:**
- BOULDERS SHALL HAVE ROUNDED EDGES.
 - INSTALL WEED CONTROL FABRIC BELOW RIVER ROCK.
 - 12" BURY ON BOULDER.



City of Newport
169 SW Coast Hwy
Newport, Oregon 97365
(541) 574-3366 Fax: (541) 265-3301

XERISCAPE ISLANDS

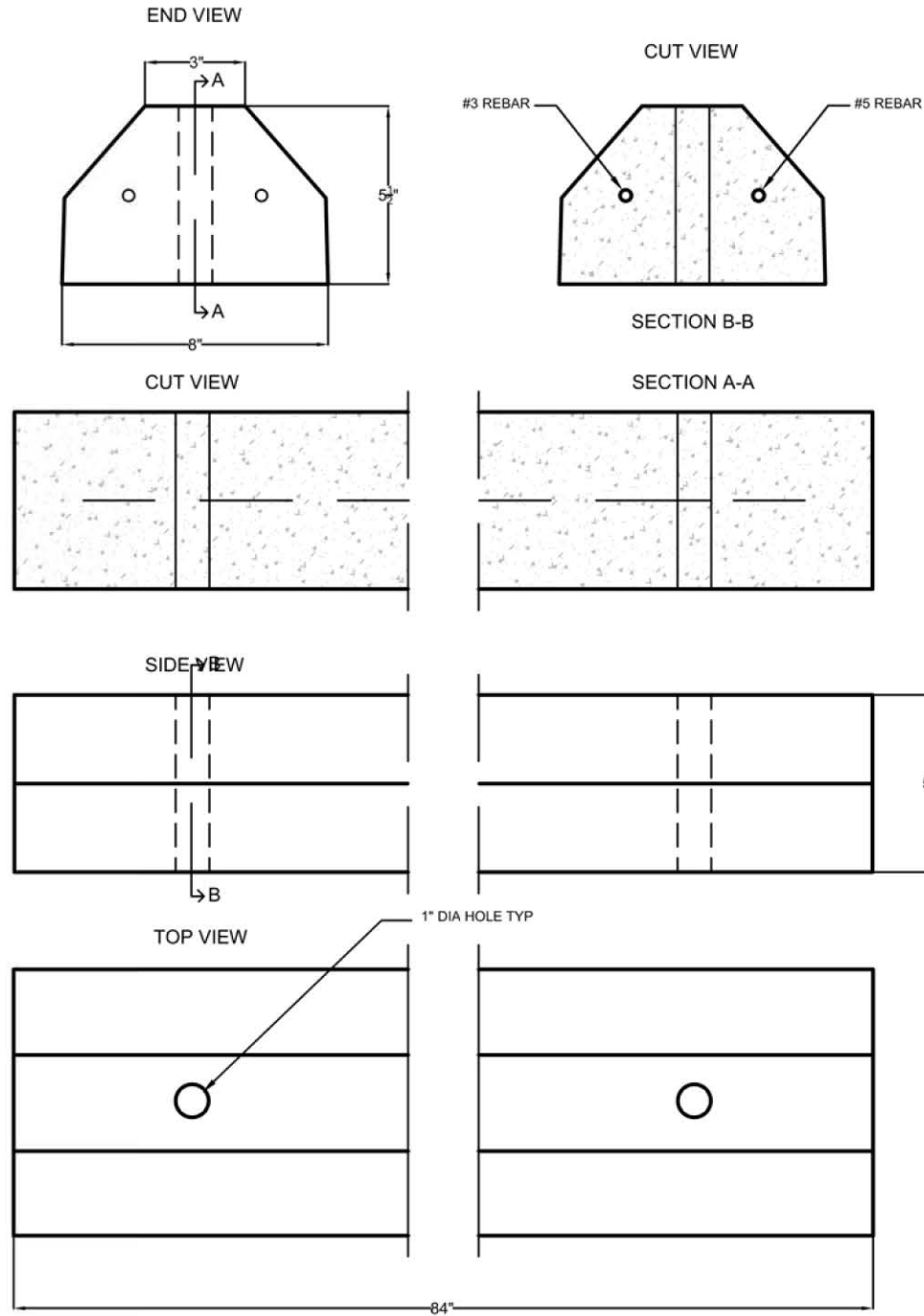
DETAIL NO.
T-502
6/13/2023



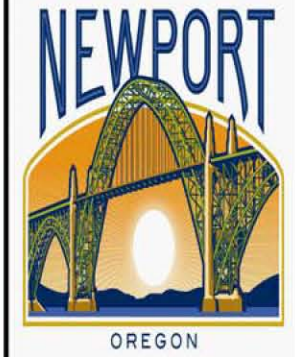
City of Newport
169 SW Coast Hwy
Newport, Oregon 97365
(541) 574-3366 Fax: (541) 265-3301

PARKING STOPS

DETAIL NO.
T-503
6/13/2023



EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

Designed By: CAB
Drawn By: MUR
Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON

DETAILS

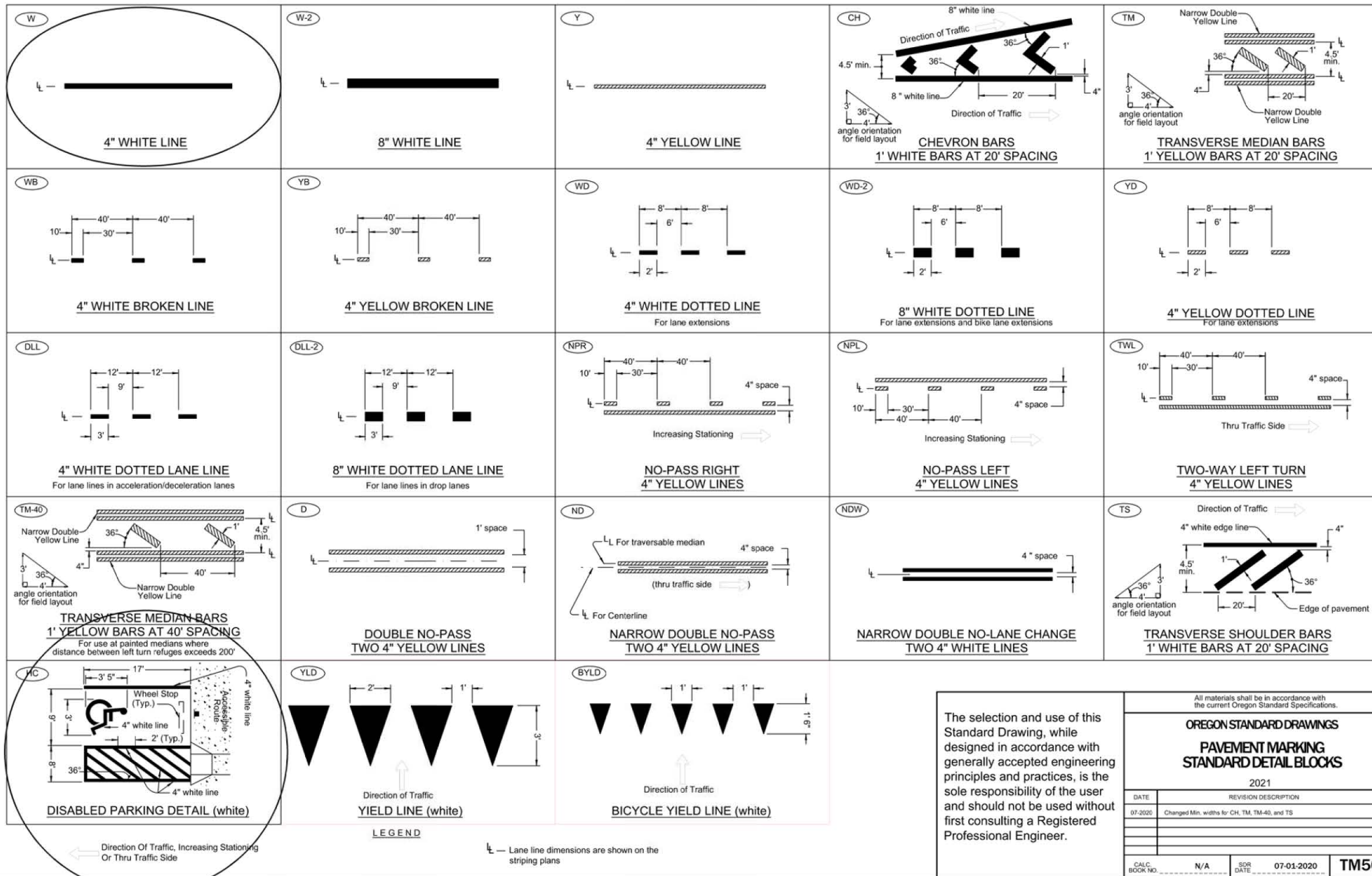
LANDSCAPE & PARKING STOP

D4

JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Bayfront Parking Lots.dwg

07-01-2020
TM500.dgn



All materials shall be in accordance with the current Oregon Standard Specifications.

OREGON STANDARD DRAWINGS
PAVEMENT MARKING
STANDARD DETAIL BLOCKS

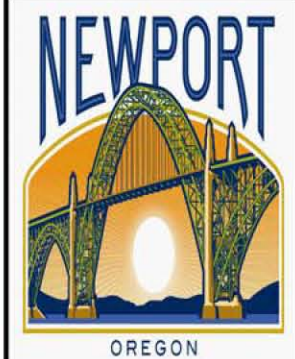
2021

DATE	REVISION DESCRIPTION
07-2022	Changed Min. widths for CH, TM, TM-40, and TS

CALC. BOOK NO.	N/A	SDR DATE	07-01-2020	TM500
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Effective Date: June 1, 2023 - November 30, 2023



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REV.	DATE	DESCRIPTION	BY

Designed By:	CAB	Checked By:	CAB
Drawn By:	MUR	Project No.:	2021-045

CITY OF NEWPORT, OREGON

DETAILS

STRIPING

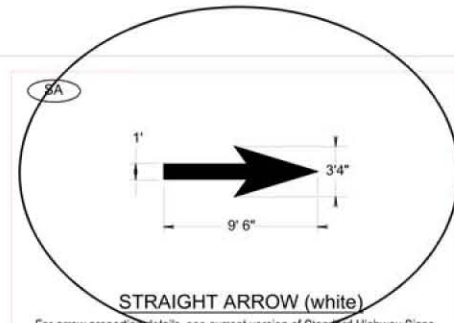


Date Sheet No. **D5**
JUNE 2023

DATE: 01/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Relubishment (2021-045)\Working\Bayfront Parking Lots.dwg

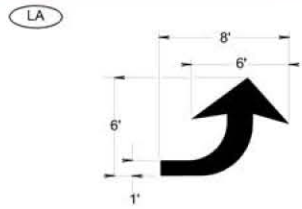
01-03-2022

TM501.dgn



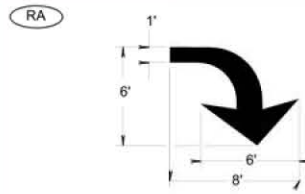
STRAIGHT ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



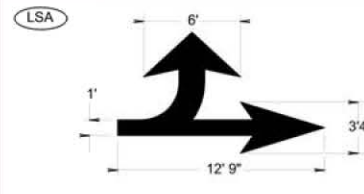
LEFT TURN ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



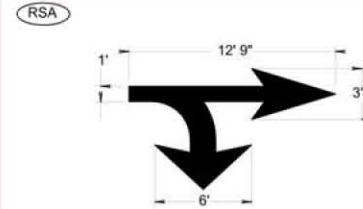
RIGHT TURN ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



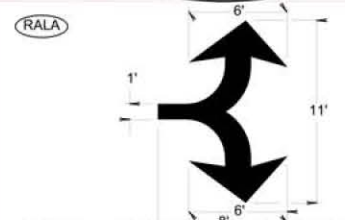
LEFT TURN STRAIGHT ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



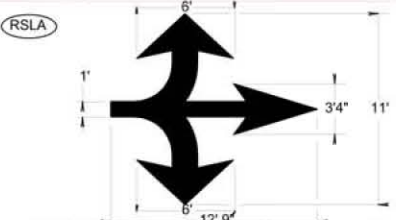
RIGHT TURN STRAIGHT ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



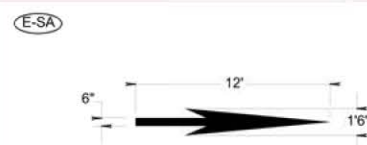
RIGHT TURN LEFT TURN ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



RIGHT TURN STRAIGHT LEFT TURN ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



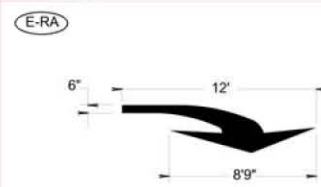
ELONGATED STRAIGHT ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



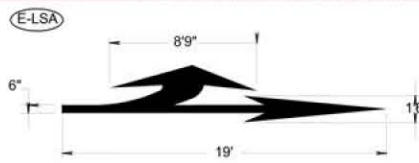
ELONGATED LEFT TURN ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



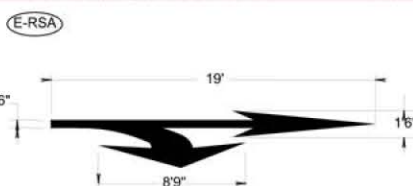
ELONGATED RIGHT TURN ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



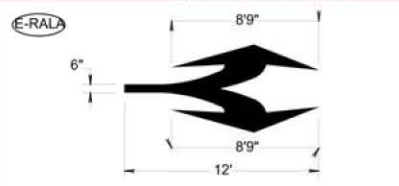
ELONGATED LEFT TURN STRAIGHT ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



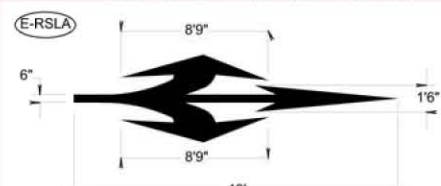
ELONGATED RIGHT TURN STRAIGHT ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



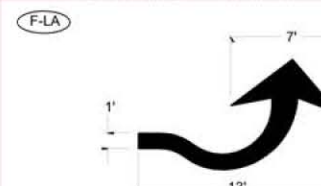
ELONGATED RIGHT TURN LEFT TURN ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



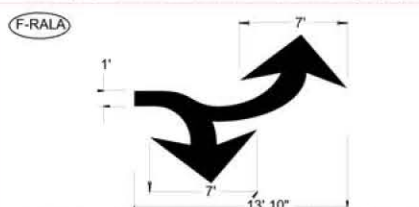
ELONGATED RIGHT TURN STRAIGHT LEFT TURN ARROW (white)

For arrow proportion details, see current version of Standard Highway Signs



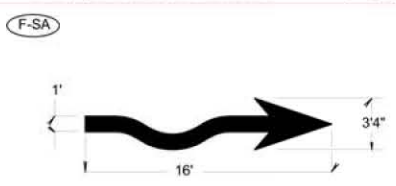
FISH-HOOK LEFT TURN ARROW (white)

For arrow proportion details, see the current ODOT Traffic Line Manual



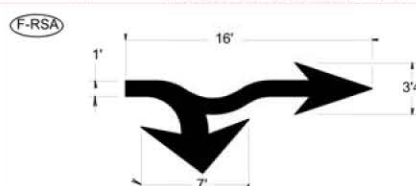
FISH-HOOK RIGHT TURN LEFT TURN ARROW (white)

For arrow proportion details, see the current ODOT Traffic Line Manual



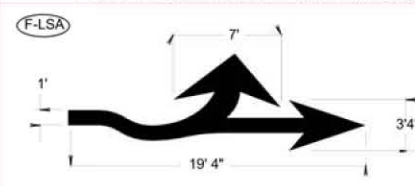
FISH-HOOK STRAIGHT ARROW (white)

For arrow proportion details, see the current ODOT Traffic Line Manual



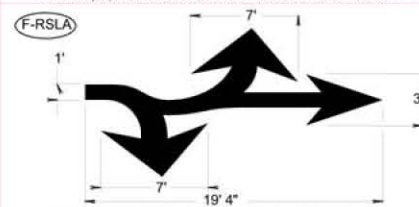
FISH-HOOK RIGHT TURN STRAIGHT ARROW (white)

For arrow proportion details, see the current ODOT Traffic Line Manual



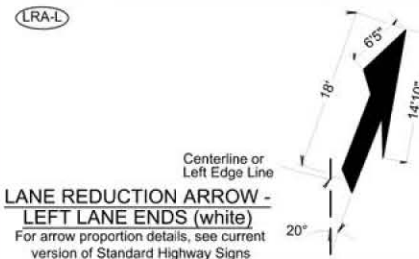
FISH-HOOK LEFT TURN STRAIGHT ARROW (white)

For arrow proportion details, see the current ODOT Traffic Line Manual



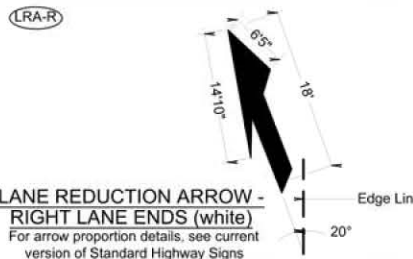
FISH-HOOK RIGHT TURN STRAIGHT LEFT TURN ARROW (white)

For arrow proportion details, see the current ODOT Traffic Line Manual



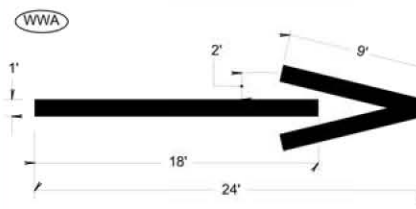
LANE REDUCTION ARROW - LEFT LANE ENDS (white)

For arrow proportion details, see current version of Standard Highway Signs



LANE REDUCTION ARROW - RIGHT LANE ENDS (white)

For arrow proportion details, see current version of Standard Highway Signs



WRONG-WAY ARROW (white)

- General Note:
- Center pavement markings within the lane width.
 - Arrow and letter dimensions nominal, excluding WWA.

All materials shall be in accordance with the current Oregon Standard Specifications.

OREGON STANDARD DRAWINGS

PAVEMENT MARKING

STANDARD DETAIL BLOCKS

2021

DATE	REVISION DESCRIPTION
07-2020	Some Detail Blocks moved to new Std. Drawing TM504
	Fish-hook Arrows added, LRA split into LRA-L and LRA-R
01-2022	Corrected bubble callout of LRA-L and typo in LRA-R

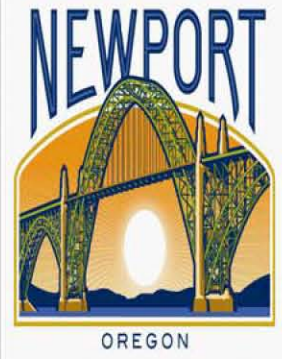
CALC. BOOK NO.	N/A	SOR DATE	01-03-2022	TM501
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Effective Date: June 1, 2023 - November 30, 2023



EXPIRES: 12/31/23

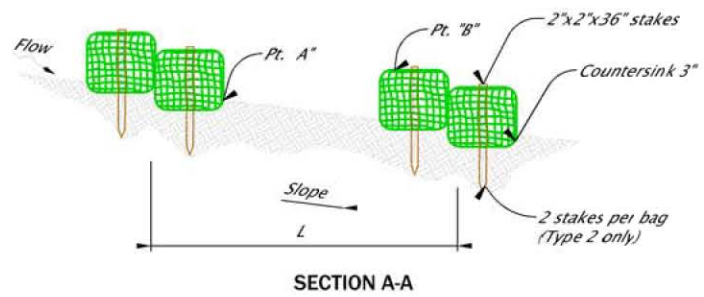
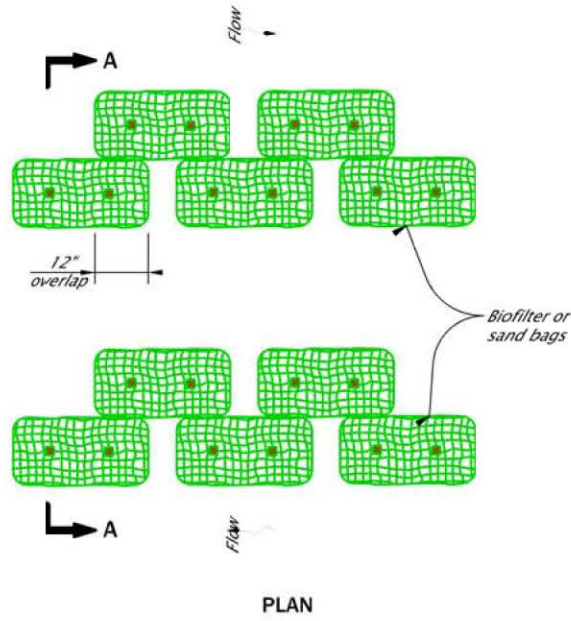


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REV.	DATE	DESCRIPTION	BY	CHECKED BY

Designed By: CAB
Drawn By: MUR
Checked By: CAB
Project No: 2021-045

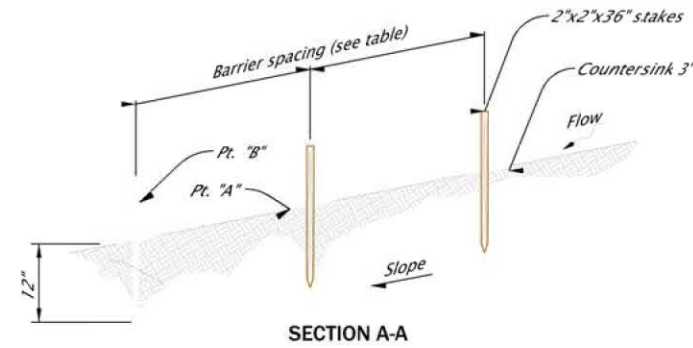
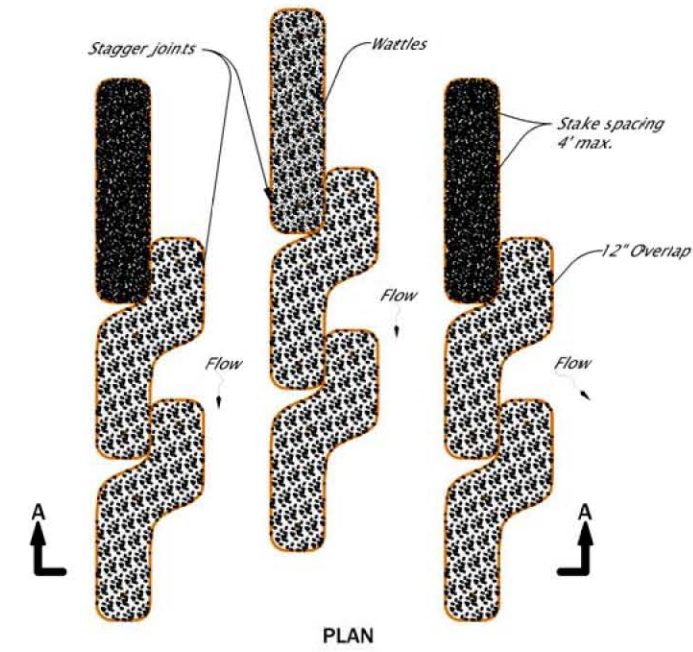
CITY OF NEWPORT, OREGON	DETAILS	STRIPING
	D6	
Date: Sheet No:	JUNE 2023	



BIOFILTER BAG / SAND BAG BARRIER - TYPE 2 AND 4
NOT TO SCALE

- NOTES:**
- For Type 2 barrier, drive stakes flush with top of bag and into undisturbed ground a min. of 12". Omit stakes if bags are placed on paved surface.
 - For Type 2 and Type 4 barriers, space bags (L) so that the elevation of point "A" is less than or equal to the elevation of point "B".
- Type 2 - Biofilter bags
Type 3 - Wattles
Type 4 - Sand bags

BARRIER SPACING		
INSTALL PARALLEL ALONG CONTOURS AS FOLLOWS		
% SLOPE	% SLOPE	MAXIMUM SPACING ON SLOPE
10% Flatter	1:10 or Flatter	300'
10 > % ≥ 15	10 > X ≥ 7.5	150'
15 > % ≥ 20	7.5 > X ≥ 5	100'
20 > % ≥ 30	5 > X ≥ 3	50'
Steeper than 30%	Steeper than 1:3	25'



FIBER ROLL BARRIER - TYPE 3
NOT TO SCALE

All materials shall be in accordance with the current Oregon Standard Specifications.

OREGON STANDARD DRAWINGS
SEDIMENT BARRIER
TYPE 2, 3 AND 4
2021

DATE	REVISION DESCRIPTION
01-2021	REMOVED CALC BOOK NUMBERS

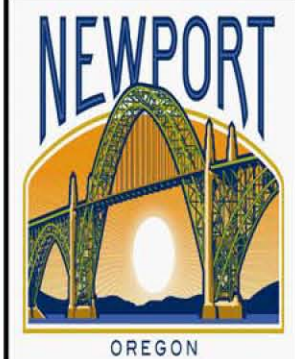
CALC BOOK NO.	N/A	SOR DATE	20-JAN-2021	RD1030
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The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without first consulting a Registered Professional Engineer.

Effective Date: June 1, 2023 - November 30, 2023



EXPIRES: 12/31/23



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Newport, Oregon 97365
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REV.	DATE	DESCRIPTION	BY	CHECKED BY

Designed By: CAB
Drawn By: MUR
Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON

DETAILS

EROSION CONTROL

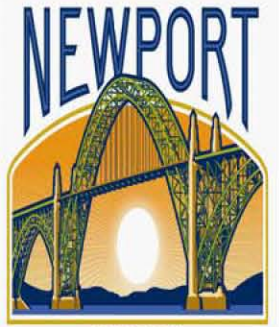
Date: Sheet No: **D8**

JUNE 2023

CITY OF NEWPORT, OREGON

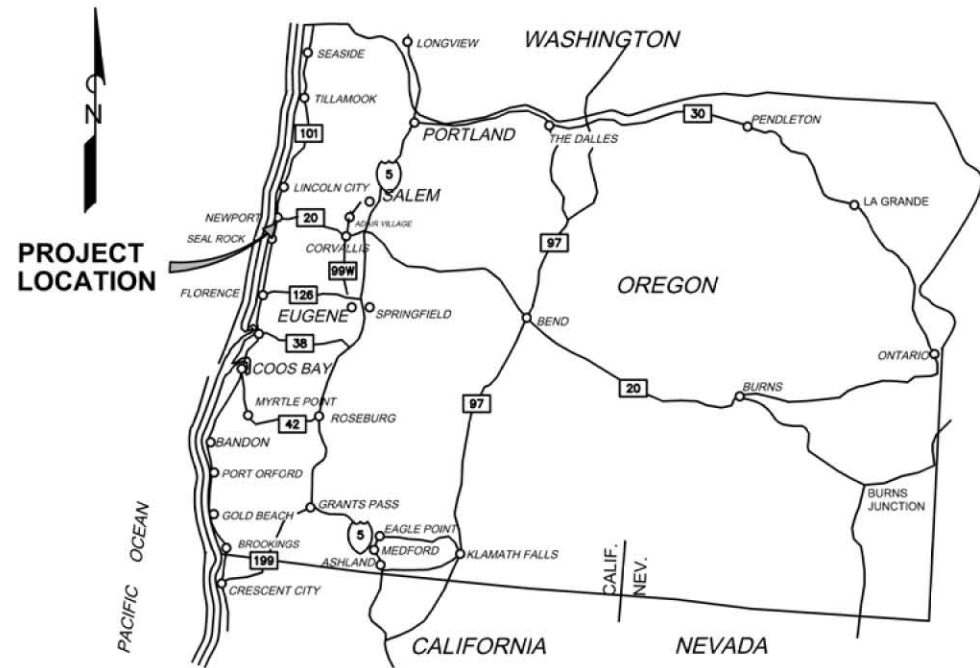
BAYFRONT PARKING IMPROVEMENTS

PROJECT NO. 2021-045

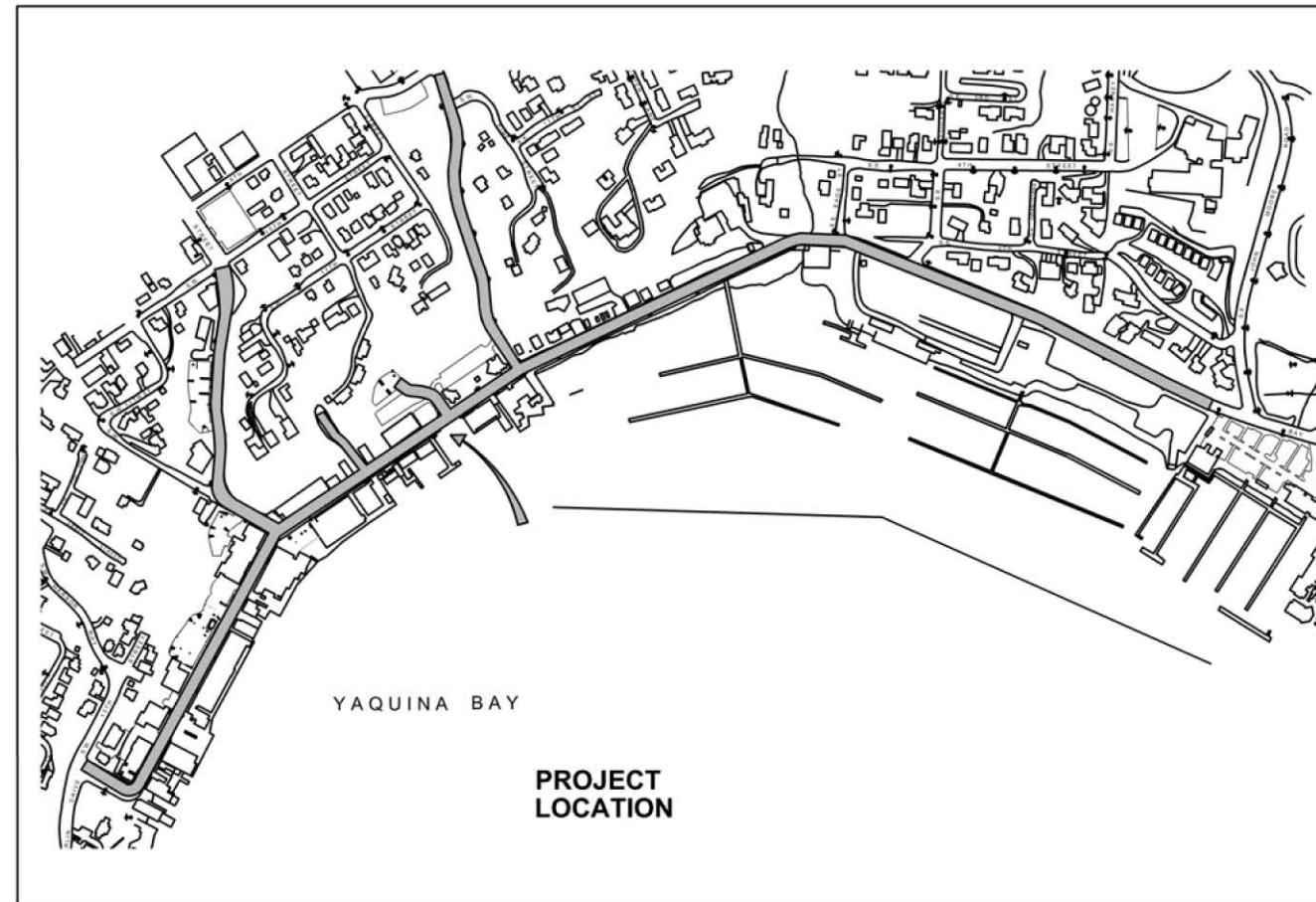


OREGON

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LOCATION MAP



VICINITY MAP

NOT TO SCALE



EXPIRES: 12/31/23

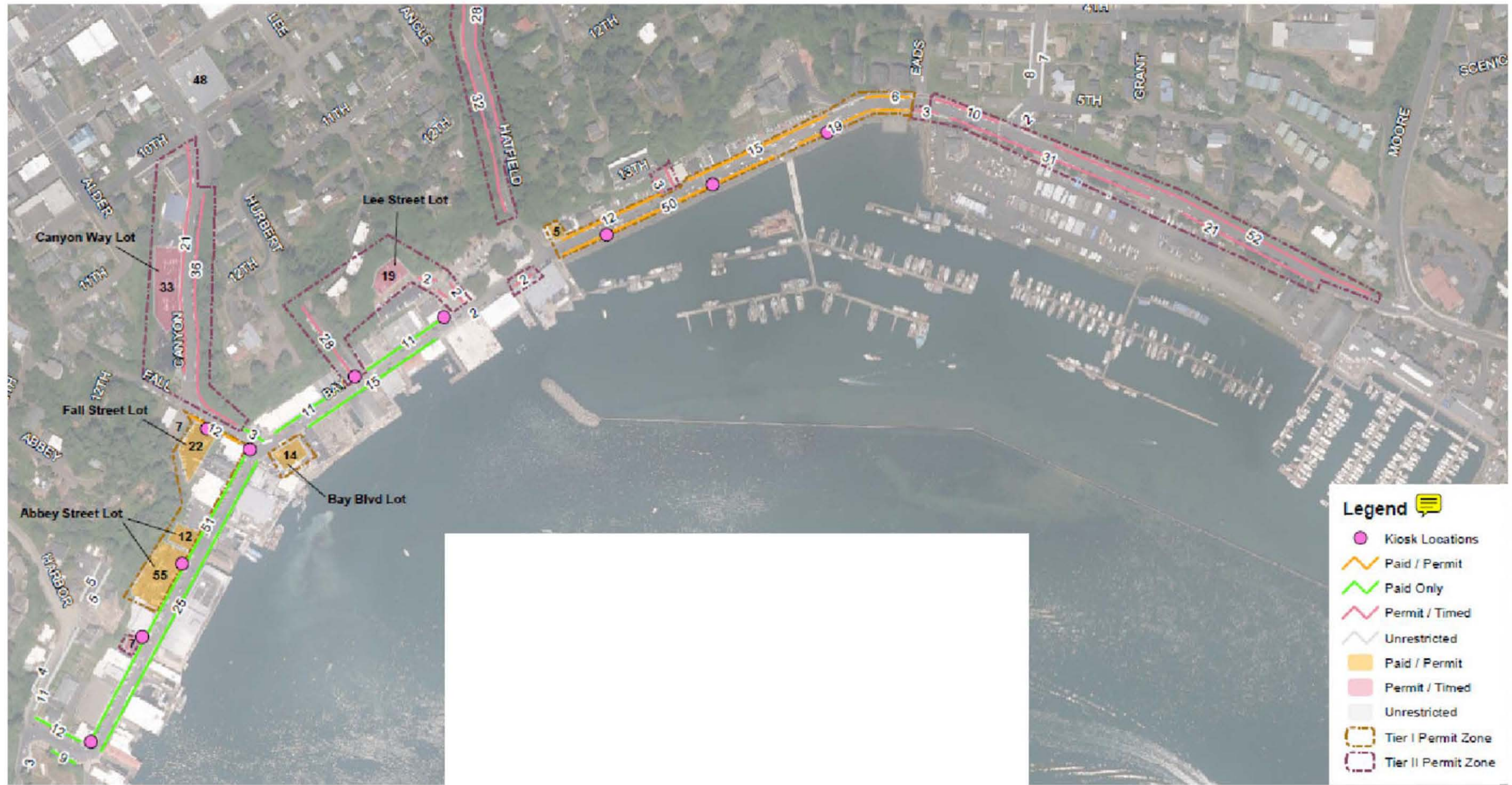
REV.	DATE	DESCRIPTION	BY

CITY OF NEWPORT, OREGON BAY BOULEVARD, NEWPORT, OR 97365	DESIGNED BY: CAB DRAWN BY: MUR CHECKED BY: CAB	PROJECT NO: 2021-045
BAYFRONT PARKING IMPROVEMENTS	COVER SHEET	

Date: Sheet No: **G1**
JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Parking Improvements.dwg

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Parking Improvements.dwg



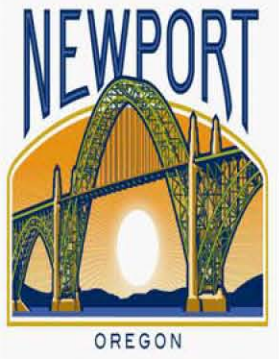
Legend

- Kiosk Locations
- ▬ Paid / Permit
- ▬ Paid Only
- ▬ Permit / Timed
- ▬ Unrestricted
- Paid / Permit
- Permit / Timed
- Unrestricted
- Tier I Permit Zone
- Tier II Permit Zone

REGISTERED PROFESSIONAL ENGINEER
19,138

OREGON
JULY 15, 1997
CHRIS A. BEATTY

EXPIRES: 12/31/23



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541-574-3376
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REV.	DATE	DESCRIPTION	BY

Designed By: CAB	Drawn By: MUR	Checked By: CAB
Project No: 2021-045		

CITY OF NEWPORT, OREGON	BAYFRONT PARKING IMPROVEMENTS	PROJECT AREA
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Date: Sheet No: **G3**
JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Relubishment (2021-045)\Working\Parking Improvements.dwg

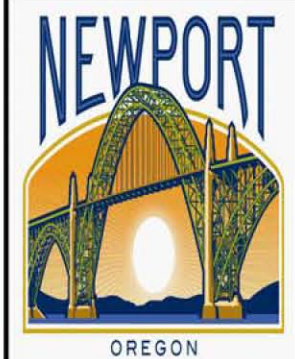


CONSTRUCTION NOTES

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <p>1 REMOVE & REPLACE SIGN POST; ATTACH EXISTING SIGN TO NEW POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS</p> <p>2 INSTALL SIGN POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS</p> <p>3 INSTALL KIOSK CONCRETE FOUNDATION; VERIFY LOCATION WITH CITY BEFORE INSTALL; SEE SHEET D2 FOR CONSTRUCTION DETAILS</p> | <p>4 YELLOW CURB REMOVED BY OTHERS</p> <p>5 LOADING ZONE MOVED BY OTHERS</p> <p>6 SIGN CLEANED BY OTHERS</p> <p>7 REMOVE POST; SIGN TO CITY</p> <p>8 IGNORE NOTE</p> |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------|



EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

Designed By: CAB Drawn By: MUR Checked By: CAB
Project No: 2021-045

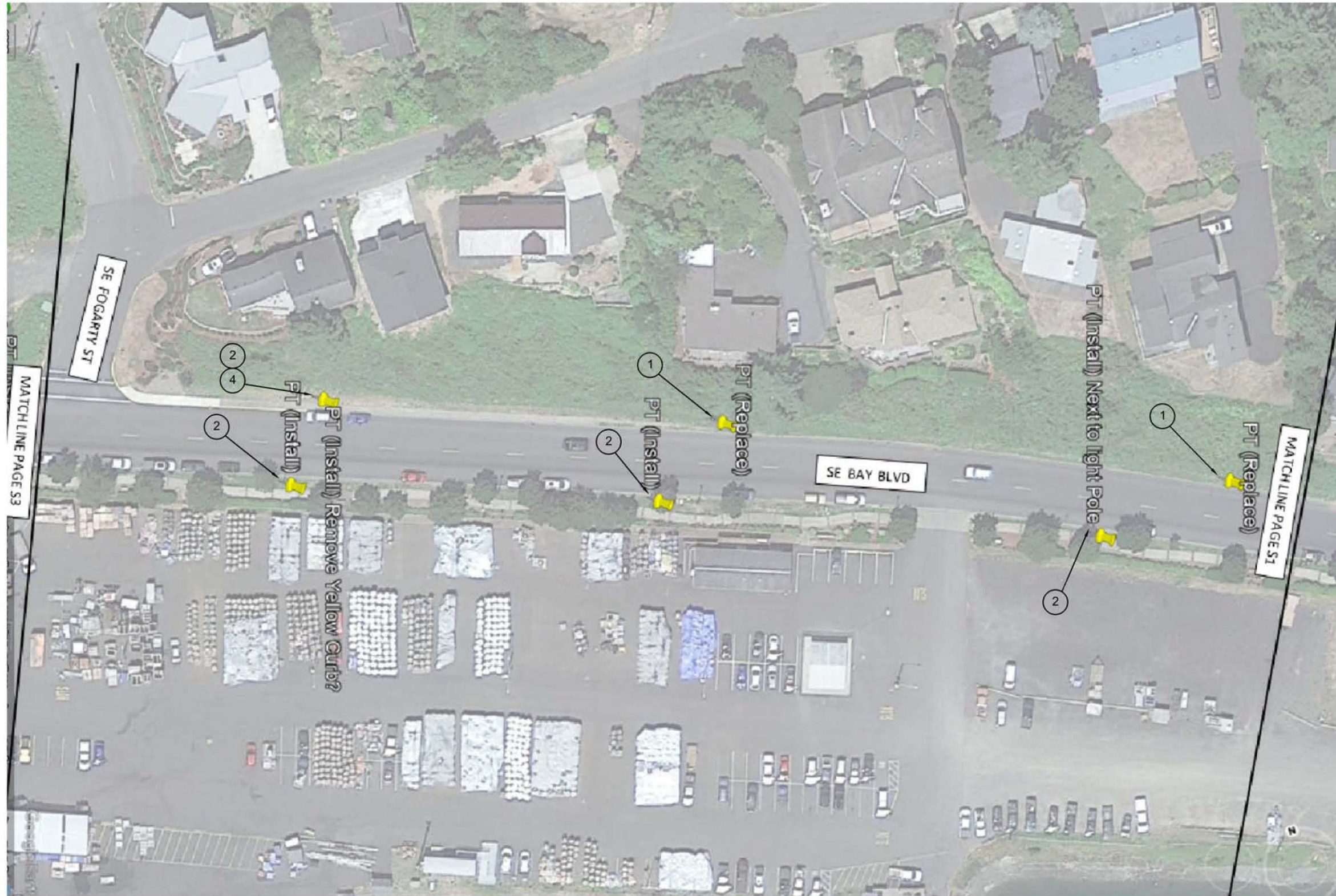
CITY OF NEWPORT, OREGON

BAYFRONT PARKING IMPROVEMENTS

SIGNAGE PLAN

Date: Sheet No: **S1**
JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Relubishment (2021-045)\Working\Parking Improvements.dwg



CONSTRUCTION NOTES

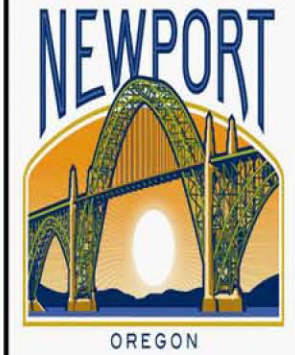
- 1 REMOVE & REPLACE SIGN POST; ATTACH EXISTING SIGN TO NEW POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS
- 2 INSTALL SIGN POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS
- 3 INSTALL KIOSK CONCRETE FOUNDATION; VERIFY LOCATION WITH CITY BEFORE INSTALL; SEE SHEET D2 FOR CONSTRUCTION

DETAILS

- 4 YELLOW CURB REMOVED BY OTHERS
- 5 LOADING ZONE MOVED BY OTHERS
- 6 SIGN CLEANED BY OTHERS
- 7 REMOVE POST; SIGN TO CITY
- 8 IGNORE NOTE



EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

Designed By: CAB Drawn By: MUR Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING IMPROVEMENTS
SIGNAGE PLAN

Sheet No: **S2**
Date: **JUNE 2023**



CONSTRUCTION NOTES

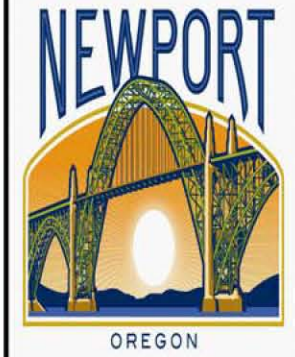
- 1 REMOVE & REPLACE SIGN POST; ATTACH EXISTING SIGN TO NEW POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS
- 2 INSTALL SIGN POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS
- 3 INSTALL KIOSK CONCRETE FOUNDATION; VERIFY LOCATION WITH CITY BEFORE INSTALL; SEE SHEET D2 FOR

CONSTRUCTION DETAILS

- 4 YELLOW CURB REMOVED BY OTHERS
- 5 LOADING ZONE MOVED BY OTHERS
- 6 SIGN CLEANED BY OTHERS
- 7 REMOVE POST; SIGN TO CITY
- 8 IGNORE NOTE



EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

Designed By: CAB	Drawn By: MUR	Checked By: CAB
Project No: 2021-045		

CITY OF NEWPORT, OREGON

BAYFRONT PARKING IMPROVEMENTS

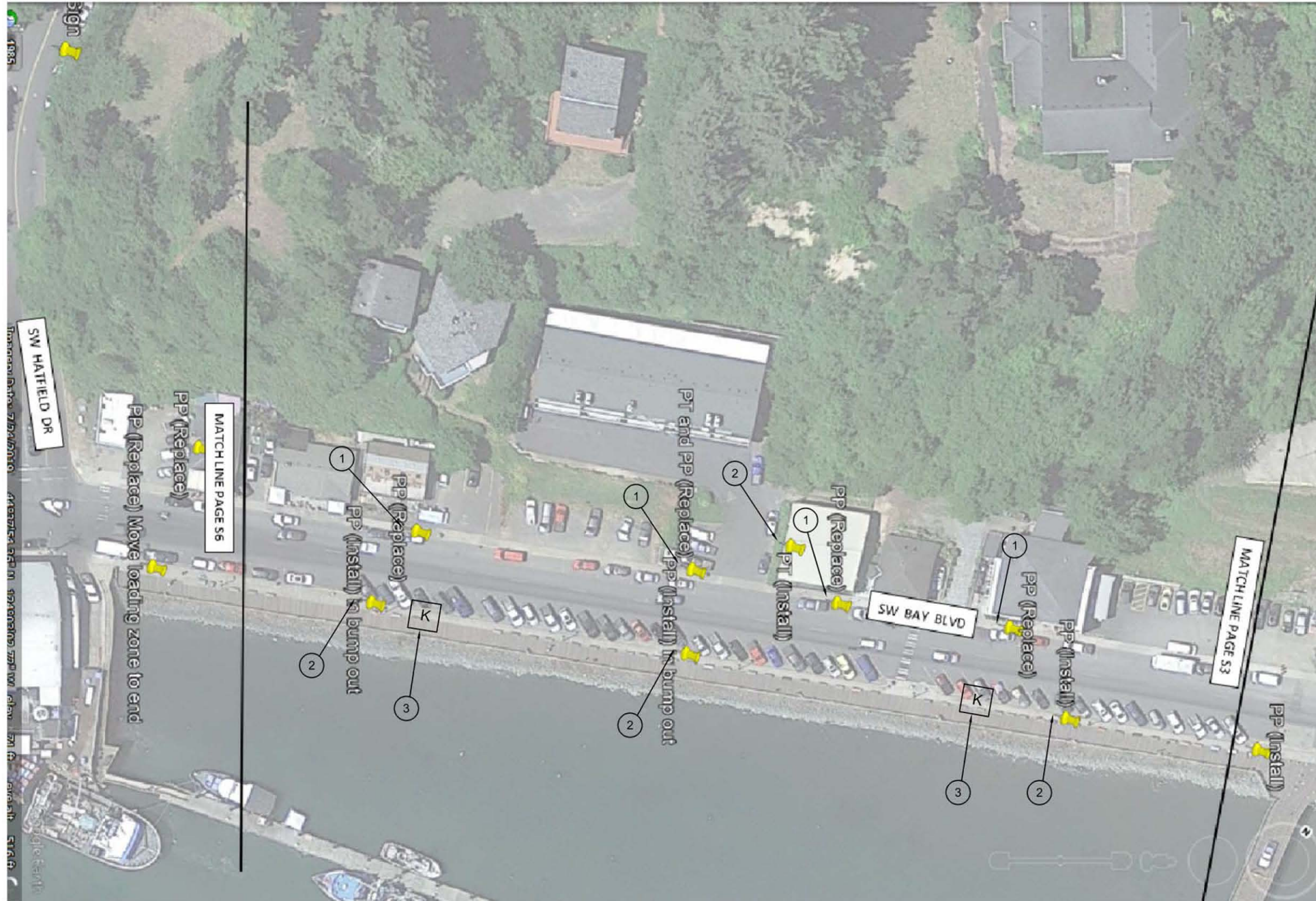
SIGNAGE PLAN

Date: Sheet No: **S3**

JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Relubishment (2021-045)\Working\Parking Improvements.dwg

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Relubishment (2021-045)\Working\Parking Improvements.dwg



CONSTRUCTION NOTES

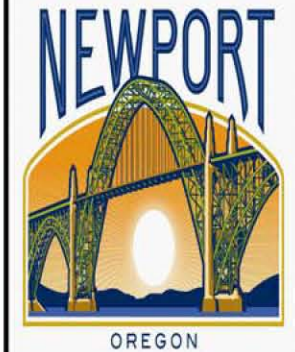
- 1 REMOVE & REPLACE SIGN POST; ATTACH EXISTING SIGN TO NEW POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS
- 2 INSTALL SIGN POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS
- 3 INSTALL KIOSK CONCRETE FOUNDATION; VERIFY LOCATION WITH CITY BEFORE

INSTALL; SEE SHEET D2 FOR CONSTRUCTION DETAILS

- 4 YELLOW CURB REMOVED BY OTHERS
- 5 LOADING ZONE MOVED BY OTHERS
- 6 SIGN CLEANED BY OTHERS
- 7 REMOVE POST; SIGN TO CITY
- 8 IGNORE NOTE



EXPIRES: 12/31/23



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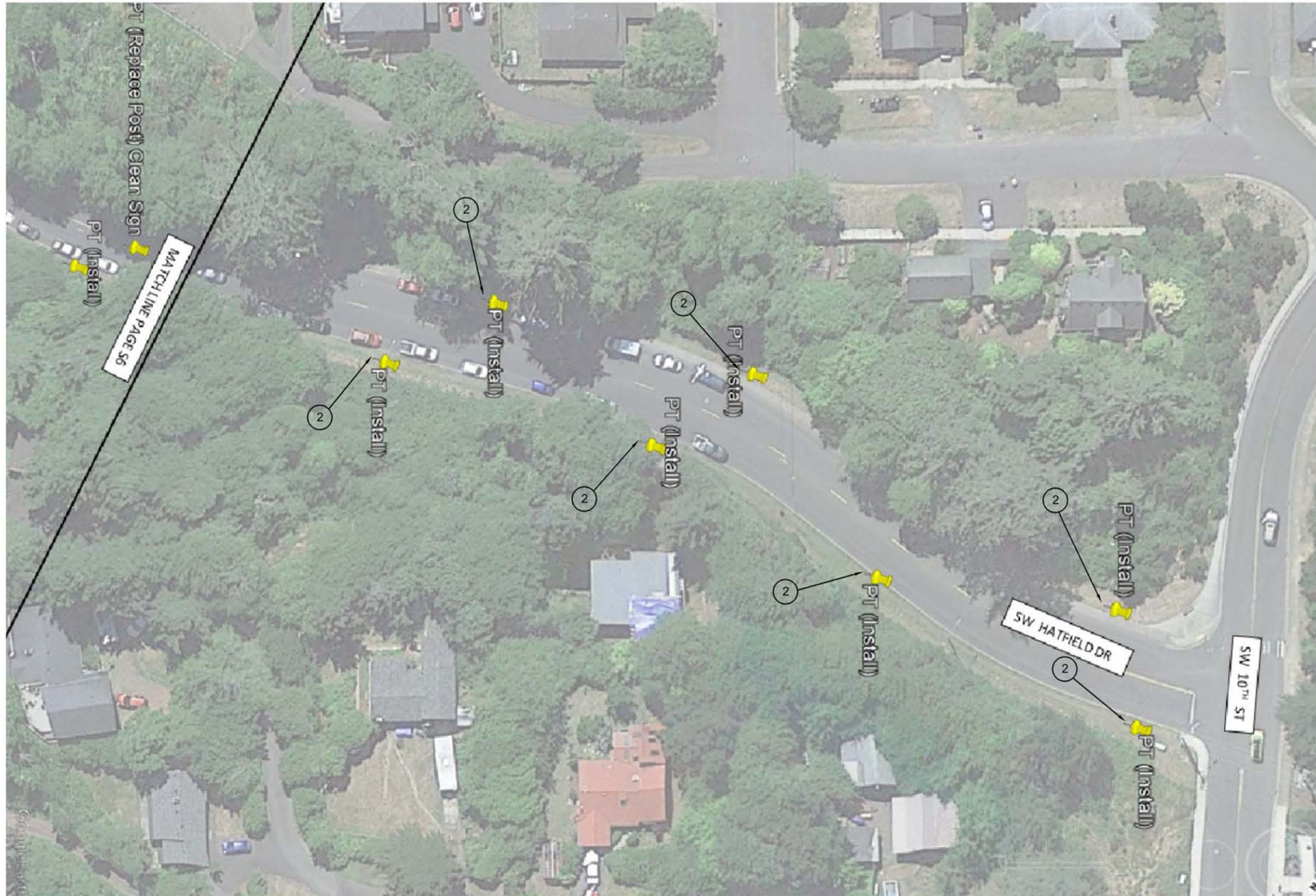
REV.	DATE	DESCRIPTION	BY

Designed By: CAB Drawn By: MUR Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING IMPROVEMENTS
SIGNAGE PLAN

Sheet No: **S4**
Date: **JUNE 2023**

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Parking Improvements.dwg



CONSTRUCTION NOTES

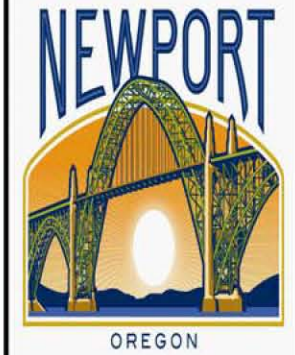
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- 2 INSTALL SIGN POST; SEE DRAWING T-410 SHEET D3 FOR CONSTRUCTION DETAILS
- 3 IINSTALL KIOSK CONCRETE FOUNDATION; VERIFY LOCATION WITH CITY BEFORE INSTALL; SEE SHEET D2 FOR CONSTRUCTION

DETAILS

- 4 YELLOW CURB REMOVED BY OTHERS
- 5 LOADING ZONE MOVED BY OTHERS
- 6 SIGN CLEANED BY OTHERS
- 7 REMOVE POST; SIGN TO CITY
- 8 IGNORE NOTE



EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

Designed By: CAB Drawn By: MUR Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON
BAYFRONT PARKING IMPROVEMENTS
SIGNAGE PLAN

Sheet No: **S5**
Date: **JUNE 2023**

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc - Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Parking Improvements.dwg

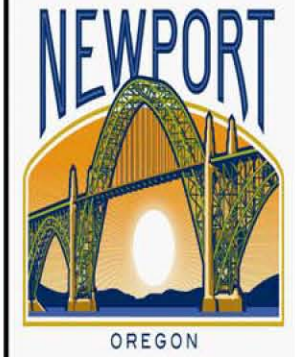


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EXPIRES: 12/31/23



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Designed By: CAB	Drawn By: MUR	Checked By: CAB
Project No: 2021-045		

CITY OF NEWPORT, OREGON

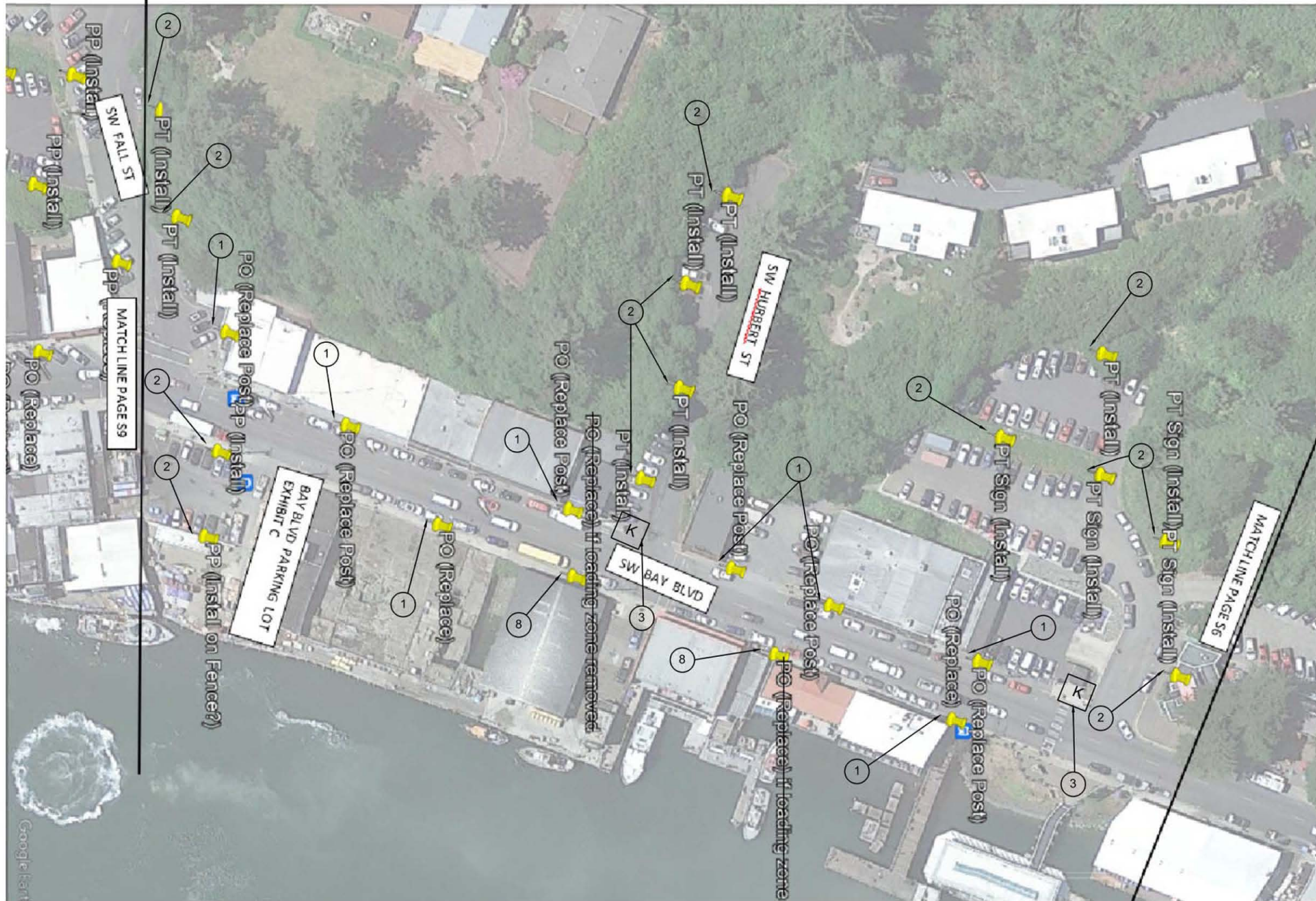
BAYFRONT PARKING IMPROVEMENTS

SIGNAGE PLAN

Sheet No: **S6**

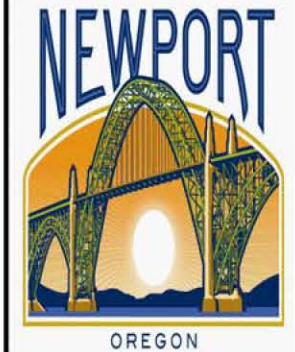
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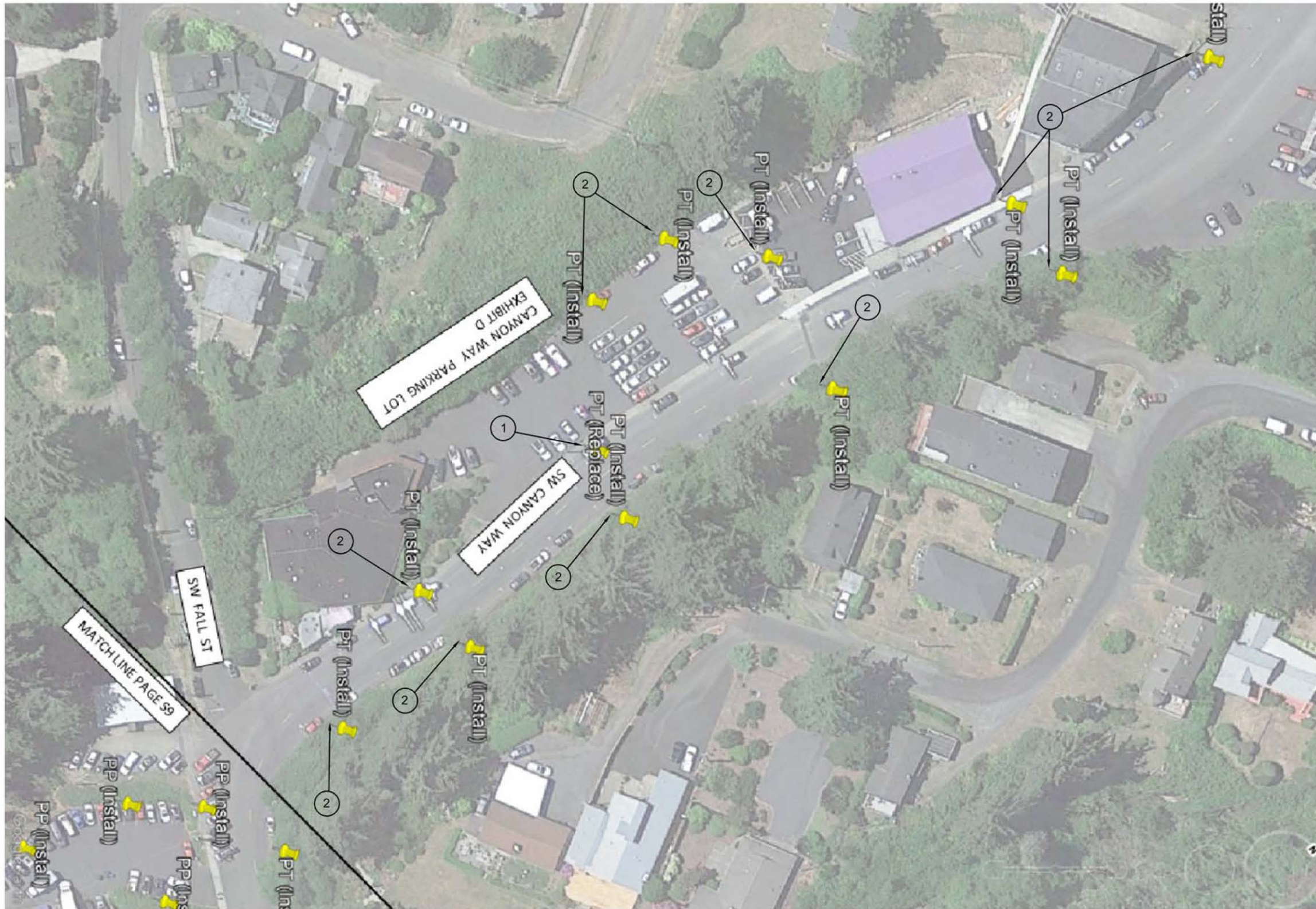
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REV.	DATE	DESCRIPTION	BY

Designed By: CAB Drawn By: MUR Checked By: CAB
Project No: 2021-045

CITY OF NEWPORT, OREGON	BAYFRONT PARKING IMPROVEMENTS	SIGNAGE PLAN
Date: Sheet No:	S7	JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Parking Improvements.dwg

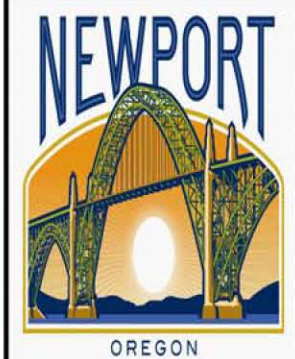


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EXPIRES: 12/31/23



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REV.	DATE	DESCRIPTION	BY

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Project No: 2021-045

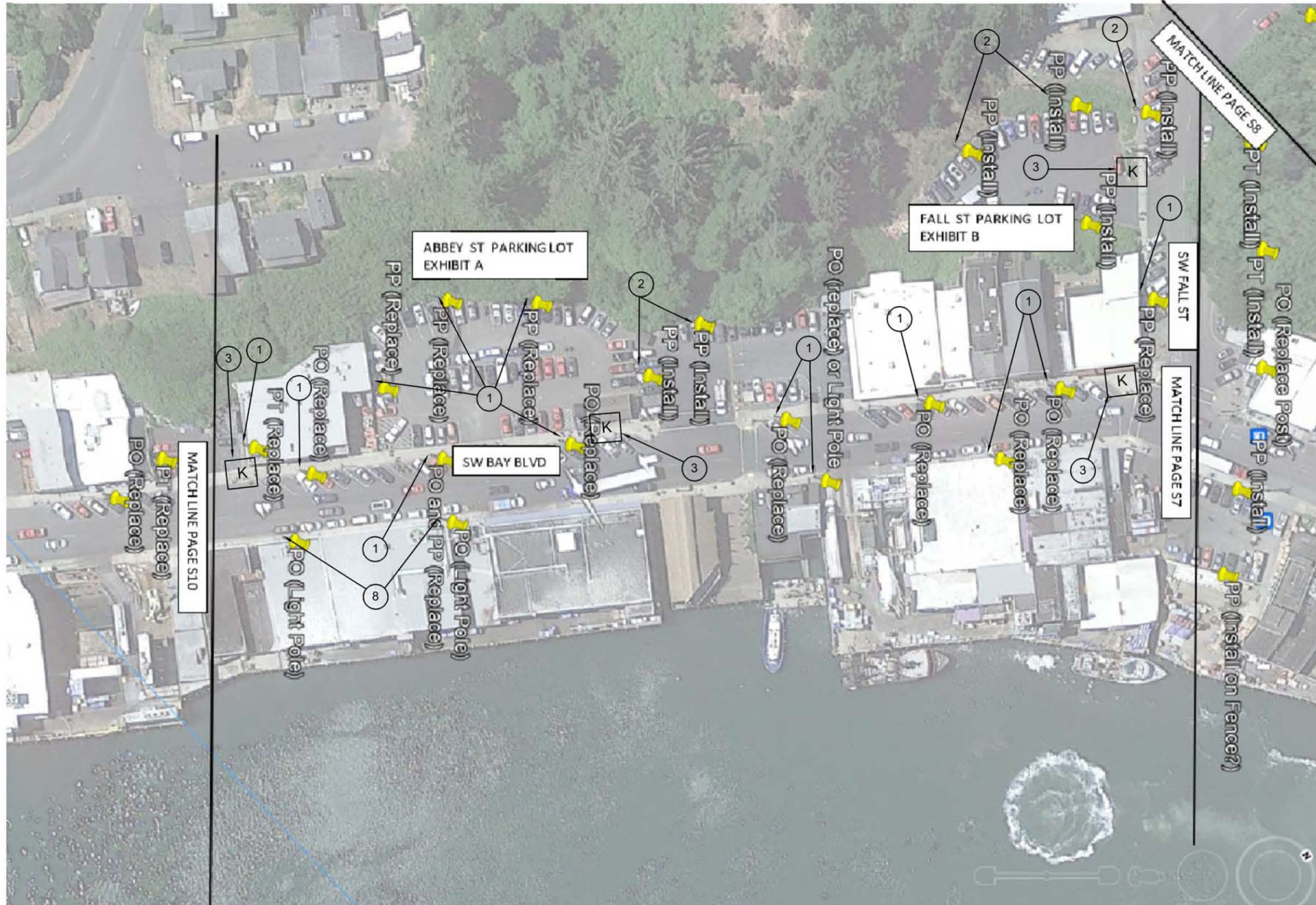
CITY OF NEWPORT, OREGON

BAYFRONT PARKING IMPROVEMENTS

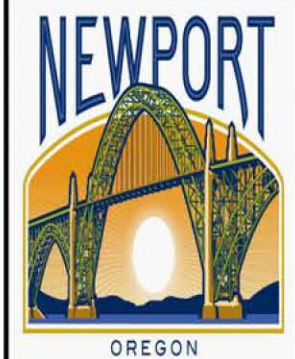
SIGNAGE PLAN

Sheet No: **S8**
Date: **JUNE 2023**

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Project No: 2021-045

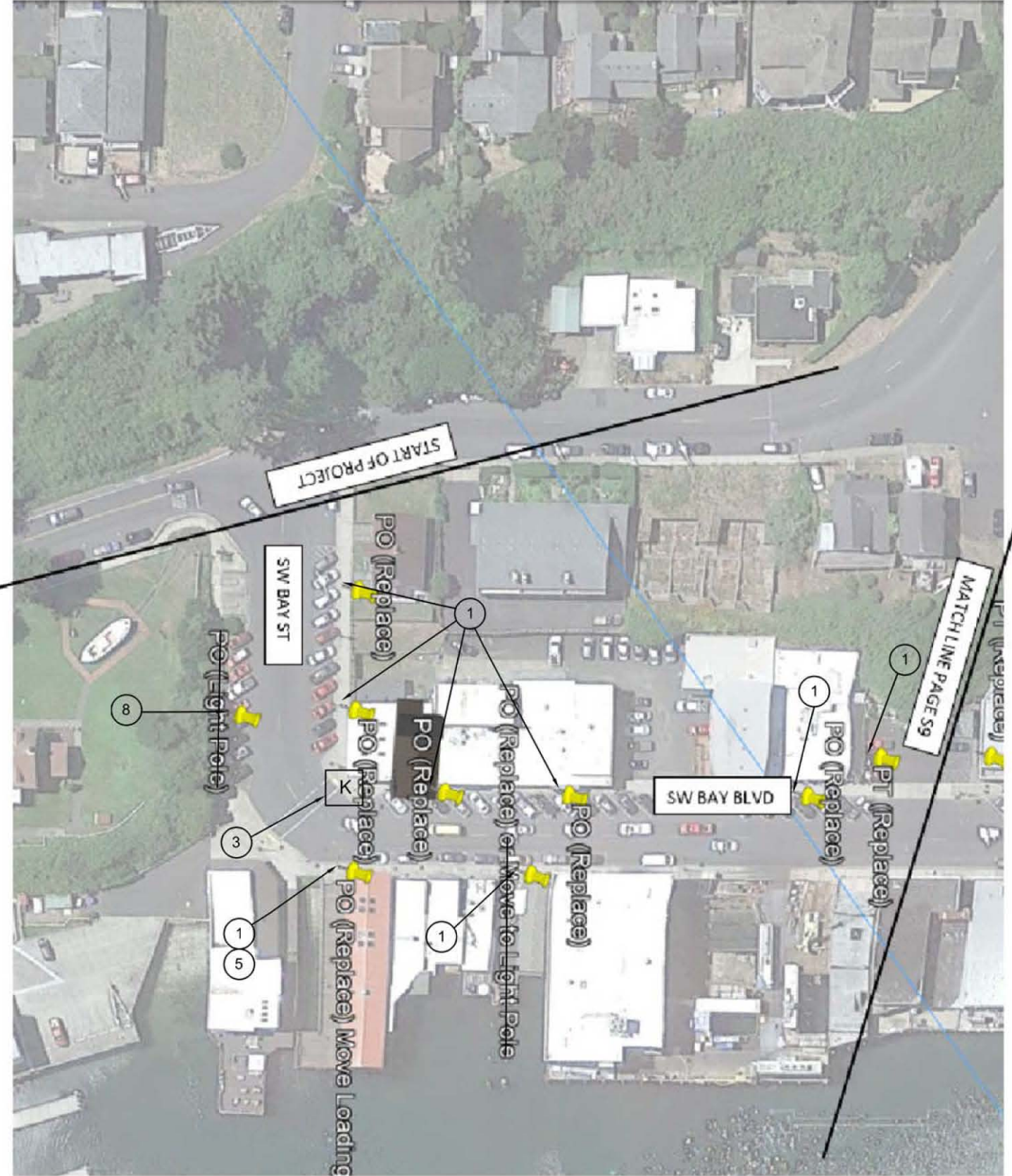
CITY OF NEWPORT, OREGON

BAYFRONT PARKING IMPROVEMENTS

SIGNAGE PLAN

Date: Sheet No: **S9**
JUNE 2023

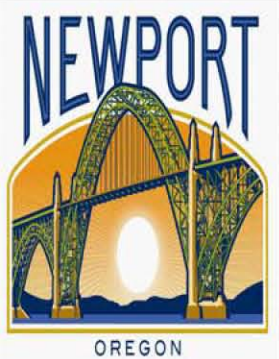
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CONSTRUCTION NOTES

- 1 REMOVE & REPLACE SIGN POST; ATTACH EXISTING SIGN TO NEW POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS
- 2 INSTALL SIGN POST; SEE DRAWING T-450 SHEET D1 FOR CONSTRUCTION DETAILS
- 3 INSTALL KIOSK CONCRETE FOUNDATION;

- VERIFY LOCATION WITH CITY BEFORE INSTALL;
SEE SHEET D2 FOR CONSTRUCTION DETAILS
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REV.	DATE	DESCRIPTION	BY

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Project No: 2021-045

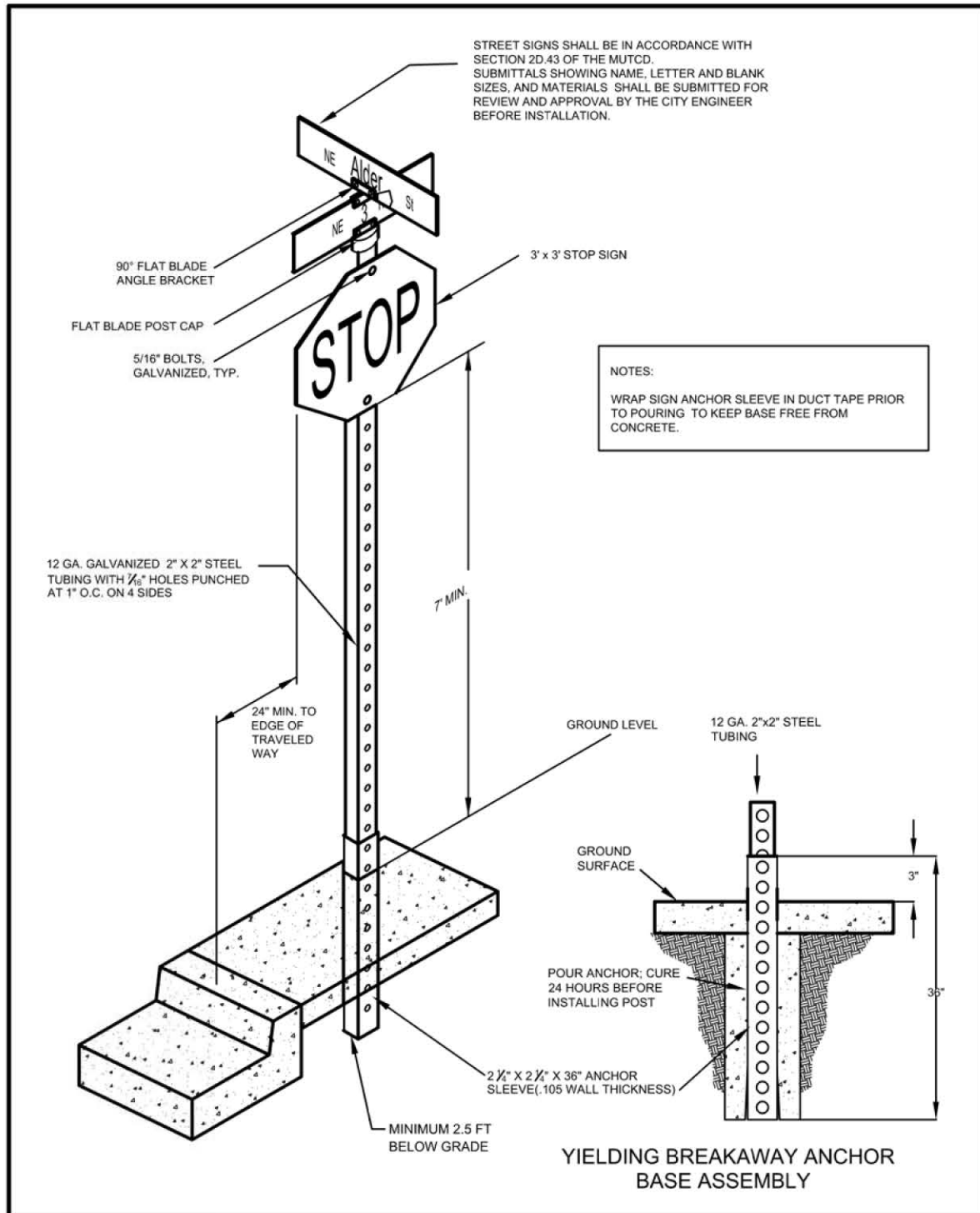
CITY OF NEWPORT, OREGON

BAYFRONT PARKING IMPROVEMENTS

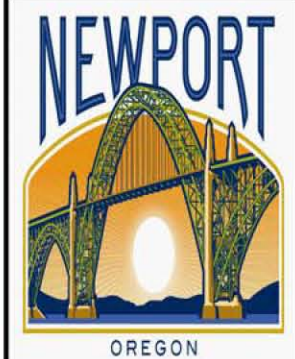
SIGNAGE PLAN

Date: Sheet No: **S10**
JUNE 2023

DATE: 6/15/23 FILE: Y:\p\k\Engineering\Projects\Misc. Small Projects\Planning - Parking Lot Refurbishment (2021-045)\Working\Parking Improvements.dwg



 <p>City of Newport 169 SW Coast Hwy Newport, Oregon 97365 (541) 574-3366 Fax: (541) 265-3301</p>	<p>STANDARD SIGN DETAIL FOR LOCAL STREETS</p>	DETAIL NO. T-450
		5/12/2020



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REV.	DATE	DESCRIPTION	BY

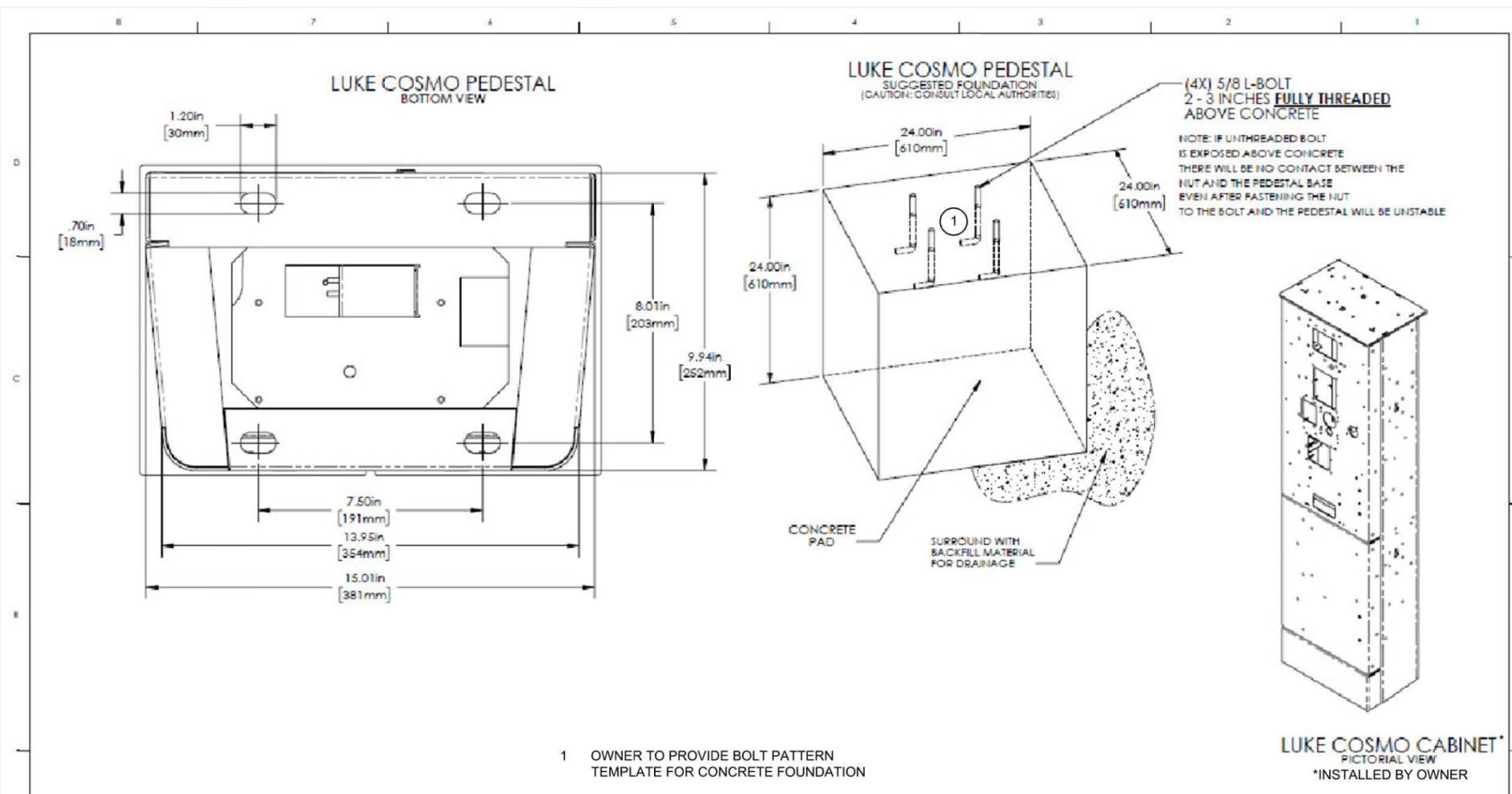
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 Project No: 2021-045

CITY OF NEWPORT, OREGON	DETAILS	T-450
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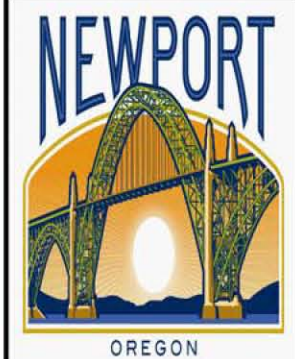
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1 OWNER TO PROVIDE BOLT PATTERN TEMPLATE FOR CONCRETE FOUNDATION

REVISIONS					
REV.	DESCRIPTION	BY	CHK'D	APPR'D	DATE (YYYY-MM-DD)
A	INITIAL RELEASE	SP			2018-05-01

<small>UNLESS OTHERWISE SPECIFIED DIMENSIONS ARE IN INCHES</small> <small>LINEAR TOLERANCE</small> X .000 Y .000 Z .000 <small>ANGULAR TOLERANCE</small> X .1° Y .1° Z .1°		T2 T2 SYSTEMS CANADA INC. 4321 STILL CREEK DR., SUITE 300, BURNABY, BC V5C 6S7 TEL: (888) 487-4122 www.t2systems.com
<small>UNDIMENSIONED FEATURES PER DIGITAL DATA</small> <small>THRU ANGLE PROJECTION</small> 		THE INFORMATION CONTAINED IN THIS DRAWING IS THE SOLE PROPERTY OF T2. ANY REPRODUCTION IN PART OR WHOLE WITHOUT THE WRITTEN PERMISSION OF T2 IS PROHIBITED.
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<small>PART NO.</small> 980.0173		<small>REV.</small> A



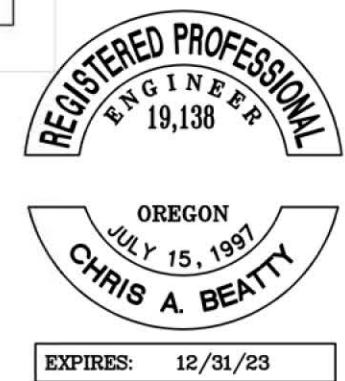
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BY		Checked By:	CAB
DESCRIPTION		Drawn By:	MJR
DATE		Designed By:	CAB
REV.		Project No:	2021-045

CITY OF NEWPORT, OREGON

DETAILS

KIOSK



D2

JUNE 2023

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CHAPTER 1.50 PENALTY

1.50.010 Default Penalty

Except as otherwise specified, the penalty for violation of any provision of this code or other ordinance shall be a civil penalty of \$500. If the violation is of a code provision or ordinance that is identical to a state statute, the city may elect to proceed on the basis of the state violation or the city violation. Each calendar day on which violation occurs or remains uncorrected constitutes a separate violation.

1.50.020 Traffic Citation Assessment and Vehicle Impound Fee

- A. Except as provided in Subsection B. of this section, all persons who are issued a traffic citation to appear in the Newport Municipal Court shall pay a traffic citation assessment fee ~~of \$10.00 per citation~~ as specified by resolution of the City Council. All persons who recover a vehicle from a vehicle impoundment shall pay a vehicle impound assessment fee ~~of \$10.00 per~~ to partially off-set City's cost of impoundment ~~impounding the vehicle. These amounts~~ The vehicle impound assessment fee shall be in an amount set by City Council resolution, and shall be ~~is~~ in addition to any other penalty, assessments, or payment.
- B. If the municipal court determines that the person issued the citation did not commit the offense or has established an affirmative defense, no traffic citation assessment fee or vehicle impound assessment fee shall be imposed.
- C. The amount of the traffic citation assessment fee and vehicle impound assessment fee shall be added to any bail amount for those who do not contest the citation and shall be included as part of the judgment for all those who contest the citation and are determined to have committed the offense.
- D. Proceeds from payment of ~~the parking~~ traffic citation assessment and vehicle impound assessment fees shall be ~~used for the police department's cost associated used for parking enforcement, parking improvements and/or enhancements, and related purposes, with maintaining traffic safety.~~ Proceeds from payment of other traffic citation assessment and vehicle impound assessment fees shall be used to offset police department costs associated with maintaining traffic safety.

Staff: The City is now setting its traffic citation fees by resolution and this chapter of the Municipal Code needs to be amended to align with that practice. Parking related traffic citation fees will be directed to the Parking Fund.

(Unless otherwise specified, new language is shown in double underline, and text to be removed is depicted with ~~strikethrough~~. Staff comments, in *italics*, are for context and are not a part of the revisions.)

CHAPTER 6.15 PARKING IN RIGHTS OF WAY AND CITY OWNED PARKING LOTS

6.15.005 Method of Parking

- A. Parking is permitted only parallel with the edge of the street, headed in the direction of lawful traffic movement, except where the street is marked or signed for angle parking. Where parking spaces are marked, vehicles shall be parked within the marked spaces. Parking in angled spaces shall be with the front head-in to the curb, except that vehicles delivering or picking up goods may be backed in. Where curbs exist the wheels of a parallel-parked car shall be within 12 inches of the curb, and the front of an angle-parked car shall be within 6 inches of the curb.
- B. If possible, parked cars shall be removed by their owners in the event of an emergency such as a fire.

6.15.010 Parking of Oversized Vehicles

Any vehicle which, because of its size or shape, cannot be parked as provided by [Section 6.15.005](#) may be parked outside the restricted or limited parking area of the city in a manner which will not impede or interfere with vehicular traffic. No vehicle may be parked to impede or interfere with a vehicle travel lane.

6.15.015 Prohibited Parking

- A. No person shall park a vehicle:
 - 1. On a bridge, viaduct or other elevated structure used as a street, unless permitted by authorized signs.
 - 2. Obstructing a street so as to prevent or interfere with orderly two-way traffic.
 - 3. In any alley except to load or unload persons or materials not to exceed 30 minutes, and then only in such a manner as to leave available space for another vehicle to pass the parked vehicle;

4. On a street for the principal purpose of:
 - a. Displaying the vehicle for sale.
 - b. Greasing or repairing the vehicle, except repairs necessitated by an emergency.
 - c. Displaying a sign from the vehicle.
 - d. Selling merchandise from the vehicle except in a duly established market place or when so authorized or licensed under the ordinances of the city.
 - e. Storage in a street right-of-way for more than 72 consecutive hours. Storage includes any parking in excess of 72 consecutive hours.

5. In a manner contrary to any sign posted regulating parking in a city-owned parking lot, and in no circumstances for more than 16 hours in a city-owned parking lot unless authorized for camping, pursuant to Chapter 9.50 or as a Special Event pursuant to Chapter 9.80.

56. And leave the vehicle without stopping the engine and effectively setting the brake. Police officers are authorized to turn off any vehicles left running and unattended and remove the key. The officer shall leave information as to how to claim the key.

67. In a location or at times where parking is prohibited as indicated by authorized signs or curb markings.

B. No person shall park a truck other than a pick-up on a street at any time between the hours of 9:00 P.M. and 7:00 A.M. in front of or adjacent to a residence, motel, apartment house, hotel or other sleeping accommodation.

C. No person shall park a vehicle between 11:00 P.M. and 75:00 A.M. leaving any audible auxiliary motor or engine running. For purposes of this section, “audible” means audible to humans in any public right of way or on any private residential property other than private property where the vehicle is parked with the permission of the owner, and “running” means either continuously or

intermittently running, whether controlled by a thermostat, timer, or other means.

- D. Parking is prohibited ~~in streets~~ immediately adjacent to yellow-marked curbs. Parking or stopping is prohibited in streets or other public areas immediately adjacent to red-marked curbs. The above prohibitions apply unless parking or stopping is necessary to comply with traffic signs and signals, or if traffic does not permit continued movement. No other sign or wording is needed to make the prohibitions effective. The prohibitions established by this section apply if the yellow or red markings are visible, even though faded or partially obliterated. Public areas include private property designated or required as a fire lane. Curbs may be painted red on public or private property only to indicate a fire lane or other area where parking and stopping is prohibited.
- E. No person may park a vehicle in a handicapped parking space without a handicapped license or permit properly displayed on or in the vehicle.

Staff: Changes merge in relevant information from existing Chapter 6.20.

6.15.020 Removal of Illegally Parked Vehicles

- A. The city may remove any illegally parked vehicle that is unattended or that is not removed after a request is made to the owner or person in charge of the vehicle, in compliance with state and city law regulating towing of vehicles by the city.
- B. The city shall not remove any vehicles that were originally legally parked unless the vehicle has remained illegally parked for a period three times longer than the time originally allowed for the vehicle to be parked, unless removal is needed in case of an emergency or to allow the orderly movement of traffic, or the vehicle has been parked for more than 72 hours.

6.15.025 Loading Zone

No person shall stop, stand, or park a vehicle in a loading zone other than to (i.) load or unload materials; or (ii.) service machinery or equipment.

- A. Stopping, standing, or parking a vehicle in a loading zone for the purpose of loading and unloading shall be only for the amount of time reasonably necessary to load and unload the vehicle and perform tasks ancillary to the loading and unloading, and the total time parked shall not exceed 30 minutes.
- B. Any person using a loading zone for parking while servicing machinery or equipment must first obtain a permit from the Newport Police Department authorizing the vehicle to park in a loading zone for a period greater than 30 minutes. The permit must be displayed in the windshield of the vehicle while parked in a loading zone. Permits may be issued on a yearly, monthly, weekly, or daily basis. The fee for the permit shall be set by City Council resolution. Pending a fee resolution, the annual fee shall be \$50.00.

6.15.030 Passenger Loading Zone

No person shall stop, stand, or park a vehicle in a passenger loading zone other than to load and unload passengers. The maximum time to be stopped or parked in a passenger loading zone is five minutes, unless actual loading and unloading requires additional time.

6.15.035 Buses and Taxis

No person may park or stand a bus or taxi on any street in any business district at any place other than at a bus stand or taxicab stand, respectively. This section does not prohibit the driver of any taxi from temporarily stopping for the purpose of loading or unloading of passengers.

6.15.040 Restricted Use of Bus and Taxicab Stands

No person shall stop, stand or park a vehicle other than a bus in a bus stand or other than a taxicab in a taxicab stand, except that the driver of a passenger vehicle may temporarily stop therein while actually engaged in loading or unloading passengers when the stopping does not interfere with any bus or taxi.

6.15.045 Parking Time Limited in Certain Areas

When signs are erected in any block, or within any public parking lot, limiting permissible parking time, no person shall

park a vehicle within the block, or parking lot, for longer than the time posted on the sign. Movement of a vehicle to a parking space on either side of the same street within the area between the intersections at each end of the block shall not extend the time limits for parking. Movement of a vehicle to another parking space within the same parking lot shall not extend the time limits for parking. After a vehicle has been moved from the posted block, or parking lot, for more than one hour, a new time limitation shall apply.

6.15.050 Parking Permits

The City Manager may put in place a program for issuing parking permits to reserve public right-of-way areas or parking lots for use by designated parties. Parking permits may apply in timed parking areas, or elsewhere depending upon the specifications of the permit.

- A. Parking permits may be obtained electronically or in hardcopy form. A hardcopy permit areis to be displayed on a vehicle in the manner specified on the permit, and shall include a description of the authorized activity, license number of the benefited vehicle, and the date or dates within which the permit is effective.
- B. Permit holders and permitted vehicles are subject to all traffic laws and regulations not explicitly superseded by the permit.

Staff: Changes provide an electronic permitting option.

6.15.055 Exemptions

- A. City and public utility vehicles are exempt from this chapter while in use for construction or repair work or other authorized use.
- B. Mail delivery vehicles are exempt from this chapter while in use for the collection, transportation, or delivery of United States mail.

6.15.060 Owner Responsibility

The owner of a vehicle parked in violation of a parking restriction shall be responsible for the violation, except where the use of the vehicle was secured by the operator without the owner's consent. Nothing in this section prevents an owner

from recovering the cost of any penalty from the driver or other person responsible for the illegal parking.

6.15.070 Citation on Illegally Parked Vehicle

Whenever a vehicle without an operator is found parked in violation of a restriction imposed by this Chapter, the officer finding the vehicle shall take its license number and any other information displayed on the vehicle which may identify its owner, and shall conspicuously affix to the vehicle a traffic citation for the operator to answer to the charge against the owner, or pay the penalty imposed within seven days during the hours and at the place specified on the citation.

6.15.080 Registered Owner Presumption

In the prosecution of a vehicle owner, charging violation of a restriction on parking, proof that the vehicle at the time of the violation was registered to the defendant shall constitute a disputable presumption that the registered owner was then the owner in fact.

6.15.090 Failure to Comply with Traffic Citation Attached to a Parked Vehicle

If the operator does not respond to a traffic citation affixed to such vehicle within a period of ten days, the Municipal Court may send to the registered owner of the vehicle, to which the traffic citation was affixed, a letter informing them of the violation and warning them that, any fine associated with the traffic citation is subject to an increase based on the number of days the traffic citation remains unpaid, and based on the city's master fee schedule set by Council resolution.

6.15.100 Penalty

Penalties for violation of this Chapter are set by Council resolution and contained in the city's master fee schedule. Penalties assessed in a traffic citation for a violation of the provisions of this Chapter shall be imposed unless the Municipal Court finds reasonable grounds exist for either increasing or reducing the penalties.

(Unless otherwise specified, new language is shown in double underline, and text to be removed is depicted with ~~strike through~~. Staff comments, in *italics*, are for context and are not a part of the revisions.)

~~CHAPTER 6.20 — CITY PARKING LOTS~~

~~6.20.005 — Parking in City-Owned Parking Lots~~

~~Vehicles may park in marked spaces in city-owned parking lots, subject to compliance with authorized signs limiting the allowable time for parking in the city-owned parking lot. The maximum amount of time a vehicle shall be parked in a city-owned parking lot is 16 hours. Vehicles parking in city-owned parking lots in violation of the posted time limits, or other posted regulations, may be towed, subject to the same restrictions applicable to towing of vehicles from private parking lots.~~

~~6.20.010 — Parking Lots — Publicly Owned/Operated~~

~~No vehicle may be parking in a parking lot owned or operated by a governmental entity contrary to the regulations of the parking lot operator or contrary to any sign posted regulating parking in the parking lot. Vehicles parked in violation of this section may be towed, subject to the same restrictions applicable to towing vehicles from private parking lots.~~

Staff: Delete chapter in its entirety. Relevant language is incorporated into Chapter 6.15.

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CHAPTER 6.20 METERED PARKING ZONES

6.20.005 Purpose

Parking meters are authorized by the City of Newport as a means to increase vehicle turnover in parking spaces, to encourage short-term parking in the metered area, and to improve safety in the public right-of-way.

6.20.010 Parking Meter Zone Designated

The public areas, streets or portions of such streets within a special area defined in Section 14.14.100 that have been designated by the City Council for placement of parking meters shall be considered parking meter zones.

6.20.015 Installation, Function, and Maintenance of Parking Meters

The City Manager will be responsible for the regulation, control, operation, maintenance and use of parking meters, including the establishment of areas within a meter zone where fees are applicable.

6.20.020 Enforcement of Metered Parking Spaces

A. Parking meters are in effect during all hours indicated on the meter and/or sign.

B. All vehicles must adhere to parking meter regulations while stopped or parked in an officially designated metered parking space unless obeying the direction of an authorized officer or unless authorized for specific actions by a vehicle or parking permit.

C. Emergency vehicles may stop or park in any metered space at any time while serving an emergency.

D. It is unlawful to store nonvehicular property in a metered parking space. Any nonvehicular property stored in a metered parking space is a nuisance and may be summarily abated.

6.20.025 Compliance with Time Limit Required

- A. It is unlawful for any person to park any vehicle in any metered parking space during the hours of operation of the meter without paying the parking meter or parking permit fee, or to allow any vehicle in their control or custody to remain in any parking meter space longer than the time designated time limit.
- B. Upon expiration of the designated time limit indicated by the parking meter or parking permit, a citation may be issued if a vehicle remains parked or stopped within the same fee area.
- C. A vehicle may not be parked in any space with a broken or "out of order" meter for a period of time longer than the time limit indicated on the meter. Payment or a valid receipt is required at all spaces regardless of whether the closest device is functioning.

6.20.030 Payment of Meter Fees

A sign or legend which indicates the interval of time for which parking is permitted and the fee payable for the time interval must be posted in all meter areas. The parking meter fee must be paid with U.S. coins, payment card or any other authorized payment method by the person within the vehicle, except:

- A. During all the days and the hours that a meter fee is not required.
- B. Any government vehicle, so identified by public registration plates, may park without meter fee for the maximum time limit allowable at any metered parking space.
- C. A vehicle with a current parking permit authorizing parking at a meter without payment of the meter fee.
- D. A vehicle, for the sole purpose of loading/unloading passengers, for a period not to exceed 5 minutes.

6.20.035 Obstruction of Meters

No vehicle or other property may obstruct access to a parking meter in a manner which prevents deposit of coins in the meter, visibility of the meter instructions or time limit, or visibility of any signs mounted on the meter pipe. A vehicle or

other property in violation of this section is hereby declared to be a nuisance and is subject to summary abatement.

6.20.040 Vandalism, Theft, Invalid Receipts

A. It is unlawful for any person to deface, injure, tamper with, willfully break, destroy, or impair the usefulness of any parking meter installed in public streets, public places, or elsewhere in the City, or to open or remove the same without lawful authority.

B. It is unlawful for any person without lawful authority to remove any coin box or the money content of such coin box or the contents of any parking meter or part thereof.

C. It is unlawful for any person to knowingly manufacture, duplicate, possess, or use any tool, key, implement or device designed to force, break, unlock, or otherwise gain entry to any parking meter maintained by the City unless authorized to do so by the City Manager.

D. Injury to or theft from each meter as described in Subsection A and possession of each item described in Subsections B and C is a separate offense. For each such offense, in addition to the penalties otherwise provided in this Code, a court may order restitution to the City of the damages incurred for repair or re-keying parking meters as a result of injury to the meters or the use or possession of the items described in Subsection C.

E. It is unlawful for any person to duplicate, copy, use or otherwise falsify a parking payment receipt.

F. It is unlawful for any person to damage, deface, or remove a space reservation device.

6.20.045 Unintentional Violation Due to Meter Failure

Any unintentional violation of a provision of this chapter by reason of a mechanical failure of a parking meter is not an offense within the meaning of this chapter once the mechanical failure is verified by the City or where there is a hood covering the meter that states “out of order.”

6.20.050 Applicability of Meters to Accessible Parking Spaces

- A. Vehicles with an official state-issued disabled person registration plate, placard, permit or decal, must adhere to all applicable parking meter regulations.
- B. Vehicle with an official state-issued "Wheelchair User" plate, placard, permit or decal, may park without a fee, provided the length of stay adheres to the storage limitations of Section 6.15.015.

6.20.055 Courtesy Permits

The City Manager may issue courtesy permits valid for a period not to exceed seven (7) days, authorizing the permittee to park a vehicle without regard to time limits and without having to pay the meter fees in any parking space. Such courtesy permits are limited to the following:

- A. Construction permit issued to a person(s) performing work within a public parking lot or right-of-way under the terms of a right-of-way permit issued pursuant to Chapter 9.10.
- B. Vehicles associated with a special event permit authorized pursuant to Chapter 9.80 where the specifically identifies affected parking spaces.

6.20.060 Meter Collection Duty

It shall be the duty of the City Manager to direct the collection of all coins deposited in parking meters.

6.20.065 Disposition of Meter Proceeds

Parking meter proceeds will be deposited in the City of Newport Parking Fund to provide for maintenance and improvement of parking areas and for the proper regulation, control and inspection of traffic upon the public streets and lots within parking special areas as defined in Chapter 14.14.100. This includes covering the cost of supervising, regulating and inspecting the parking of vehicles as provided for in this chapter, the cost of placing and maintaining lines or markings designating parking spaces and expenses associated with the installation, operation, maintenance, control and use of the parking meters installed under this chapter and other related expenses.

6.20.070 Establishment of Fees and Maximum Time Limits

Parking meter fees and maximum time limits shall be established by resolution of the City Council.

Staff: This initial draft draws from codes adopted by the [Hood River](#), [Corvallis](#), and [Portland](#). The language will be vetted with the Parking Advisory Committee at its next meeting and is subject to change.

DRAFT

(Unless otherwise specified, new language is shown in double underline, and text to be removed is depicted with ~~strike through~~. Staff comments, in *italics*, are for context and are not a part of the revisions.)

CHAPTER 6.25 RECREATIONAL VEHICLE PARKING

6.25.005 Definitions

Public Or Private Parking Lot means a parking lot that is open to the general public for parking, whether for a fee or not. Parking lot does not include areas reserved for owners or tenants of a property.

Recreational Vehicle or RV means a vehicle with or without motive power that is designed for use as temporary living quarters and as further defined by the Oregon Department of Transportation in OAR Chapter 735, Division 022. Examples include motor homes, camping trailers, tent trailers, truck campers and camper vans.

6.25.010 Parking of Recreational Vehicles

- A. Recreational vehicles may not be parked and occupied in the right-of-way or on any public or private parking lot between the hours of 11:00 P.M. and 5:00 A.M., except in areas where camping is permitted as identified in Section 9.50.050 or as part of a Special Event Permit approved pursuant to Chapter 9.80.

Staff: Since this chapter applies to both public and private parking areas, it is not being merged into the other parking related amendments.

Updated Regulatory Sign Concepts

Paid (Meter Only) Areas
Green Streets on Map



Paid/Permit Streets
Orange Streets on Map



Paid/Permit Lots
Orange Lots on Map



Permit/Timed Streets
Pink Streets on Map



Permit/Timed Lots
Pink Streets on Map

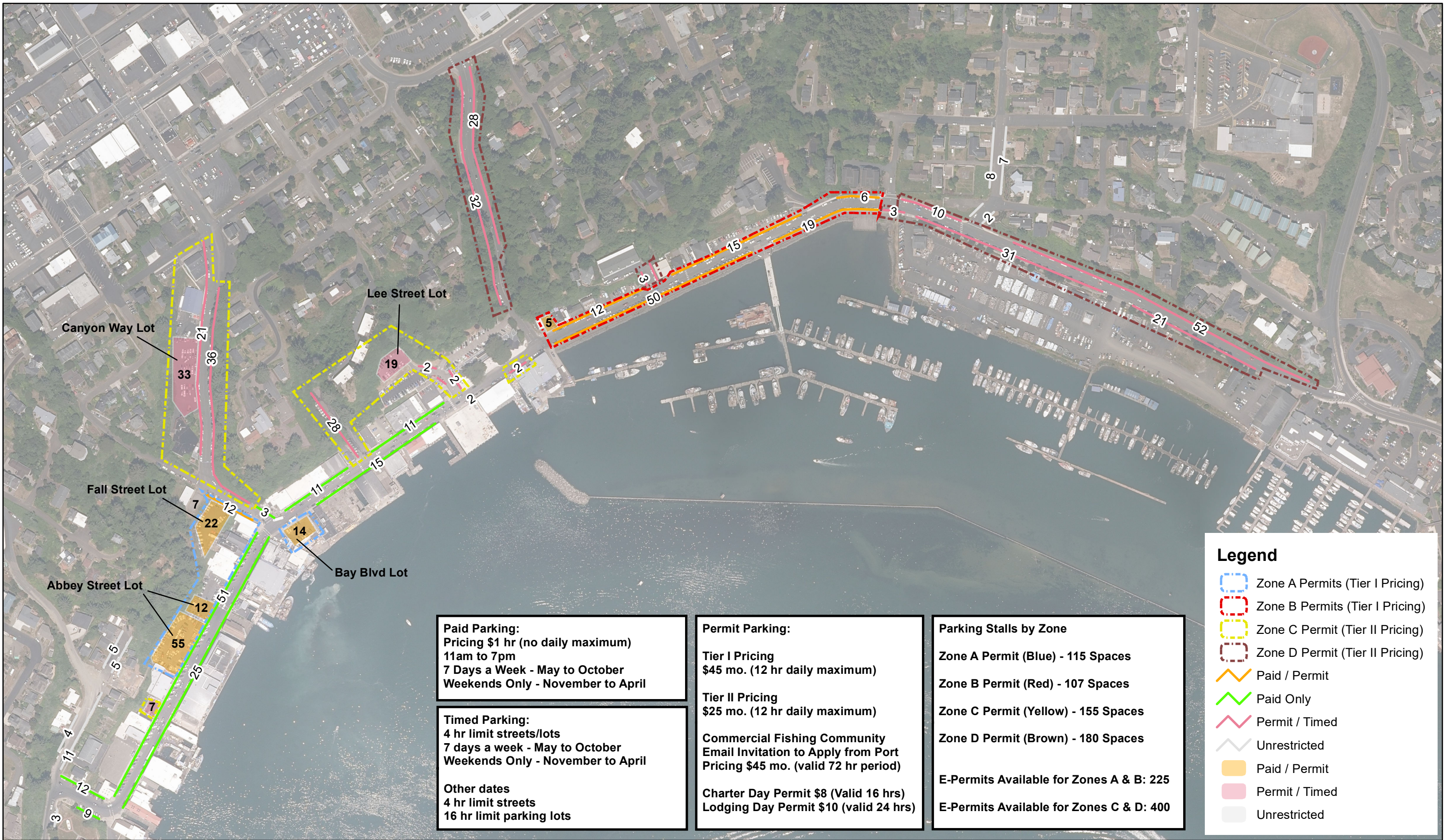


Loading Zone Options
Upland Side of Bay Blvd



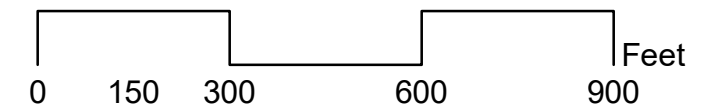
Loading Zone Estuary
Side of Bay Blvd



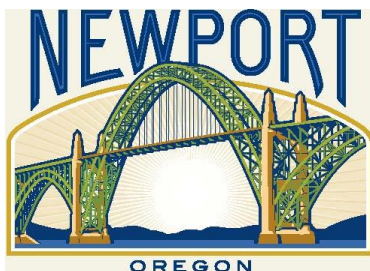


Draft Bay Front Parking Management Alternative

Aerial Image Taken 2021
 4-inch, 4-band Digital Orthophotos
 Date: February 15, 2023



This map is for informational use only and has not been prepared for, nor is it suitable for legal, engineering, or surveying purposes. It includes data from multiple sources. The City of Newport assumes no responsibility for its compilation or use and users of this information are cautioned to verify all information with the City of Newport Community Development Department.



Draft Bayfront Parking Management Solution Implementation Schedule

Task	2023							
	Apr	May	June	July	Aug	Sept	Oct	
1. T2 Systems Contract Executed	█							
2. T2 Project Kick-off		█						
3. Stakeholder Outreach					* █		* █	
4. Parking System Setup (Mobile Pay, Permits, Enforcement)		█ ◆			◆ █	★ █		
5. License Plate Recognition Install					◆ █			
6. Parking Lot Improvements		▲ █	● █			■ █		
7. Sign Pole Purchase and Install			● █			■ █		
8. Regulatory Sign Design and Install				▲ █			■ █	
9. Pay Station Install & Configuration							■ █	
10. Launch Metering/Permit Program							* █	

Legend		
★ Wrap-up Configuration	▲ Design	* Public Engagement Activities
◆ Training Sessions	● Bid Project	■ Initiate Construction

- T2 parking system config/training timeframes may change based on staff & material availability
- August public engagement to include rollout of planned implementation, pricing, etc.
 - Commercial Fishing User Group
 - Port Commission
 - Seafood Processors
 - Bayfront Businesses
- Incorporate feedback into a round of adjustments and wrap-up configuration in September
- October public engagement to include information on upcoming changes and kick-off event.
- 30-day minimum grace period on tickets to be provided after go live date (warnings only)