



City of Newport

Lincoln County, Oregon

VOLUME 3 - Project Drawings
-Conformed Documents-

FOR THE CONSTRUCTION OF

Newport Abalone and 30th Roadway Improvements



June, 2015
Project No. 2302-027



Prepared By:
Civil West Engineering Services, Inc.
www.civilwest.com

609 SW Hubert Street • Newport, Oregon 97365 • Ph. 541.264.7040 • Fax 541.264.7041
486 E Street • Coos Bay, Oregon 97420 • Ph. 541.266.8601 • Fax 541.266.8681
10558 Highway 62 Suite B-1 • Eagle Point, Oregon 97524 • Ph. 541.326.4828

CITY OF NEWPORT
LINCOLN COUNTY, OREGON



SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

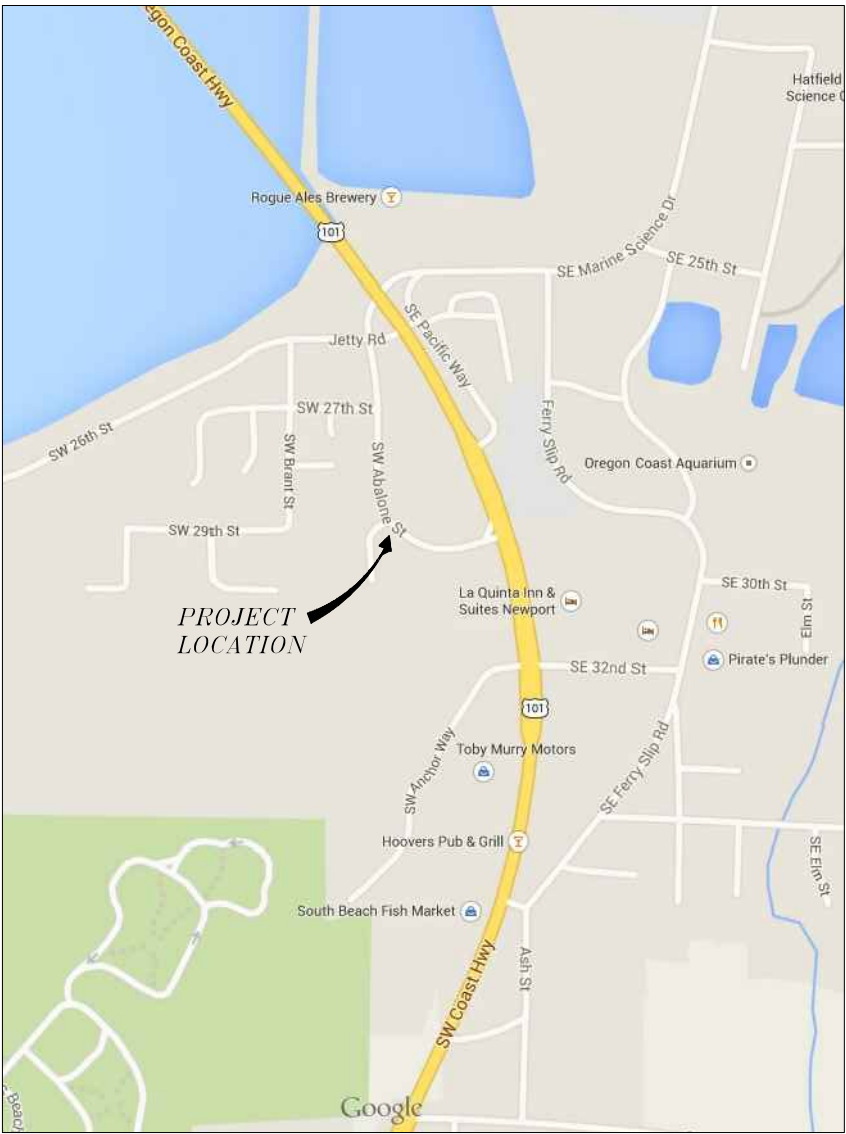
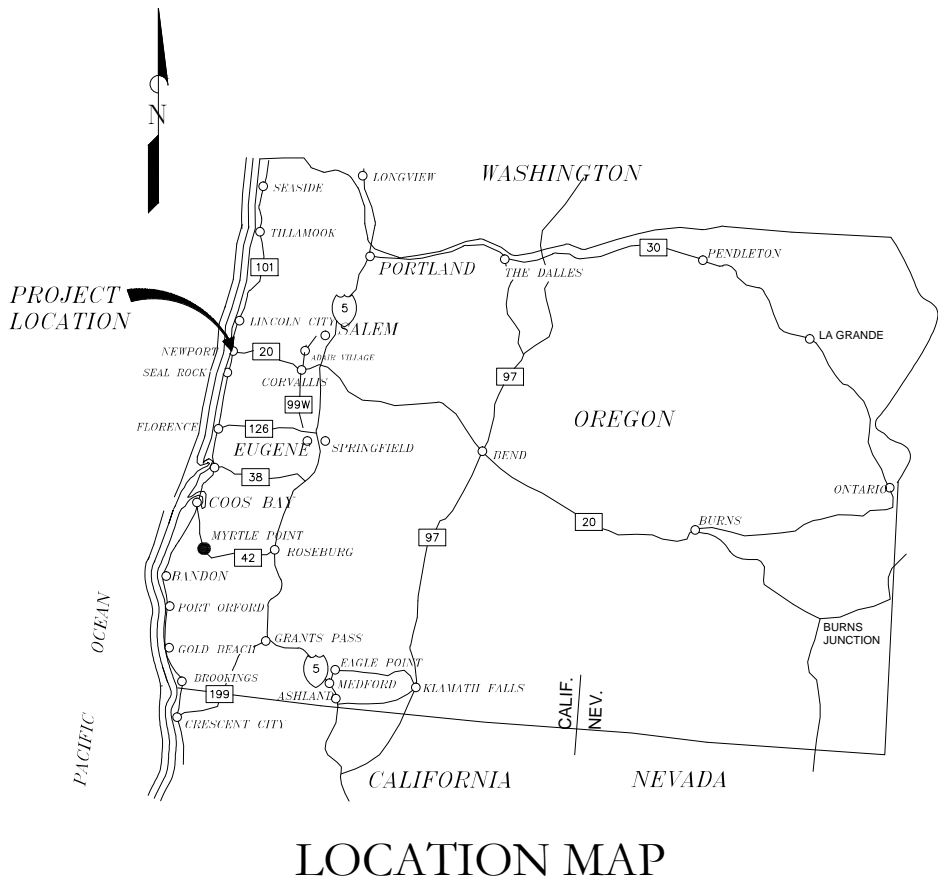
PROJECT NO. 2302-027
JUNE 2015

REGISTERED PROFESSIONAL
ENGINEER
88387PE
Ralph R. Wenziger
OREGON
SEPT 10, 2013
RALPH R. WENZIGER

EXPIRATION DATE: 06/30/16

Civil West
Engineering Services, Inc.

609 SW Hurbert Street
Newport, Oregon 97365
541-264-7040
www.civilwest.com



VICINITY MAP
NOT TO SCALE

REV.	DATE	DESCRIPTION	BY
	7/2/15	ADDENDUM #2 (AD2)	FW
	7/16/15	ADDENDUM #5 (AD5)	FW
DESIGNED BY:	7/17/15	ADDENDUM #6 (AD6)	FW
DRAWN BY:		ALF	MEH
CHECKED BY:			
PROJECT NO.		2302-027	
CITY OF NEWPORT LINCOLN COUNTY, OREGON		SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	
LOCATION & VICINITY MAP		G1	
DATE		JULY 2015	

DATE:6/15/15 FILE:Z:\CW_Projects\2302 Newport\2302-027 Newport Abalone and 30th Roadway\Imps\Drawings\Dwg\Detail Sheets.dwg

GENERAL NOTES

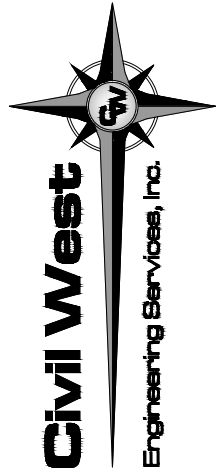
1. OREGON LAW REQUIRES CONTRACTORS TO FOLLOW THE RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH 952-001-0090 AND ORS 757.542 THROUGH ORS 757.562 AND ORS 757.993. YOU MAY OBTAIN COPIES OF THE RULES FROM THE CENTER BY CALLING (503) 246-1987.
2. THE CONTRACTOR SHALL CONTACT 'ONE CALL' FOR UTILITY LOCATES PRIOR TO EXCAVATION. (1-800-332-2344)
3. THE EXISTING UTILITY CROSSINGS OF THE PIPELINES ARE SHOWN ACCORDING TO AVAILABLE INFORMATION. THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL THE UTILITY CROSSINGS ALONG THE LENGTH OF THE PIPELINES AS SPECIFIED. NO GUARANTEE IS MADE THAT ALL OF THE EXISTING UTILITIES ARE SHOWN. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN EXCAVATING AND PROTECT ALL EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION.
4. OVERHEAD ELECTRICAL DISTRIBUTION SYSTEMS ARE NOT SPECIFICALLY INDICATED ON THE DRAWINGS BUT DO EXIST ALONG THE PIPELINE ROUTES.
5. EXISTING WATER METERS, VALVE BOXES AND MANHOLES MAY NOT BE SPECIFICALLY INDICATED ON THE DRAWINGS BUT DO EXIST ALONG THE PIPELINE / PAVEMENT IMPROVEMENT ROUTES. CONTRACTOR SHALL. LOCATE PRIOR TO THE START OF CONSTRUCTION AND ADJUST AS REQUIRED FOR A COMPLETE PAVING IMPROVEMENT PROJECT.
6. THE LOCATION AND DEPTH SHOWN ON THESE DRAWINGS FOR THE EXISTING WATERLINES ARE APPROXIMATE ONLY AND BASED ON AS BUILT DRAWINGS, VALVE LOCATIONS AND OTHER INFORMATION. THERE ARE NO TRACER WIRES FOR LOCATING THE MAJORITY OF EXISTING WATERLINES AND EXISTING WATERLINES MAY BE IN CLOSE PROXIMITY TO NEW WATERLINE ROUTES.
7. CONTRACTOR SHALL POTHOLE AND LOCATE EXISTING WATERLINES PRIOR TO PLACEMENT OF NEW WATERLINES. EXISTING WATERLINES SHALL REMAIN IN SERVICE AND BE PROTECTED IN PLACE UNTIL COMPLETION OF NEW WATERLINES. CONTRACTOR SHALL PROVIDE TEMPORARY CONNECTIONS AS REQUIRED TO MAINTAIN CONTINUED SERVICE TO CUSTOMERS UNTIL COMPLETION OF NEW WATERLINE.
8. AFTER COMPLETION OF NEW WATERLINES AND ALL TESTING AND CONNECTIONS HAVE BEEN MADE. DESIGNATED PORTIONS OF THE EXIST'G WATERLINES ARE TO BE ABANDONED IN PLACE, REMOVE TEMPORARY CONNECTIONS, EXISTING VALVES, COVERS AND PROVIDE END CAPS OR PLUGS AS REQUIRED FOR ABANDONMENT.
9. THE PIPELINE PROFILES HAVE BEEN MARKED TO INDICATE THE REQUIRED BACKFILL CLASSES (A, B, & E) SEE TECHNICAL SPECIFICATION FOR SPECIFIC BACKFILL MATERIAL REQUIREMENTS.
10. IF NO RECORD WAS AVAILABLE TO INDICATE THE ELEVATION OF AN EXISTING UTILITY A MINIMUM COVER OF 30-INCHES WAS ASSUMED. THE CONTRACTOR SHALL EXERCISE USING CAUTION WHILE EXCAVATING NEAR THESE ESTIMATED UTILITY LOCATIONS WHICH ARE INDICATED ON THE PROFILE DRAWINGS.
11. CONTRACTOR SHALL INSTALL NEW WATERLINES WITH A MINIMUM CLEARANCE OF 6-INCHES AT ALL CROSSINGS TO EXISTING WATERLINES, STORM DRAINS, UNDERGROUND TELEPHONE AND ELECTRICAL UNLESS OTHERWISE SPECIFIED OR SHOWN OR AS APPROVED BY THE ENGINEER. A 6-INCH CLEARANCE IS NOT REQUIRED WHERE NEW WATERLINE WILL TIE DIRECTLY INTO THE EXISTING WATERLINE AT SAID CROSSING.
12. ALL MATERIALS IN CONTACT WITH WATER SHALL BE NSF 61 APPROVED.
13. ALL MATERIALS AND WORKMANSHIP SHALL BE AS SPECIFIED AND SHALL CONFORM TO THE PROJECT DESIGN SPECIFICATIONS AND DRAWINGS, AS WELL AS THE LATEST EDITION OF THE OREGON STANDARD SPECIFICATIONS (OSS). THESE DRAWINGS SHALL BE COORDINATED AND USED IN CONJUNCTION WITH THE TECHNICAL SPECIFICATIONS AND APPROVED SUBMITTALS.
14. CONSTRUCTION PERMITS AS REQUIRED FROM LINCOLN COUNTY ROAD DEPARTMENT TO WORK WITHIN THE RIGHT-OF-WAY SHALL BE OBTAINED BY THE OWNER PRIOR TO THE START OF CONSTRUCTION.
15. CONTRACTOR TO COORDINATE WITH AFFECTED UTILITIES FOR REMOVAL AND/OR RELOCATION OF POWER POLES, TELEPHONE PEDESTALS, ETC. PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.

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G3 - LEGENDS, ABBREVIATIONS, & SYMBOLS
G4 - SHEET INDEX MAP

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E2 - SW 27TH ST. & SW BRANT LIGHTING AND CONDUIT PLAN
E3 - ELECTRICAL DETAILS - LIGHTING



541-264-7040
www.civilwest.com
609 SW Hurbert Street
Newport, Oregon 97365

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Designed By: RWW		Drawn By: ALF	Checked By: MEH
Project No: 2302-027			

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	GENERAL NOTES & SHEET INDEX
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Sheet No:	G2
Date:	June 2015

GENERAL

GRADING

SYMBOL LEGEND

SYMBOL LEGEND

LINE TYPE LEGEND


HATCH LEGEND

LINETYPE LEGEND

HATCH LEGEND

GRADING LEGEND

REGISTERED PROFESSIONAL
ENGINEER
88387PE
OREGON
SEPT 10, 2013
RALPH R. WENZIGER



Civil West
Engineering Services, Inc.

[illegible]

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	LEGENDS, ABBREVIATIONS, & SYMBOLS
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G3

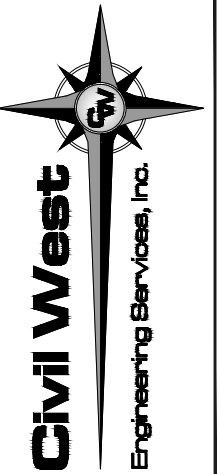
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SHEET INDEX MAP
1" = 200'



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<div>CITY OF NEWPORT</div> <div>LINCOLN COUNTY, OREGON</div>	<div>SW ABALONE ST. & 30TH ST.</div> <div>ROADWAY IMPROVEMENTS</div>		REV.	DATE	DESCRIPTION	BY	
		Designed By: RWV		Drawn By: ALF	Checked By: MEH		
		Project No: 2302-027					
		<div>SHEET INDEX MAP</div>					

Sheet No.

Date

G4

June 2015

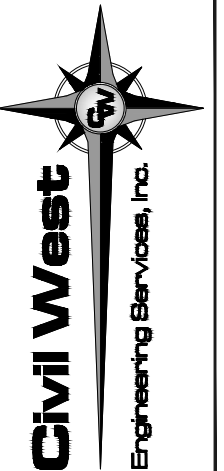


SHEET NOTES:

1. EXISTING SEWER LINES AND WATER LINES SHALL BE PROTECTED IN PLACE UNLESS NOTED OTHERWISE.
2. ADJUST EXISTING STORM DRAIN MANHOLE RIMS, SANITARY SEWER MANHOLE RIMS, VALVE CAN & LIDS TO MATCH NEW FINISH GRADES.
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SW 35th ST PROFILE - STA 0+00 TO STA 3+00
SCALE: HORZ 1"=40'
VERT 1"=10'



Newport, Oregon 97365

[illegible]

CITY OF NEWPORT
LINCOLN COUNTY, OREGON

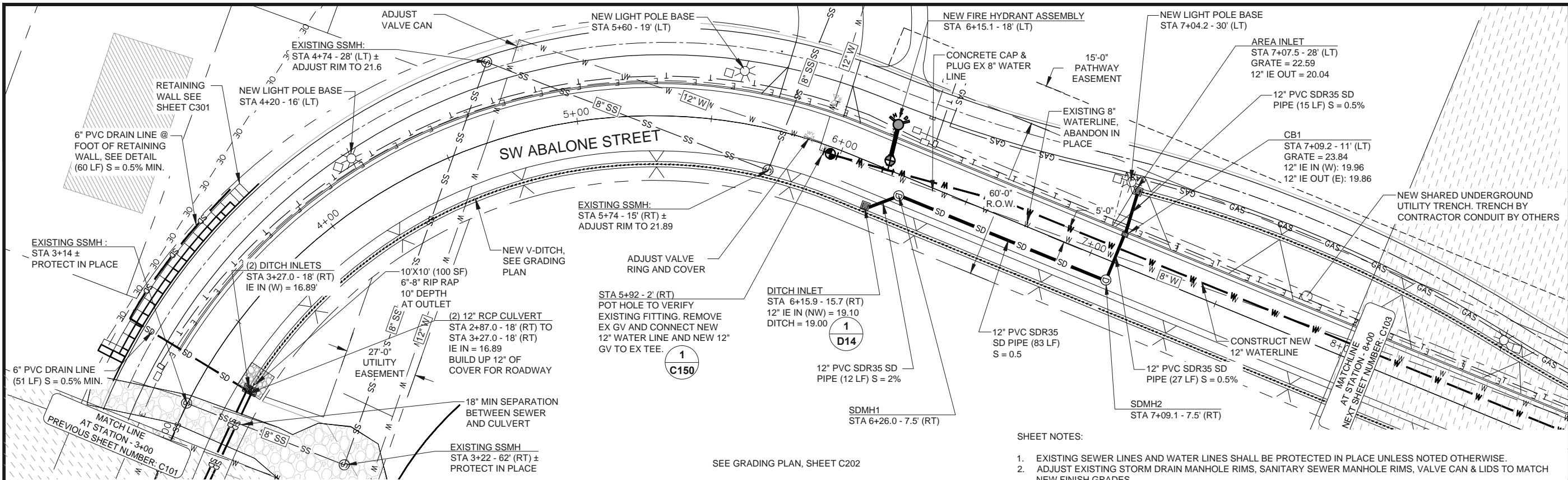
SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

UTILITY PLAN & PROFILE - SW 35TH ST -
STA 0+00 TO STA 3+00

C101

June 2015

DATE: 6/15/15 FILE: C:\CW_Projects\2302\2302-027 Newport Abalone and 30th Roadway Imps\Drawings\DWG\Abalone & 30th - DESIGN.dwg

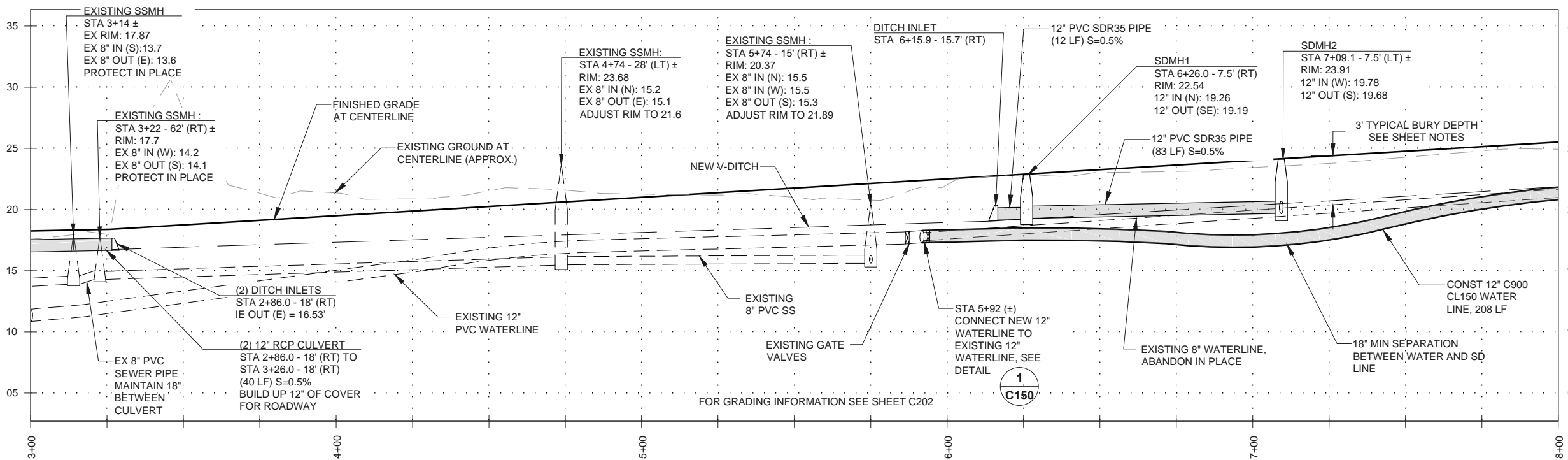


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C102

SW ABALONE ST UTILITY PLAN - STA 3+00 TO STA 8+00
SCALE: 1"=40'

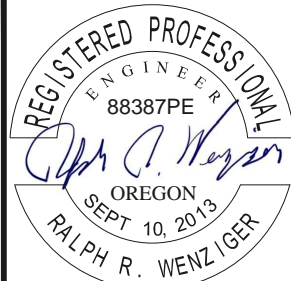
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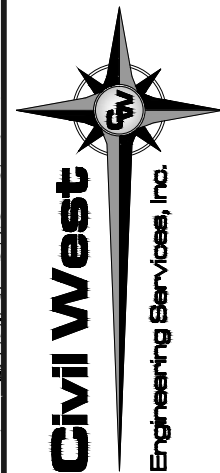


2
C102

SW ABALONE ST PROFILE - STA 3+00 TO STA 8+00
SCALE: HORZ 1"=40'
VERT 1"=10'



EXPIRATION DATE: 06/30/16



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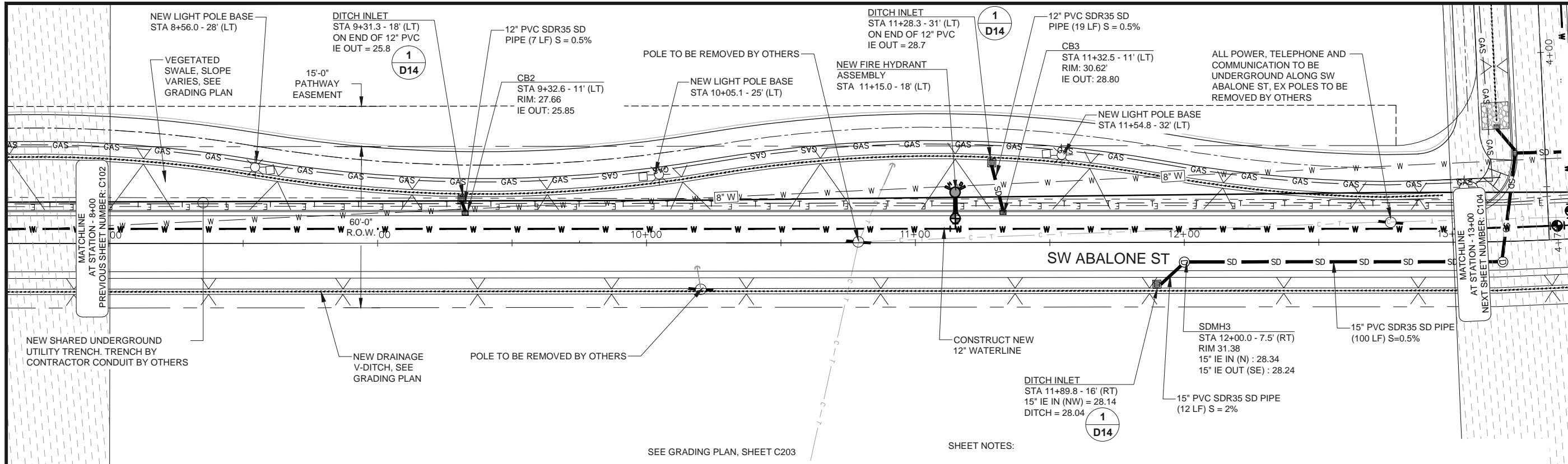
REV.	DATE	DESCRIPTION	BY

Designed By:	Drawn By:	Checked By:
RWW	ALF	MEH

Project No: 2302-027

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	UTILITY PLAN & PROFILE - SW ABALONE ST - STA 3+00 TO STA 8+00
Sheet No:	C102	June 2015

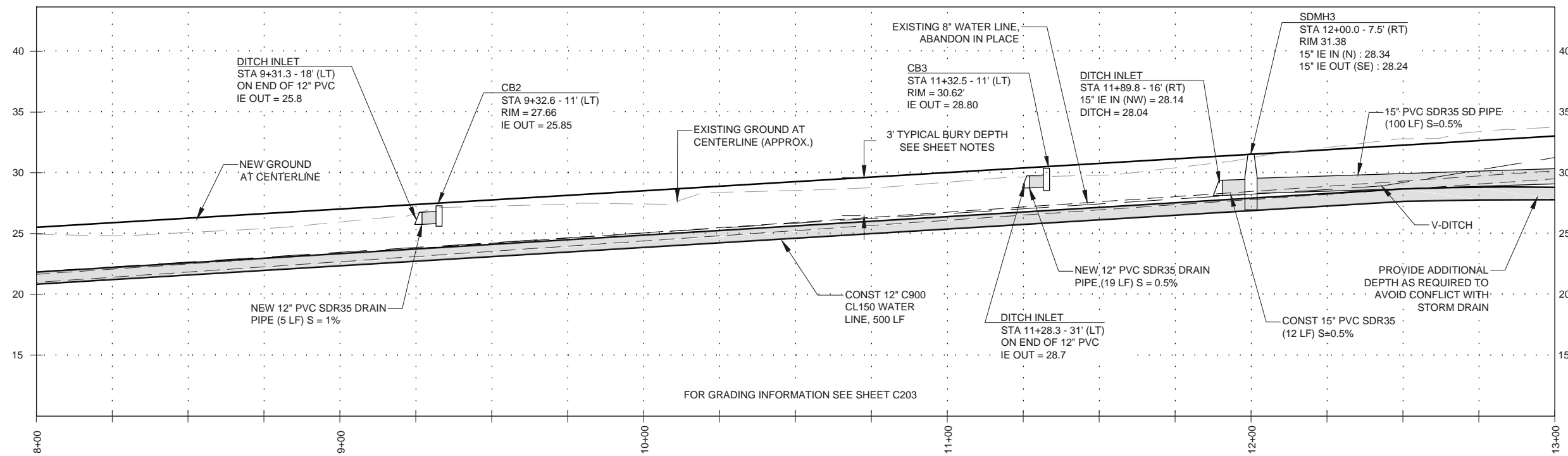
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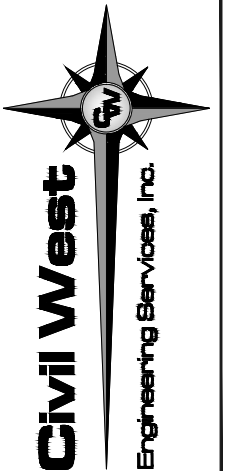
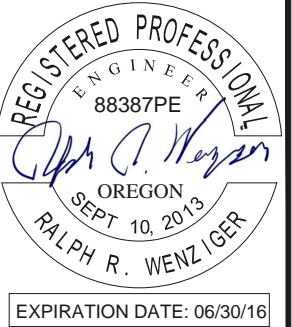
1
C103 SW ABALONE ST UTILITY PLAN - STA 8+00 TO STA 13+00
SCALE: 1"=40'

SHEET NOTES:

- EXISTING UTILITIES TO BE PROTECTED IN PLACE UNLESS NOTED OTHERWISE.
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2
C103 SW ABALONE ST PROFILE - STA 8+00 TO STA 13+00
SCALE: HORZ 1"=40'
VERT 1"=10'



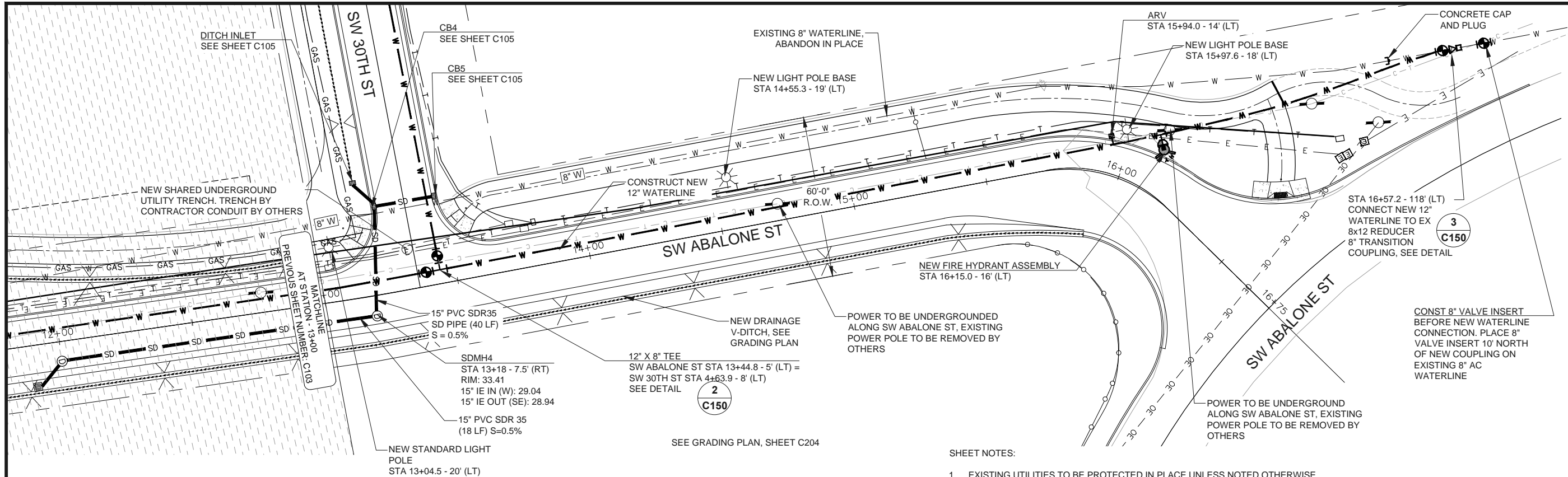
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Designed By: RWW	Drawn By: ALF	Checked By: MEH
Project No: 2302-027		

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	UTILITY PLAN & PROFILE - SW ABALONE ST - STA 8+00 TO STA 13+00
Sheet No: Date:	C103 June 2015	

DATE:6/15/15 FILE:2\CW_Projects\2302 Newport\2302-027 Newport Abalone and 30th Roadway Imps\Drawings\Uvg\Abalone & 30th - DESIGN.dwg

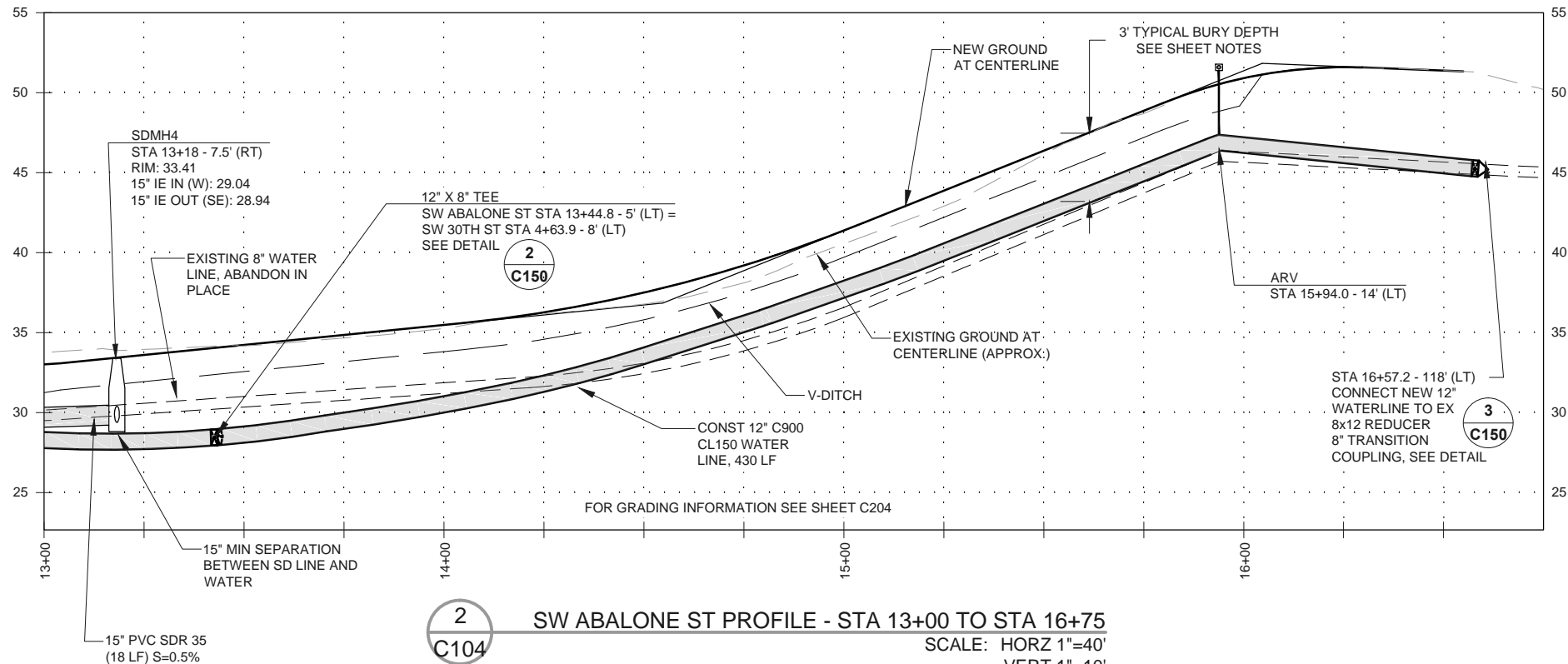


1 SW ABALONE ST UTILITY PLAN - STA 13+00 TO STA 16+50
C104 SCALE: 1"=40'



SHEET NOTES:

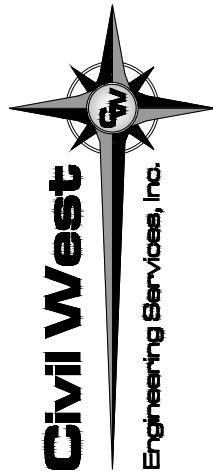
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2 SW ABALONE ST PROFILE - STA 13+00 TO STA 16+75
C104 SCALE: HORZ 1"=40'
VERT 1"=10'



EXPIRATION DATE: 06/30/16



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609 SW Hurbert Street
Newport, Oregon 97365

REV.	DATE	DESCRIPTION	BY

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RWW	ALF	MEH

Project No: 2302-027

CITY OF NEWPORT
LINCOLN COUNTY, OREGON

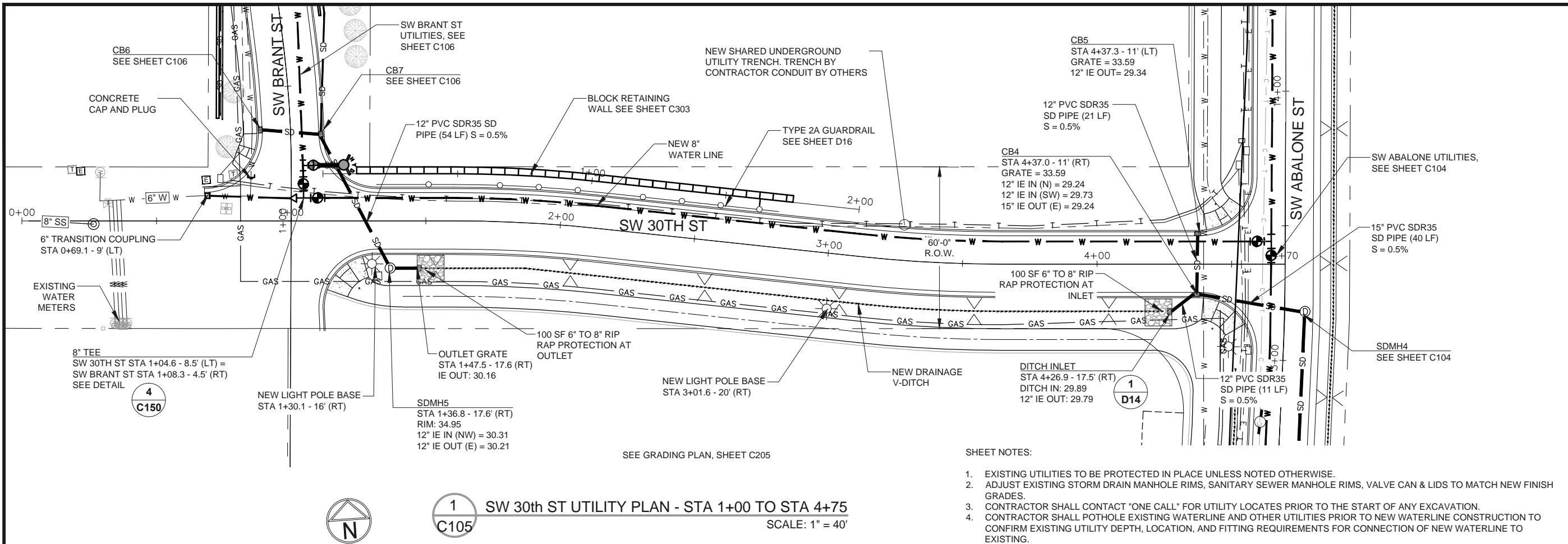
SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

UTILITY PLAN & PROFILE - SW ABALONE
ST - STA 13+00 TO STA 16+75

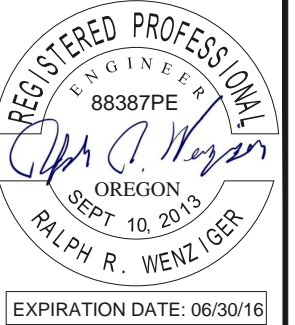
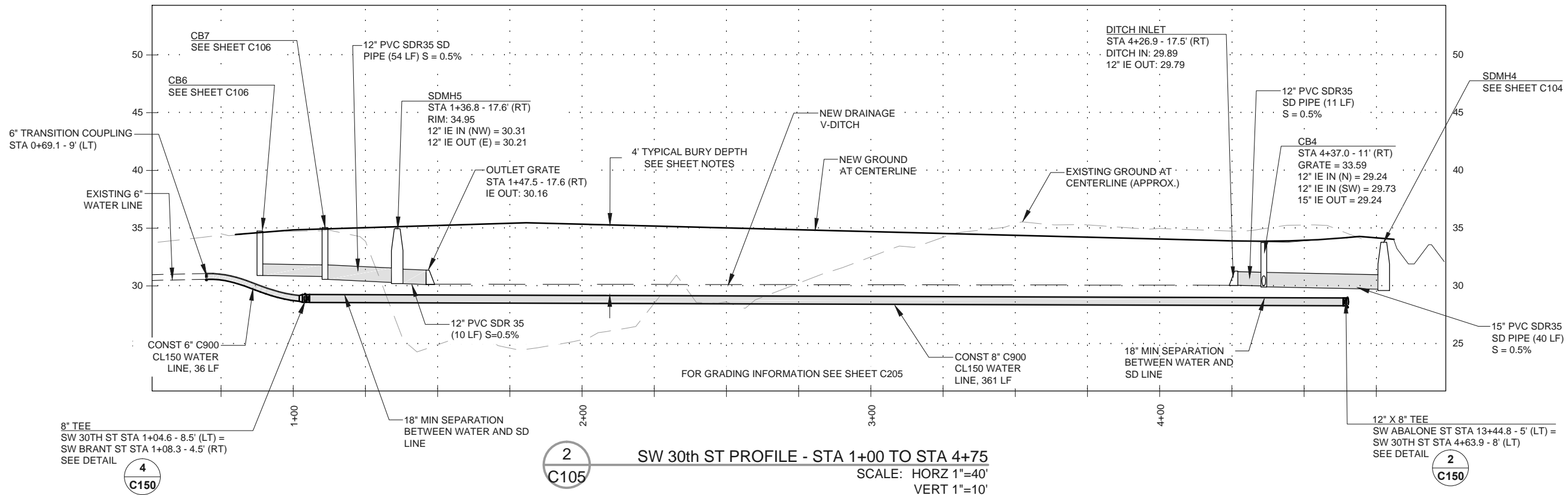
C104

June 2015

DATE: 6/15/15 FILE: C:\CW_Projects\2302\2302-027 Newport Abalone and 30th Roadway Improvements\Drawings\Abalone & 30th - DESIGN.dwg



- SHEET NOTES:
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Designed By: RWW	Drawn By: ALF	Checked By: MEH
Project No: 2302-027		

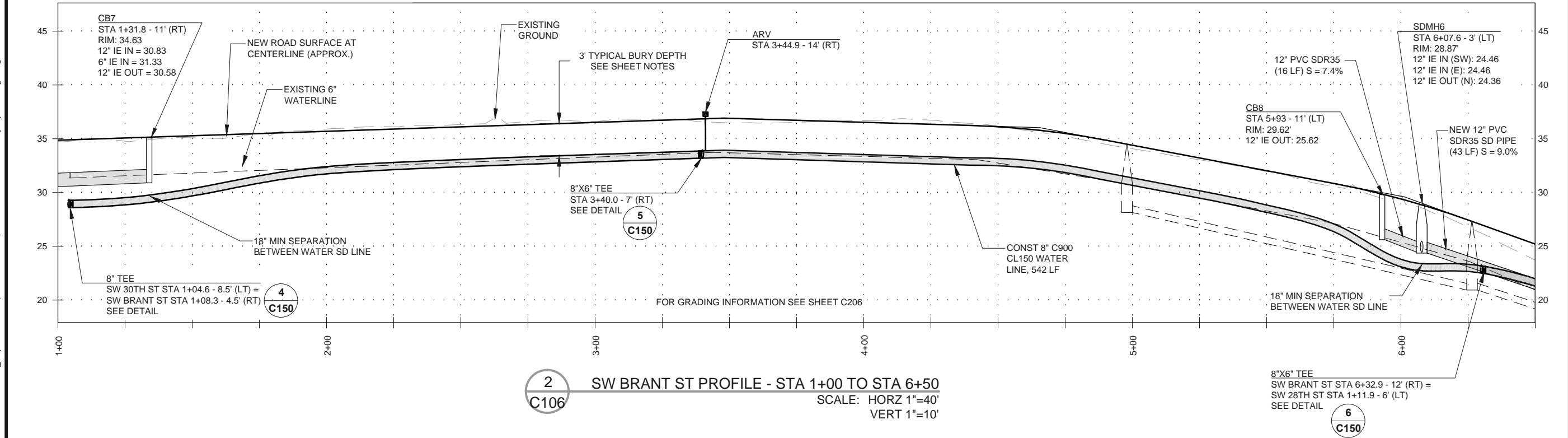
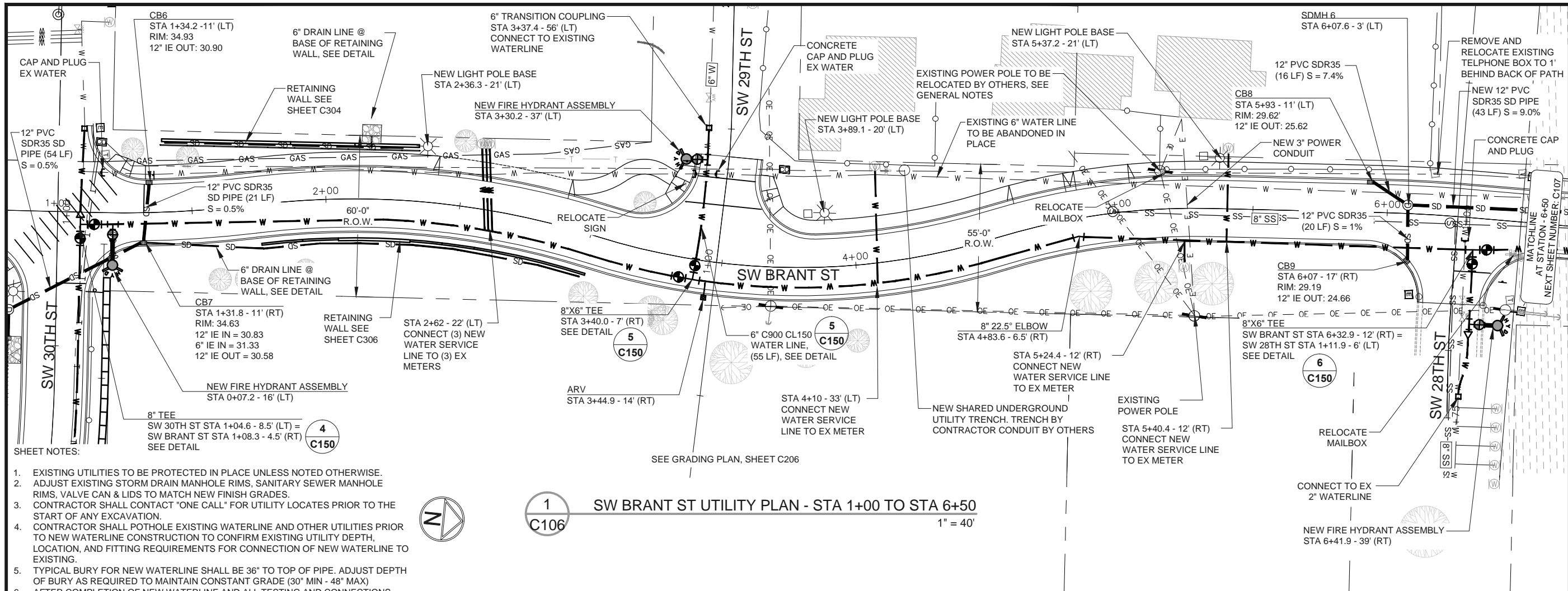
CITY OF NEWPORT
LINCOLN COUNTY, OREGON

SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

UTILITY PLAN & PROFILE - SW 30TH ST -
STA 1+00 TO STA 4+75

Sheet No: **C105**
Date: **June 2015**

DATE: 6/15/15 FILE: C:\CW_Projects\2302-027 Newport\Drawings\Imps\Drawings\Abalone & 30th - DESIGN.dwg



REGISTERED PROFESSIONAL
ENGINEER
88387PE
Ralph R. Wenziger
OREGON
SEPT 10, 2013
RALPH R. WENZIGER
EXPIRATION DATE: 06/30/16

Civil West
Engineering Services, Inc.

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REV.	DATE	DESCRIPTION	BY

Designed By: RWW	Drawn By: ALF	Checked By: MEH
Project No: 2302-027		

CITY OF NEWPORT
LINCOLN COUNTY, OREGON

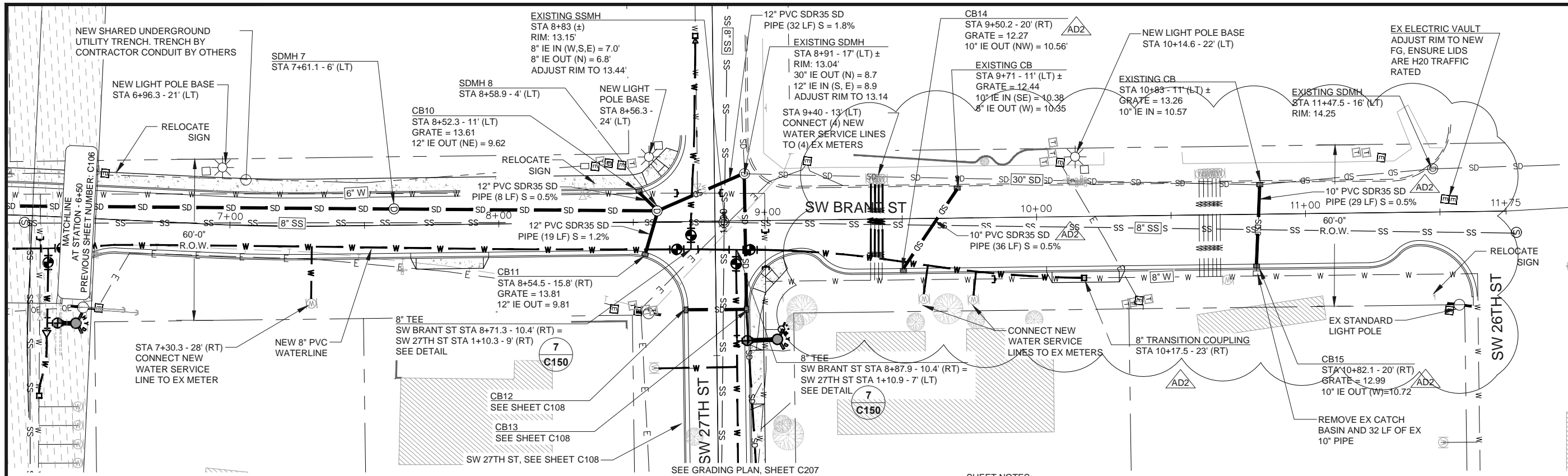
SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

UTILITY PLAN & PROFILE - SW BRANT ST -
STA 1+00 TO STA 6+50

Sheet No:
Date:

C106
June 2015

DATE: 7/10/15 FILE: \\C:\Projects\2302\2302 Newport Abalone and 30th Roadway Improvements\Drawings\Abalone & 30th - DESIGN.dwg



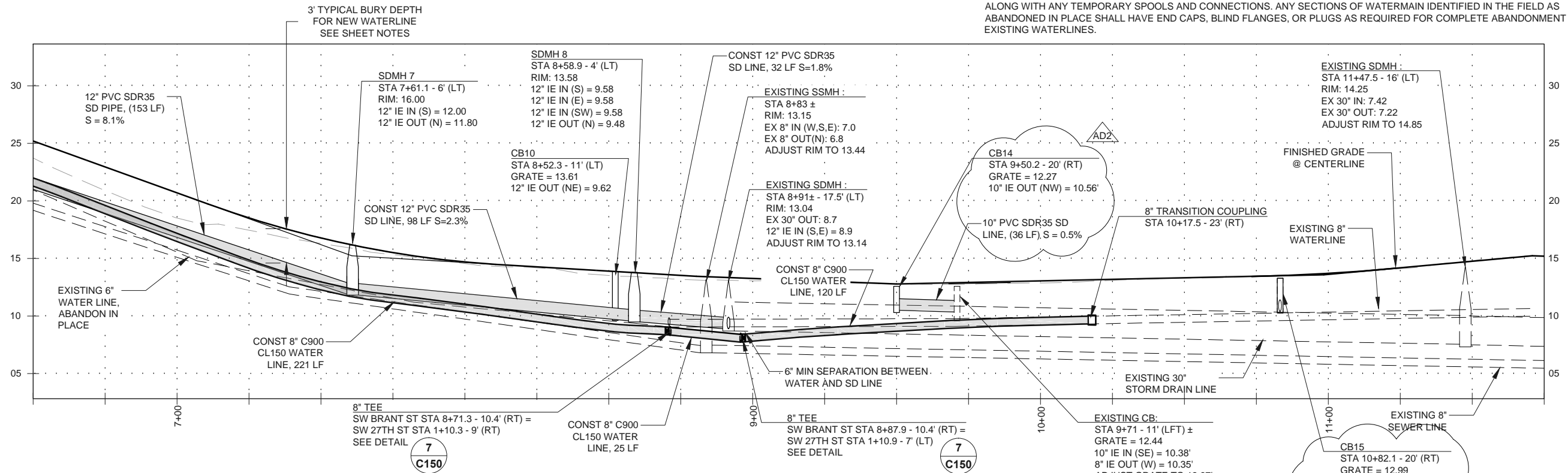
1
C107

SW BRANT ST UTILITY PLAN - STA 6+50 TO STA 11+75

1" = 40'

SHEET NOTES:

- EXISTING UTILITIES TO BE PROTECTED IN PLACE UNLESS NOTED OTHERWISE.
- ADJUST EXISTING STORM DRAIN MANHOLE RIMS, SANITARY SEWER MANHOLE RIMS, VALVE CAN & LIDS TO MATCH NEW FINISH GRADES.
- CONTRACTOR SHALL CONTACT "ONE CALL" FOR UTILITY LOCATES PRIOR TO THE START OF ANY EXCAVATION.
- CONTRACTOR SHALL PROTECT EXISTING WATERLINE AND OTHER UTILITIES PRIOR TO NEW WATERLINE CONSTRUCTION TO CONFIRM EXISTING UTILITY DEPTH, LOCATION, AND FITTING REQUIREMENTS FOR CONNECTION OF NEW WATERLINE TO EXISTING.
- TYPICAL BURY FOR NEW WATERLINE SHALL BE 36" TO TOP OF PIPE. ADJUST DEPTH OF BURY AS REQUIRED TO MAINTAIN CONSTANT GRADE (30" MIN - 48" MAX).
- AFTER COMPLETION OF NEW WATERLINE AND ALL TESTING AND CONNECTIONS HAVE BEEN MADE, DESIGNATED PORTIONS OF EXISTING WATERLINE ARE TO BE ABANDONED IN PLACE. EXISTING VALVES AND BOXES SHALL BE REMOVED IN THEIR ENTIRETY ALONG WITH ANY TEMPORARY SPOOLS AND CONNECTIONS. ANY SECTIONS OF WATERMAIN IDENTIFIED IN THE FIELD AS ABANDONED IN PLACE SHALL HAVE END CAPS, BLIND FLANGES, OR PLUGS AS REQUIRED FOR COMPLETE ABANDONMENT OF EXISTING WATERLINES.

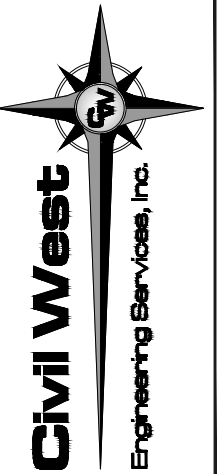
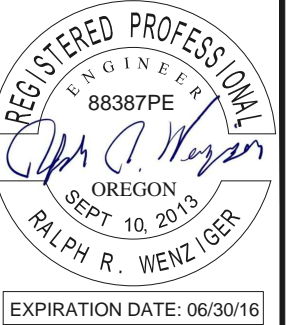


FOR GRADING INFORMATION SEE SHEET C207

2
C107

SW BRANT ST PROFILE - STA 6+50 TO STA 11+75

SCALE: HORZ 1"=40'
VERT 1"=10'



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REV.	DATE	DESCRIPTION	BY
1	7/2/2015	ADDENDUM #2 (AD2)	

Designed By:	Drawn By:	Checked By:
RWW	ALF	MEH

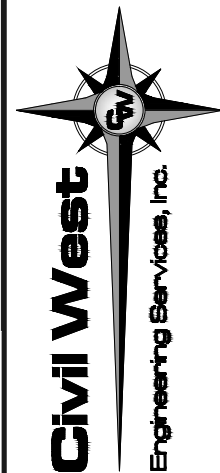
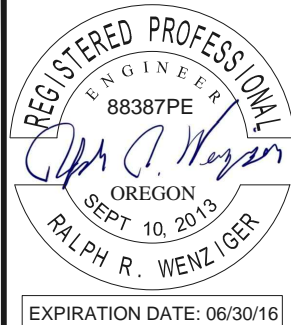
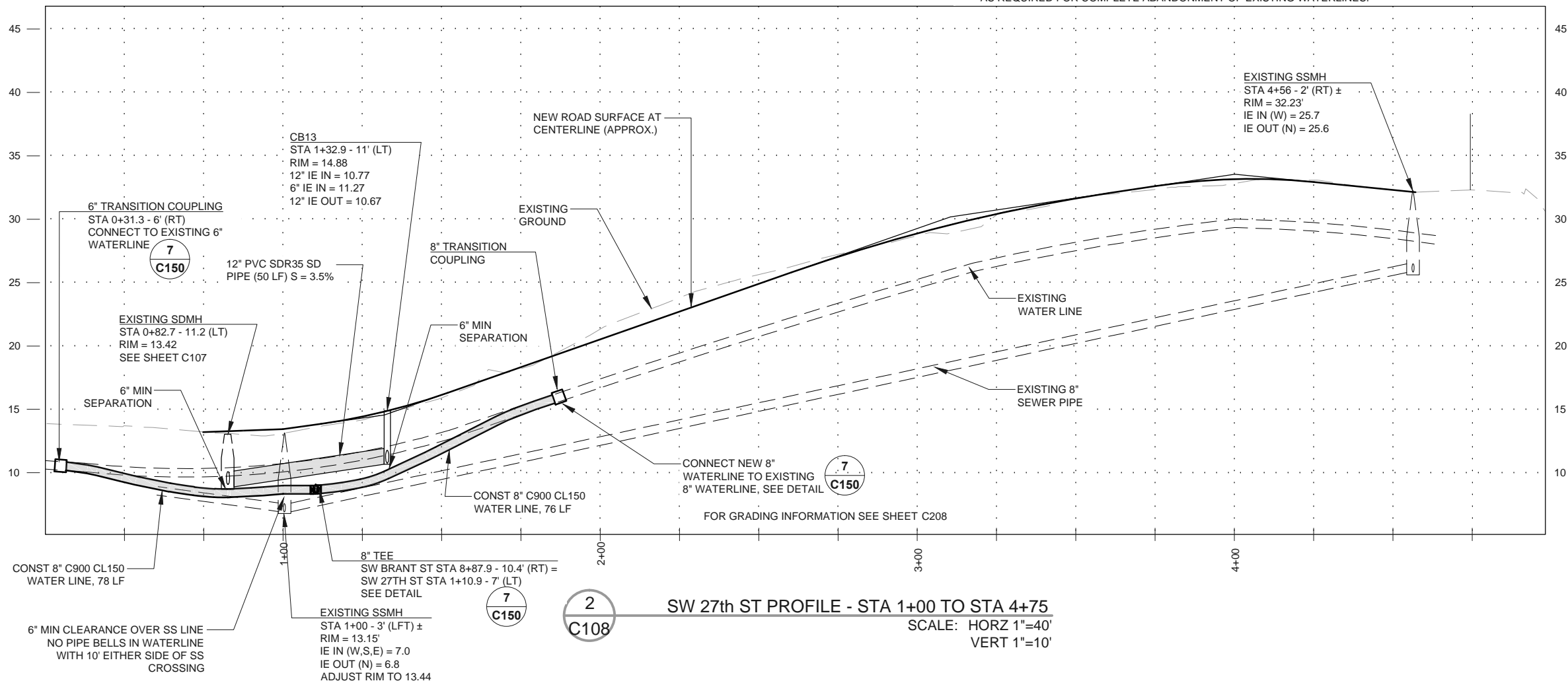
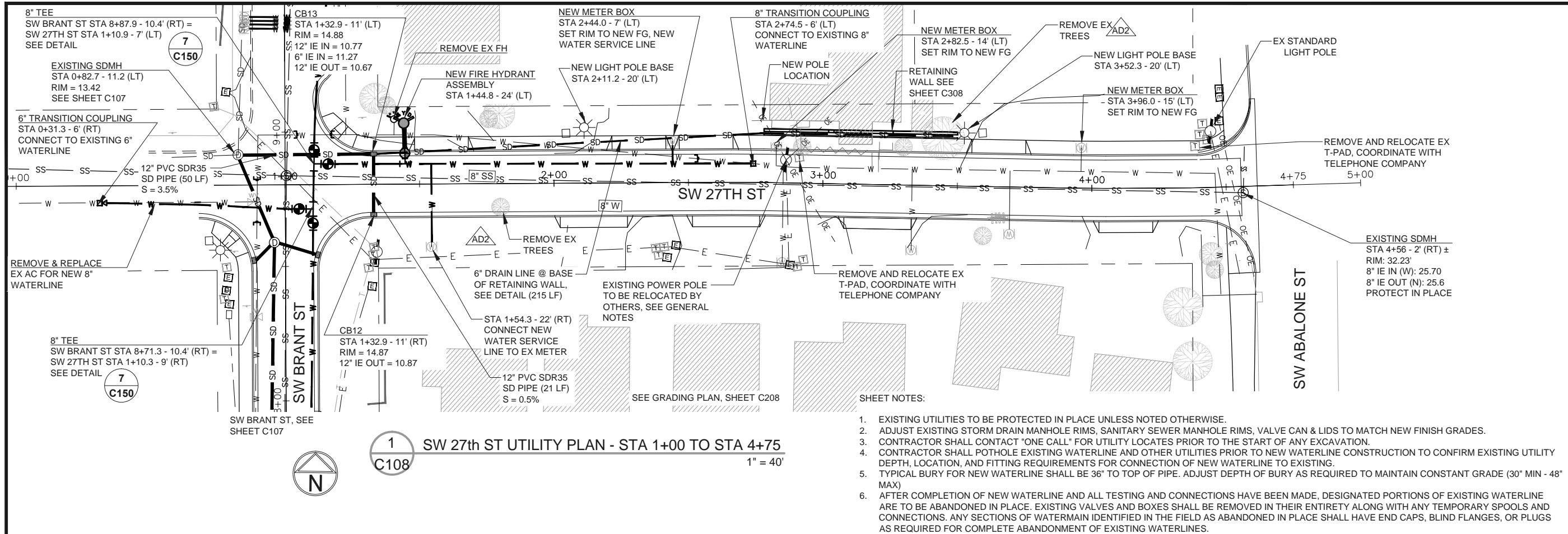
Project No: 2302-027

CITY OF NEWPORT
LINCOLN COUNTY, OREGON
SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS
UTILITY PLAN & PROFILE - SW BRANT ST -
STA 6+50 TO STA 11+75

C107

July 2015

DATE: 7/10/15 FILE: \\C:\Projects\2302\2302-027 Newport Abalone and 30th Roadway Improvements\Drawings\Abalone & 30th - DESIGN.dwg



REV.	DATE	DESCRIPTION	BY
1	7/2/2015	ADDENDUM #2 (AD2)	TW

Designed By:	Drawn By:	Checked By:
RWW	ALF	MEH

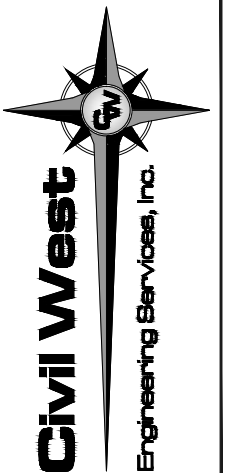
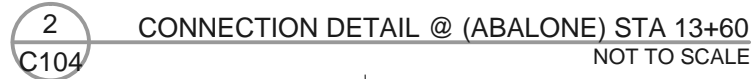
Project No: 2302-027

CITY OF NEWPORT
LINCOLN COUNTY, OREGON

SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

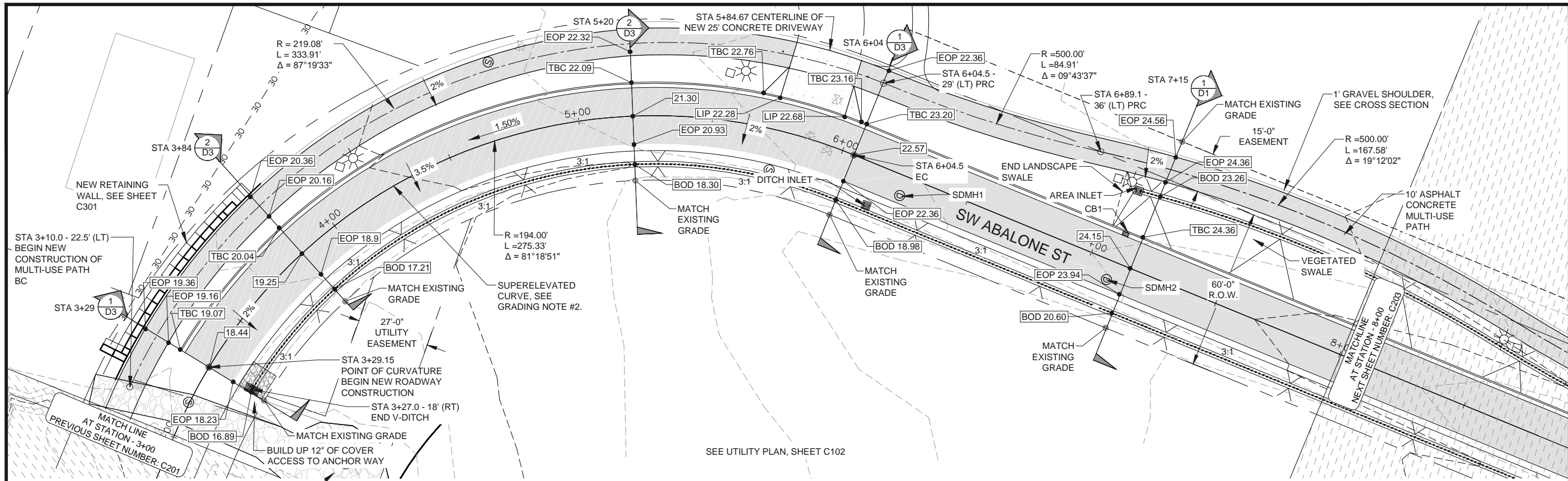
UTILITY PLAN & PROFILE - SW 27TH ST -
STA 1+00 TO STA 4+75

C108
July 2015



CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	WATER LINE CONNECTION DETAILS
Date	Sheet No.	C150
June 2015		

DATE: 6/15/15 FILE: C:\CW_Projects\2302 Newport Abalone and 30th Roadway Improvements\Drawings\Abalone & 30th - DESIGN.dwg



GRADING NOTES:

1. STA 3+29 TO STA 3+84 - GRADING SHALL TRANSITION GRADUALLY TO CREATE SMOOTH TRANSITION INTO SUPERELEVATED CURVE. CONSTRUCT STATIONS TO CORRESPOND WITH CROSS SECTIONS AS INDICATED, SEE SHEET D3.
2. STA 3+84 TO STA 5+20 - FULLY SUPERELEVATED CURVE, SEE CROSS SECTION, 2/D3.
3. STA 5+20 TO STA 6+04 - GRADING SHALL TRANSITION GRADUALLY TO CREATE SMOOTH TRANSITION OUT OF SUPERELEVATED CURVE. CONSTRUCT STATIONS TO CORRESPOND WITH CROSS SECTIONS AS INDICATED, SEE SHEET D3.
4. STA 6+04 TO STA 7+15 - GRADING SHALL TRANSITION GRADUALLY TO CREATE SMOOTH TRANSITION OUT OF SUPERELEVATED CURVE. ROADWAY CROSS SECTION AT STA 7+15 REPRESENTS TYPICAL CROSS SECTION FOR SW ABALONE ST. SEE DETAIL SHEETS.

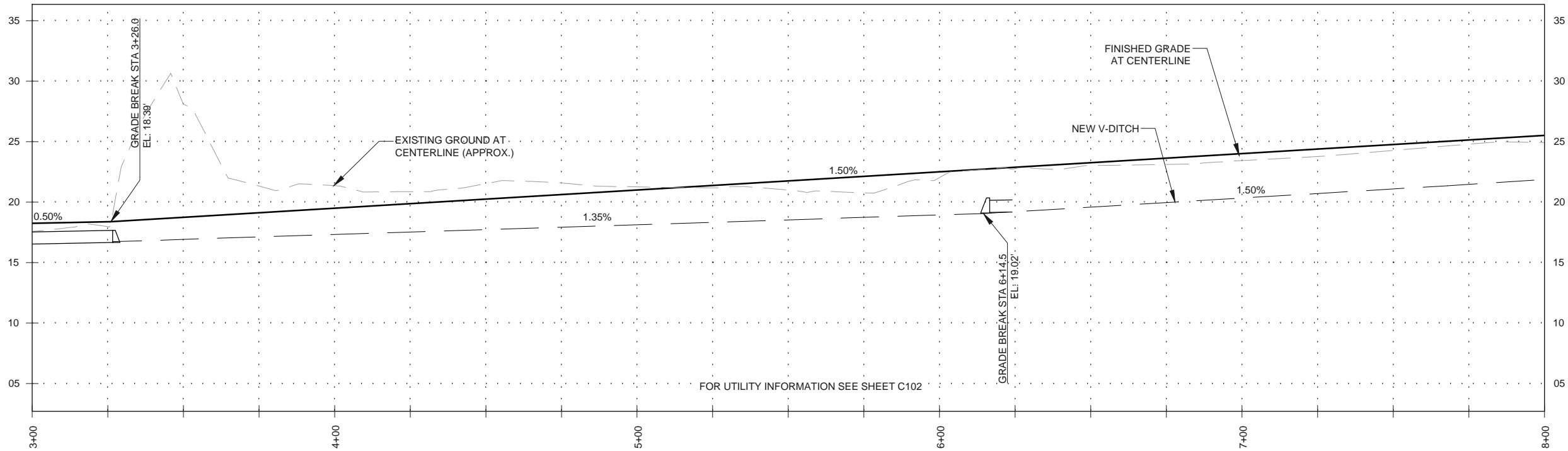


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C202

SW ABALONE ST GRADING PLAN - STA 3+00 TO STA 8+00
SCALE: 1"=40'

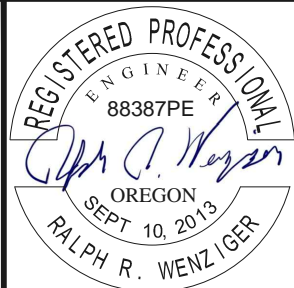
SHEET NOTES:

1. CONTRACTOR TO COORDINATE WITH PROPERTY OWNER(S) FOR REMOVAL & RELOCATION OF EXISTING MAILBOXES, FENCING, AND ALL OTHER FEATURES AND EQUIPMENT WITHIN CONSTRUCTION AREA.
2. ADJUST EXISTING STORM DRAIN MANHOLE RIMS, SANITARY SEWER MANHOLE RIMS, VALVE CAN & LIDS TO MATCH NEW FINISH GRADES.

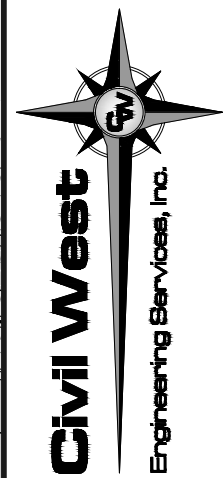


2
C202

SW ABALONE ST PROFILE - STA 3+00 TO STA 8+00
SCALE: HORZ 1"=40'
VERT 1"=10'



EXPIRATION DATE: 06/30/16



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Designed By:	Drawn By:	Checked By:
RWW	ALF	MEH

Project No: 2302-027

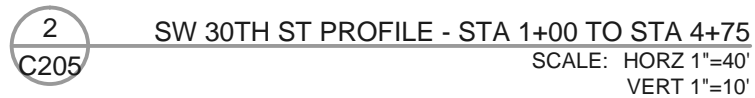
CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	GRADING PLAN & PROFILE - SW ABALONE ST - STA 3+00 TO STA 8+00
Sheet No: Date:	C202 June 2015	

The logo for Civil West Engineering Services, Inc. features a stylized compass rose with eight points. In the center of the compass is a circle containing the letters 'CW'. A long, thin, black vertical line extends downwards from the bottom of the compass rose. To the left of this line, the words 'Civil West' are written in a large, bold, black serif font. To the right of the line, the words 'Engineering Services, Inc.' are written in a smaller, black serif font.

2 SW ABALONE ST PROFILE - STA 8+00 TO STA 13+00
C203 SCALE: HORZ 1"=40'
VERT 1"=10'

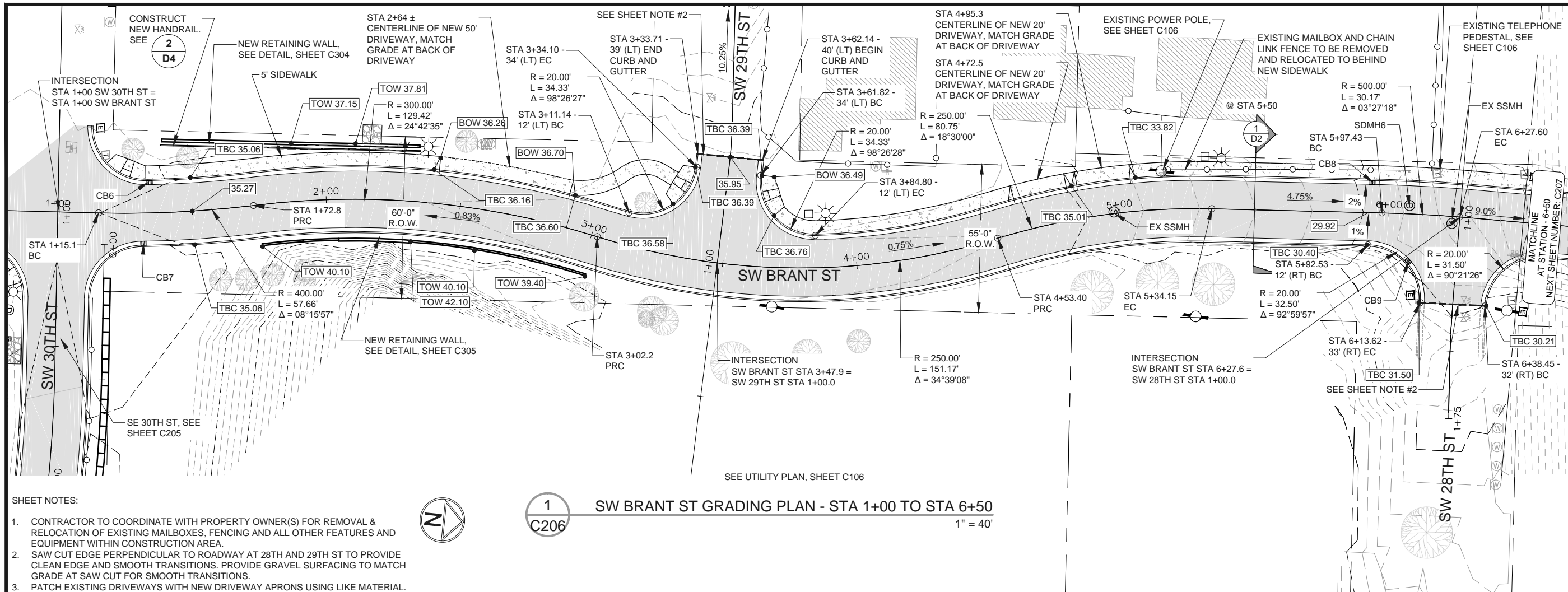


1. CONTRACTOR TO COORDINATE WITH PROPERTY OWNER(S) FOR REMOVAL & RELOCATION OF EXISTING MAILBOXES, FENCING, AND ALL OTHER FEATURES AND EQUIPMENT WITHIN CONSTRUCTION AREA.
2. ADJUST EXISTING STORM DRAIN MANHOLE RIMS, SANITARY SEWER MANHOLE RIMS, VALVE CAP & LIDS TO MATCH NEW FINISH GRADES.



Date _____ Sheet No. _____	CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	GRADING PLAN & PROFILE - SW 30TH ST - STA 1+00 TO STA 4+75	<div style="text-align: center;"> <h1>C205</h1> <p>June 2015</p> </div>
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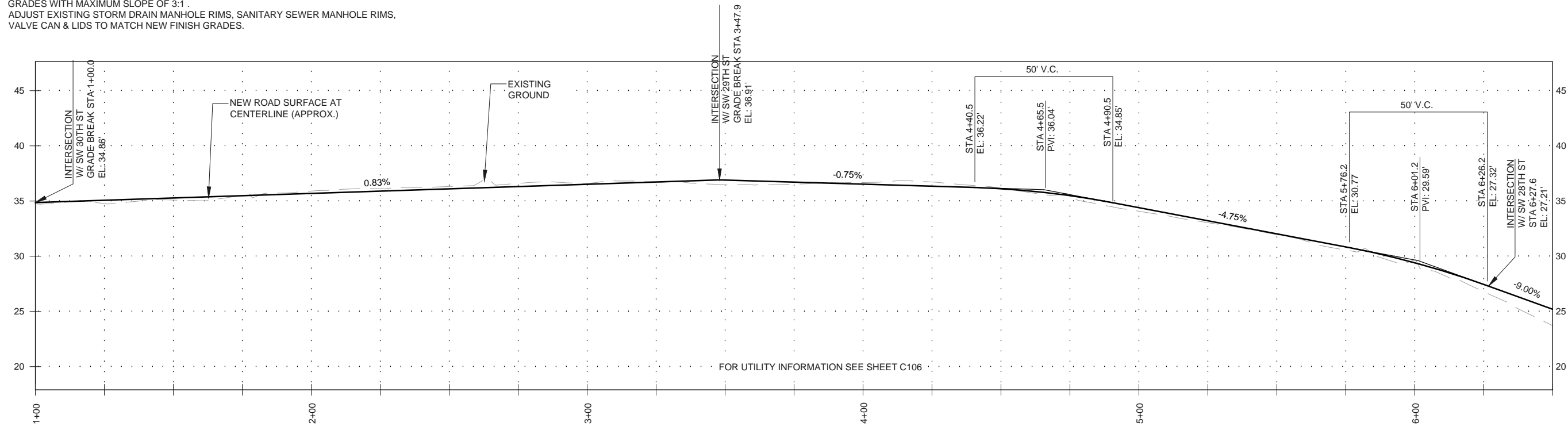
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- SHEET NOTES:
- CONTRACTOR TO COORDINATE WITH PROPERTY OWNER(S) FOR REMOVAL & RELOCATION OF EXISTING MAILBOXES, FENCING AND ALL OTHER FEATURES AND EQUIPMENT WITHIN CONSTRUCTION AREA.
 - SAW CUT EDGE PERPENDICULAR TO ROADWAY AT 28TH AND 29TH ST TO PROVIDE CLEAN EDGE AND SMOOTH TRANSITIONS. PROVIDE GRAVEL SURFACING TO MATCH GRADE AT SAW CUT FOR SMOOTH TRANSITIONS.
 - PATCH EXISTING DRIVEWAYS WITH NEW DRIVEWAY APRONS USING LIKE MATERIAL.
 - BACK OF NEW CURB AND NEW SIDEWALK SHALL BE CUT OR FILLED TO EXISTING GRADES WITH MAXIMUM SLOPE OF 3:1.
 - ADJUST EXISTING STORM DRAIN MANHOLE RIMS, SANITARY SEWER MANHOLE RIMS, VALVE CAN & LIDS TO MATCH NEW FINISH GRADES.



1 SW BRANT ST GRADING PLAN - STA 1+00 TO STA 6+50
C206 1" = 40'



2 SW BRANT ST PROFILE - STA 1+00 TO STA 6+50
C206 SCALE: HORZ 1"=40'
VERT 1"=10'

REGISTERED PROFESSIONAL ENGINEER
88387PE
OREGON
SEPT 10, 2013
RALPH R. WENZIGER
EXPIRATION DATE: 06/30/16

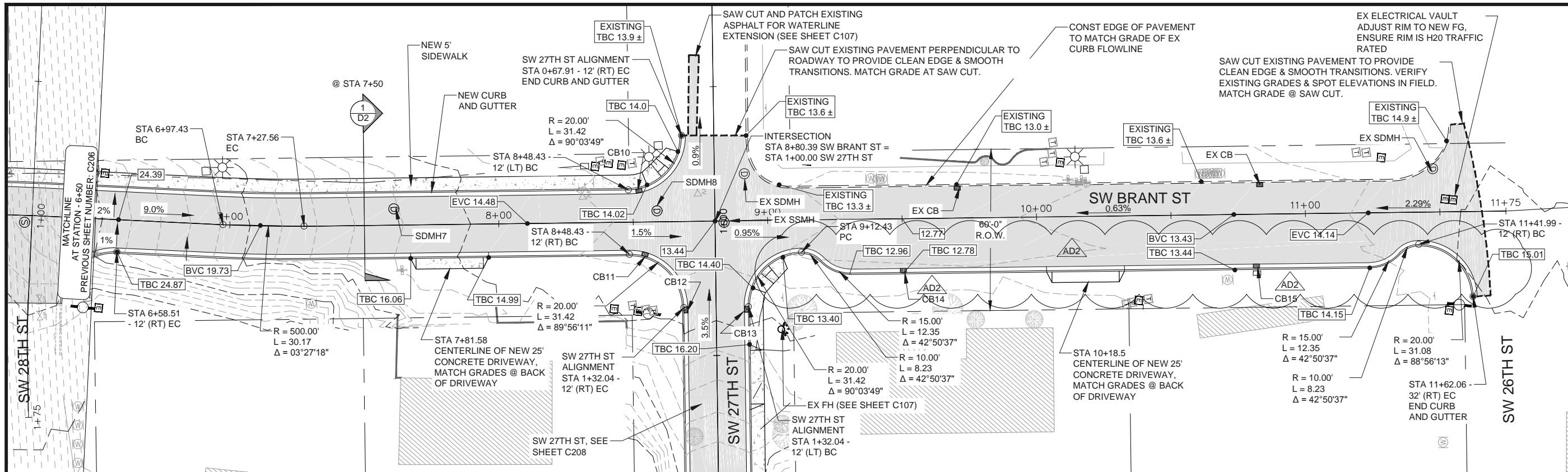
Civil West
Engineering Services, Inc.
609 SW Hubert Street
Newport, Oregon 97365
541-264-7040
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REV.	DATE	DESCRIPTION	BY

Designed By: RWW	Drawn By: ALF	Checked By: MEH
Project No: 2302-027		

CITY OF NEWPORT
LINCOLN COUNTY, OREGON
SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS
GRADING PLAN & PROFILE - SW BRANT ST
- STA 1+00 TO STA 6+50
C206
June 2015

DATE: 7/10/15 FILE: \\CW_Projects\2302 Newport\Drawings\2302-027 Newport Abalone and 30th Roadway\ImpsDrawings\2302-027 Newport Abalone & 30th - DESIGN.dwg



SEE UTILITY PLAN, SHEET C107



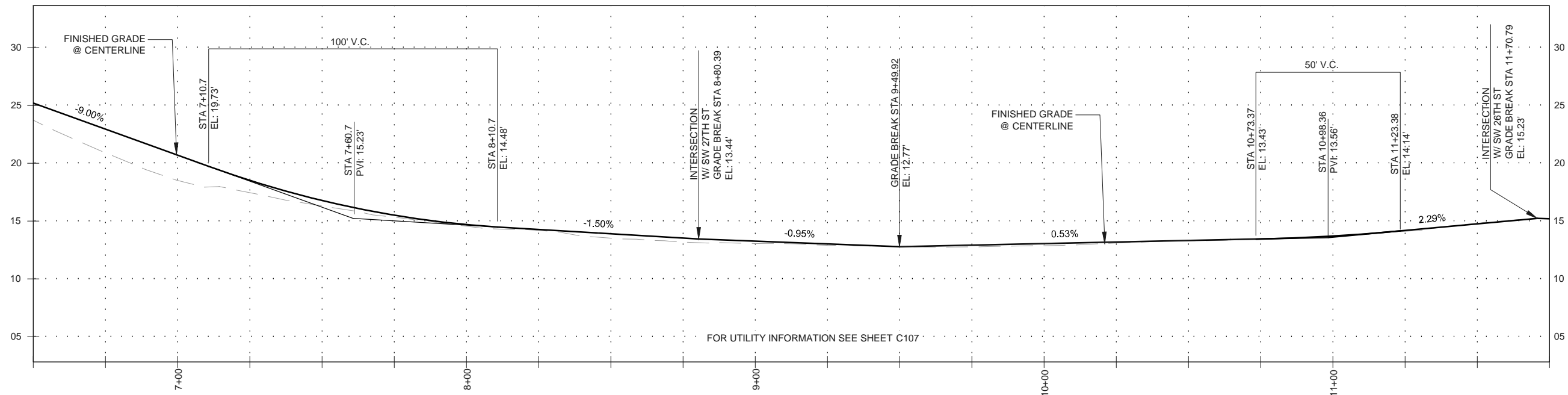
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C207

SW BRANT ST GRADING PLAN - STA 6+50 TO STA 11+75

1" = 40'

SHEET NOTES:

- CONTRACTOR TO COORDINATE WITH PROPERTY OWNER(S) FOR REMOVAL & RELOCATION OF EXISTING MAILBOXES, FENCING AND ALL OTHER FEATURES AND EQUIPMENT WITHIN CONSTRUCTION AREA.
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- ADJUST EXISTING STORM DRAIN MANHOLE RIMS, SANITARY SEWER MANHOLE RIMS, VALVE CAN & LIDS TO MATCH NEW FINISH GRADES.



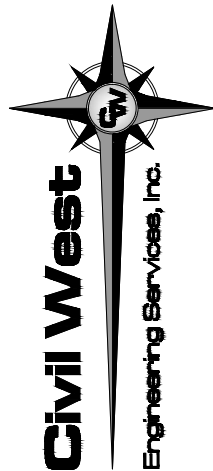
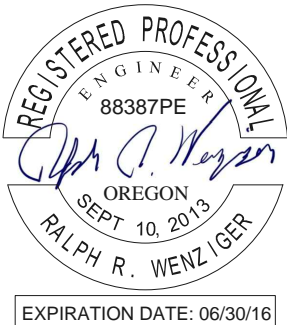
FOR UTILITY INFORMATION SEE SHEET C107

2
C207

SW BRANT ST PROFILE - STA 6+50 TO STA 11+75

SCALE: HORZ 1"=40'

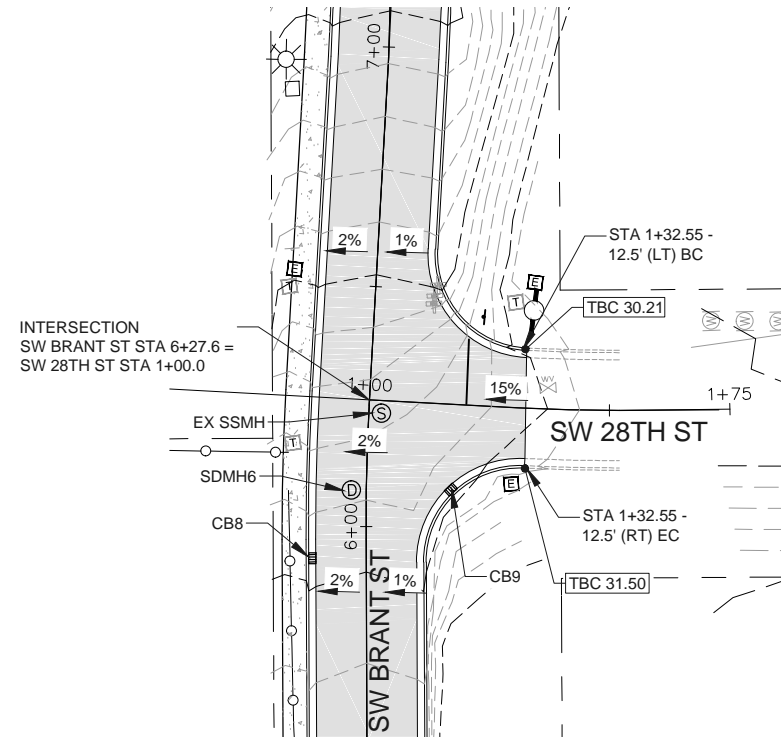
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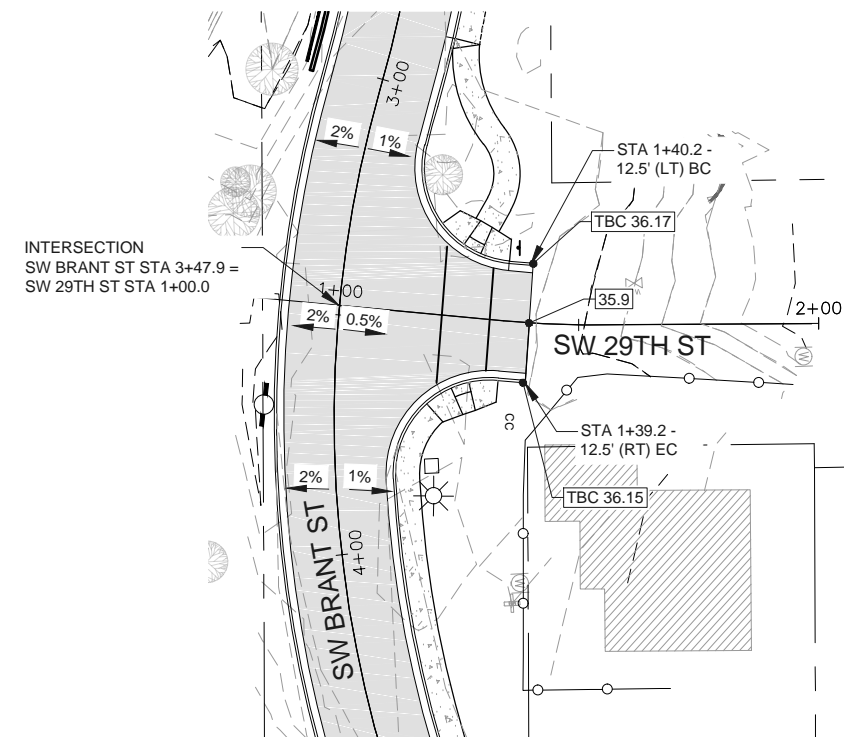
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REV.	DATE	DESCRIPTION	BY
1	7/2/2015	ADDENDUM #2 (AD2)	
Designed By: RWW Drawn By: ALF Checked By: MEH			
Project No: 2302-027			

CITY OF NEWPORT LINCOLN COUNTY, OREGON		SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	GRADING PLAN & PROFILE - SW BRANT ST - STA 6+50 TO STA 11+75
Date	Sheet No.		



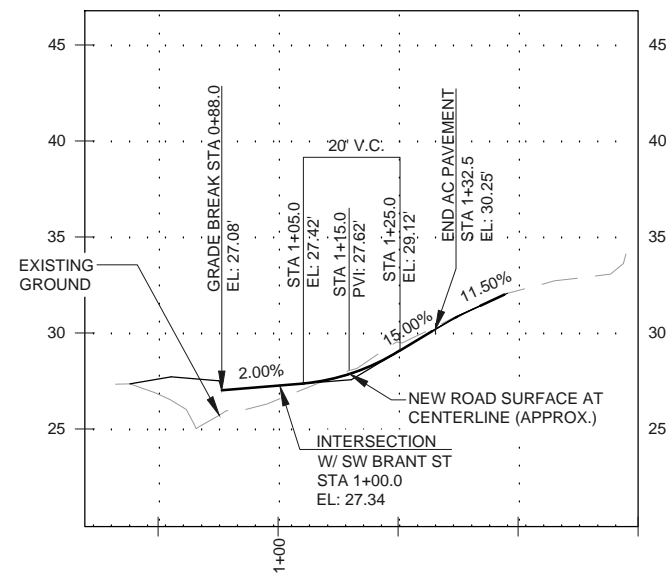
1 SW 28th ST GRADING PLAN - STA 1+00 TO STA 1+75
C209 1" = 40'



3 SW 29th ST GRADING PLAN - STA 1+00 TO STA 2+00
C209 1" = 40'

- SHEET NOTES:

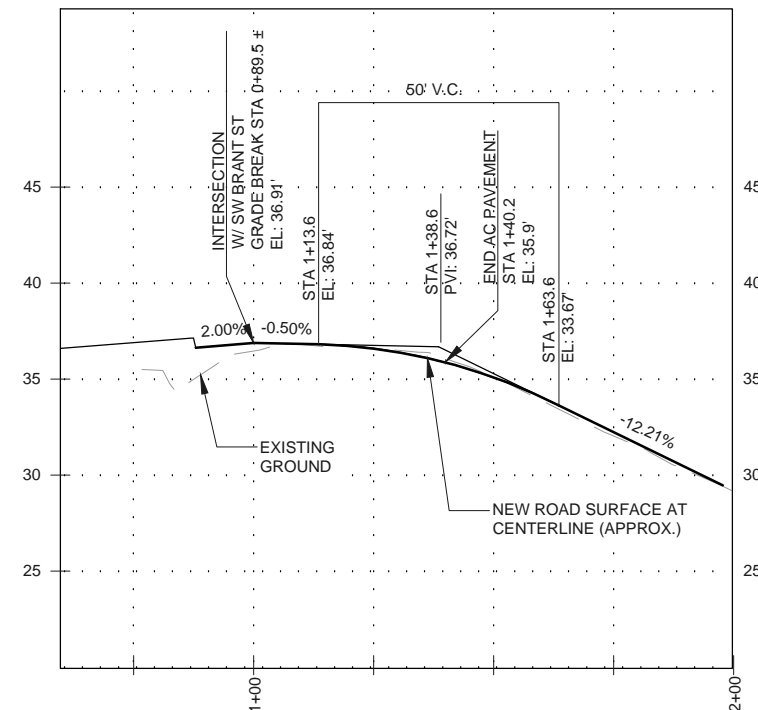
1. CONTRACTOR TO COORDINATE WITH PROPERTY OWNER(S) FOR REMOVAL & RELOCATION OF EXISTING MAILBOXES, FENCING AND ALL OTHER FEATURES AND EQUIPMENT WITHIN CONSTRUCTION AREA.
2. PATCH EXISTING DRIVEWAYS WITH NEW DRIVEWAY APRONS USING LIKE MATERIAL.
3. BACK OF NEW CURB AND NEW SIDEWALK SHALL BE CUT OR FILLED TO EXISTING GRADES WITH MAXIMUM SLOPE OF 3:1 .
4. ADJUST EXISTING STORM DRAIN MANHOLE RIMS, SANITARY SEWER MANHOLE RIMS, VALVE CAN & LIDS TO MATCH NEW FINISH GRADES.



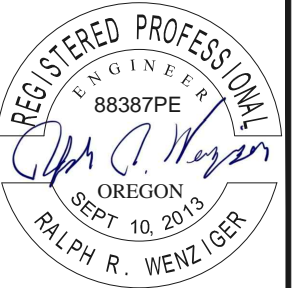
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C209

SW 28th ST PROFILE - STA 1+00 TO STA 1+75

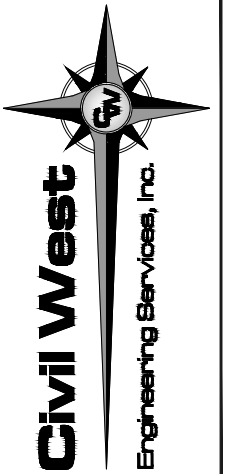
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VERT 1"=10'



4 SW 29th ST PROFILE - STA 1+00 TO STA 2+00
C209 SCALE: HORZ 1"=40'
VERT 1"=10'



EXPIRATION DATE: 06/30/16



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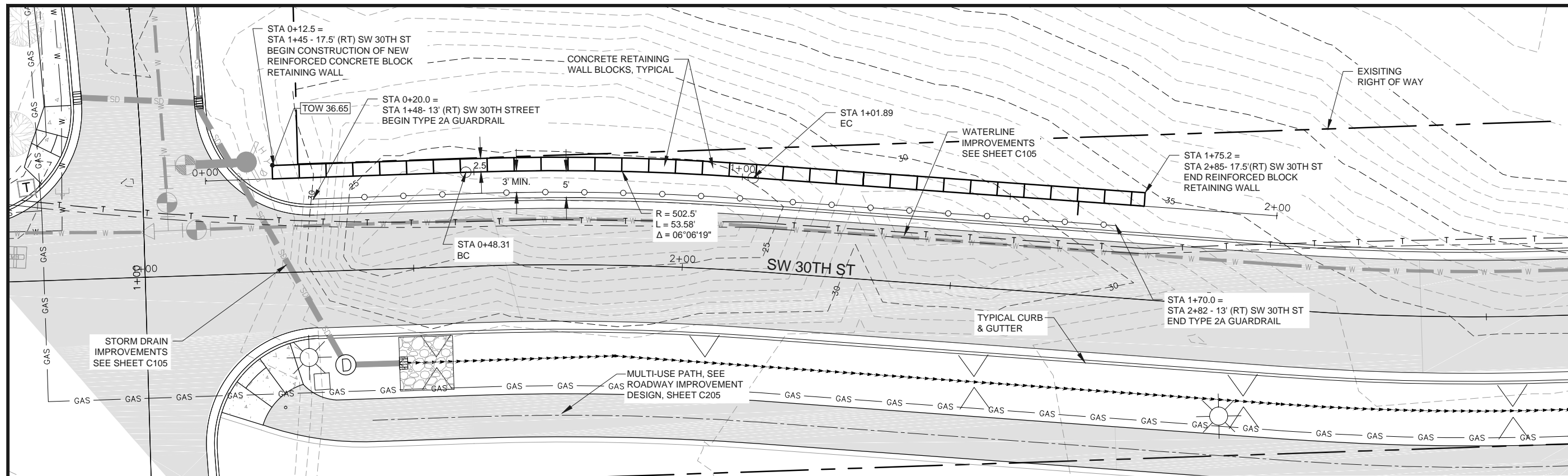
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Designed By:	RWW	Drawn By:	ALF	Checked By:	MEH
Project No: 2302-027					

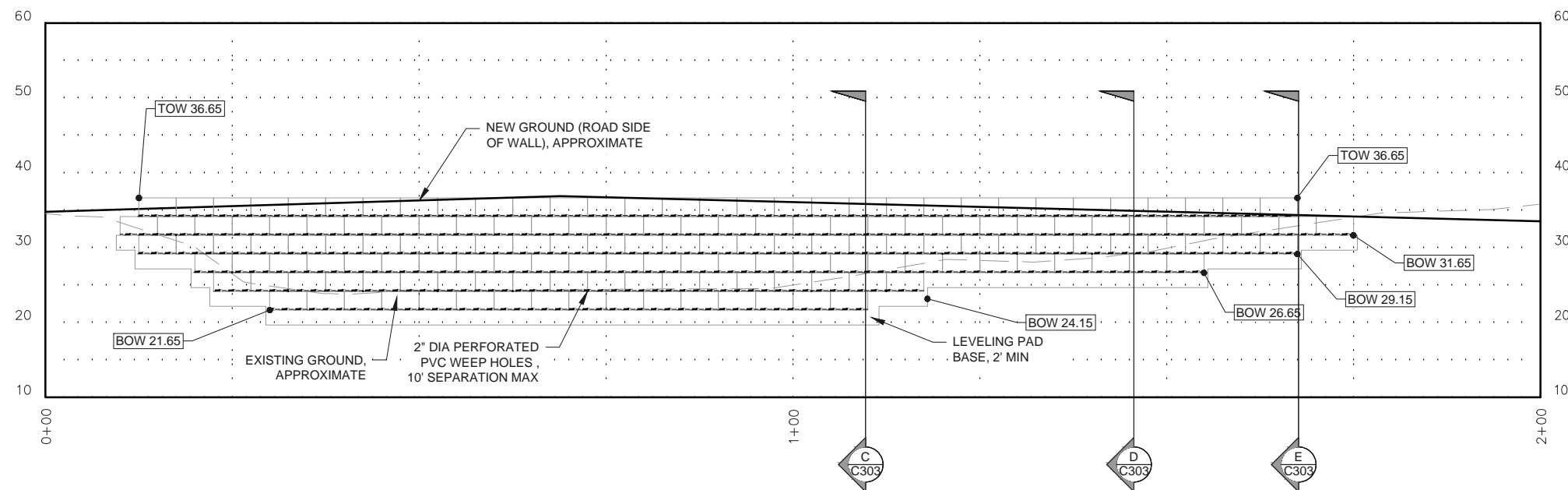
CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	GRADING PLAN & PROFILE - SW 28TH ST & SW 29TH ST
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C209

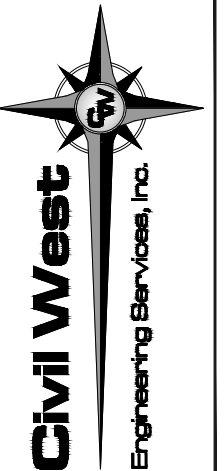
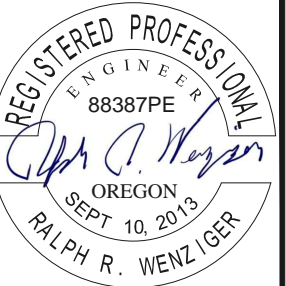
June 2015



RETAINING WALL PLAN - SW 30TH ST
SCALE: 1" = 20'



RETAINING WALL PROFILE - SW 30TH ST
SCALE: 1" = 20'

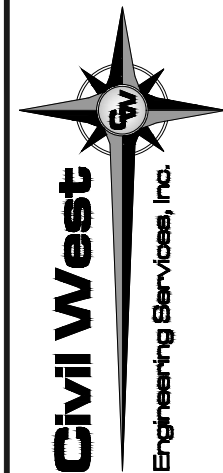
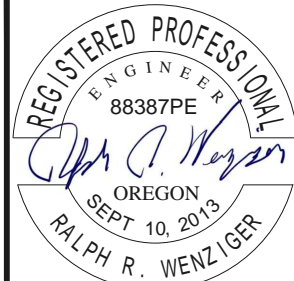
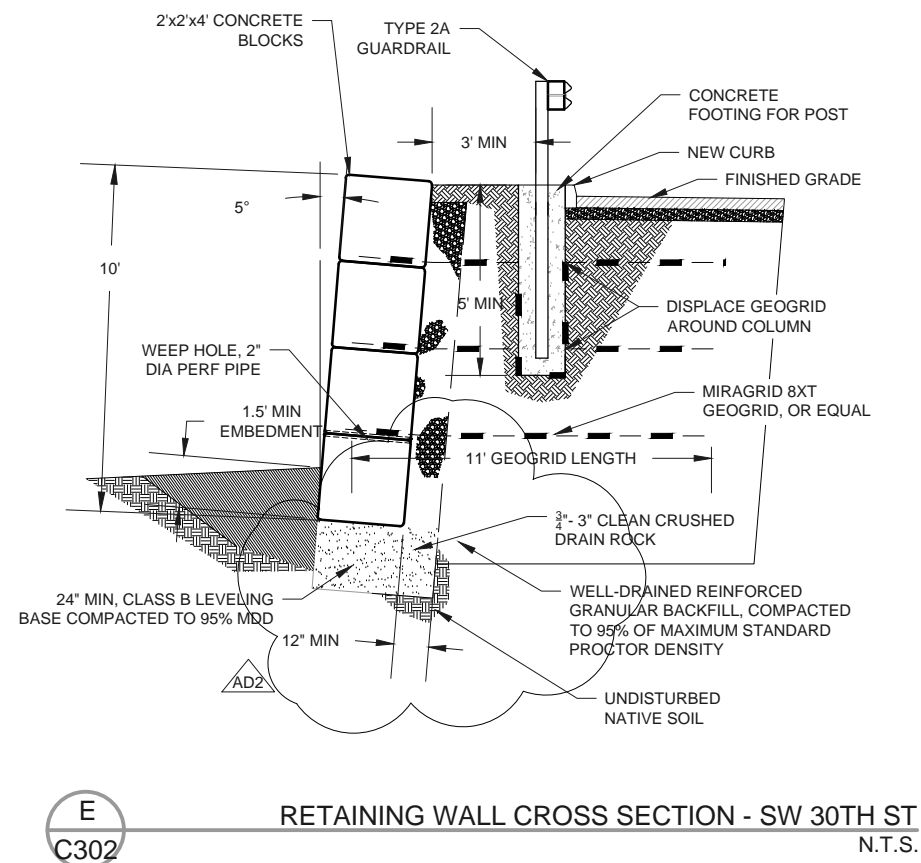
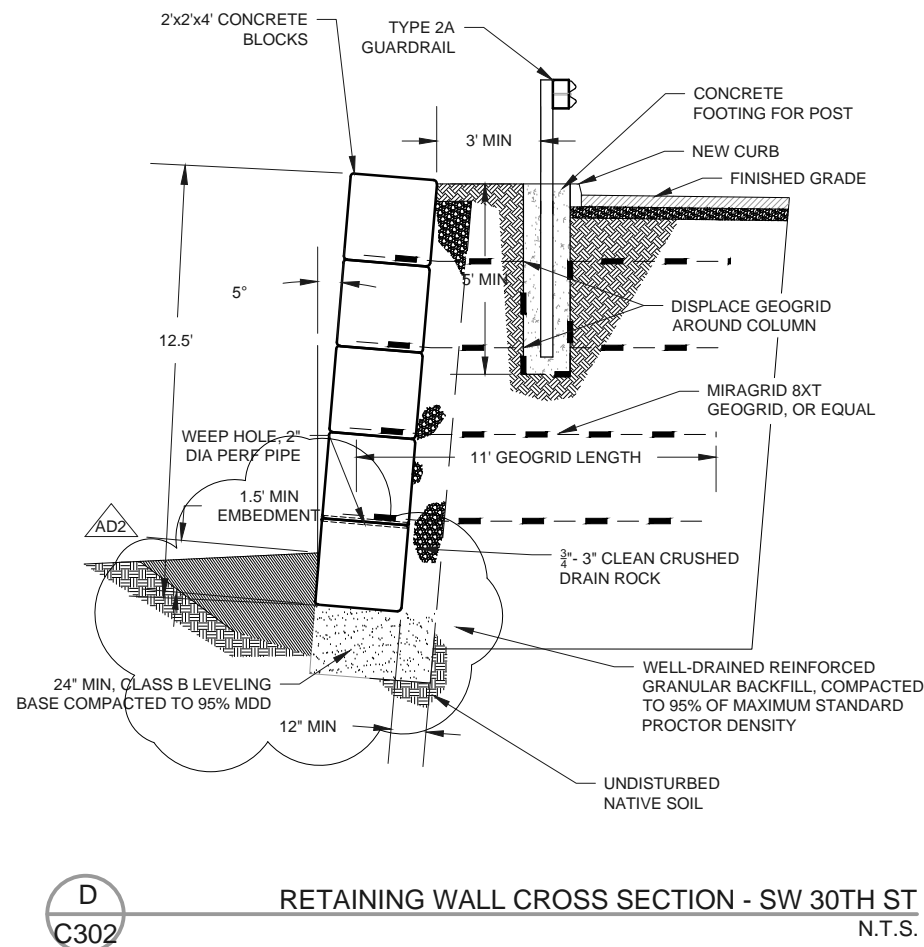
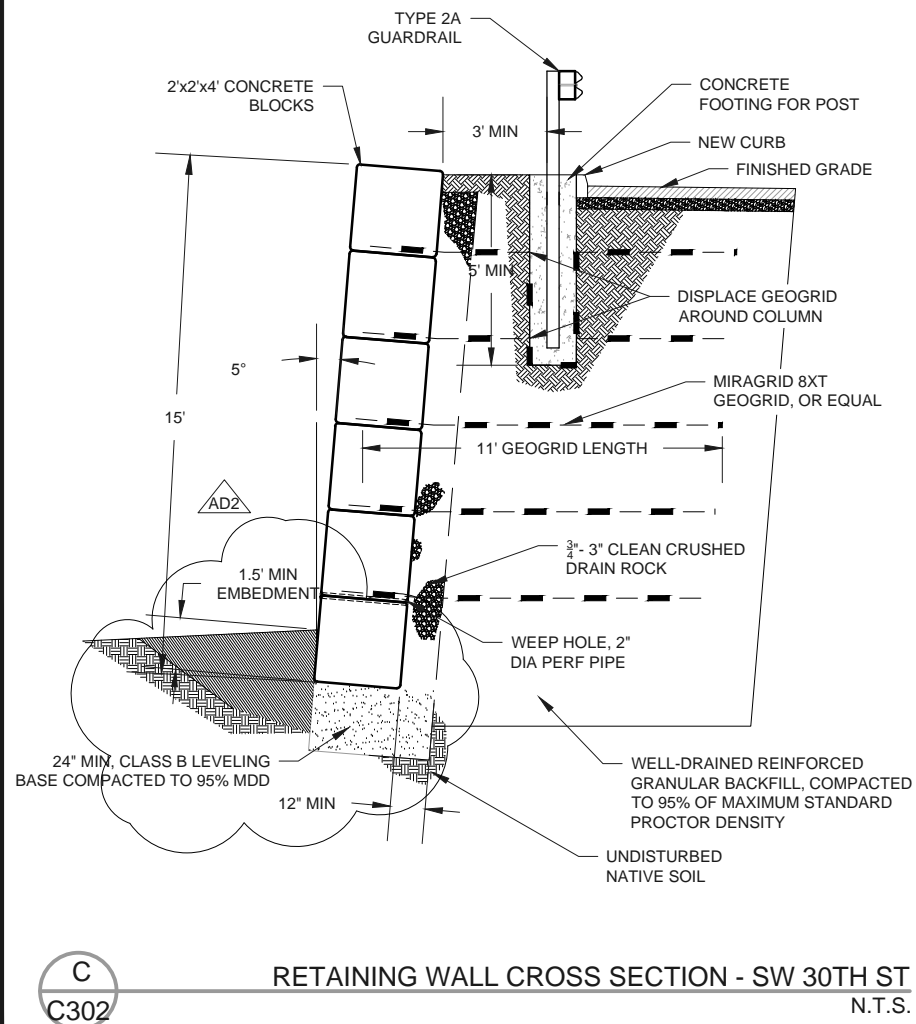
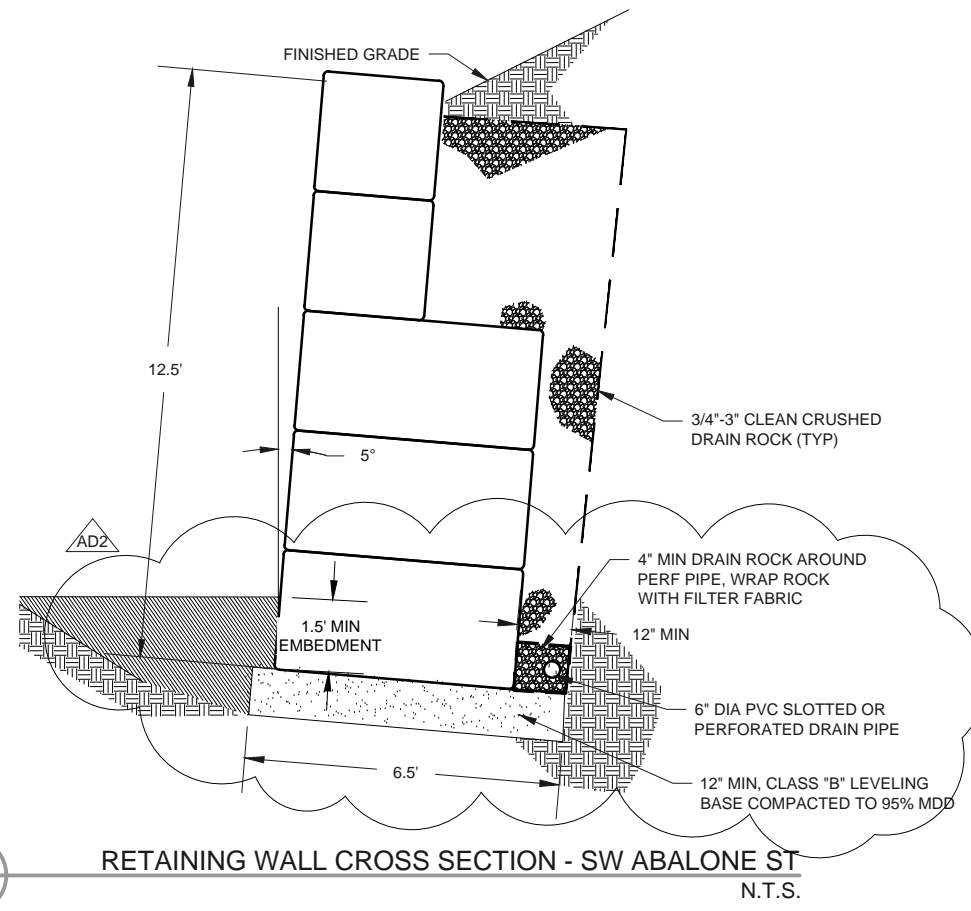
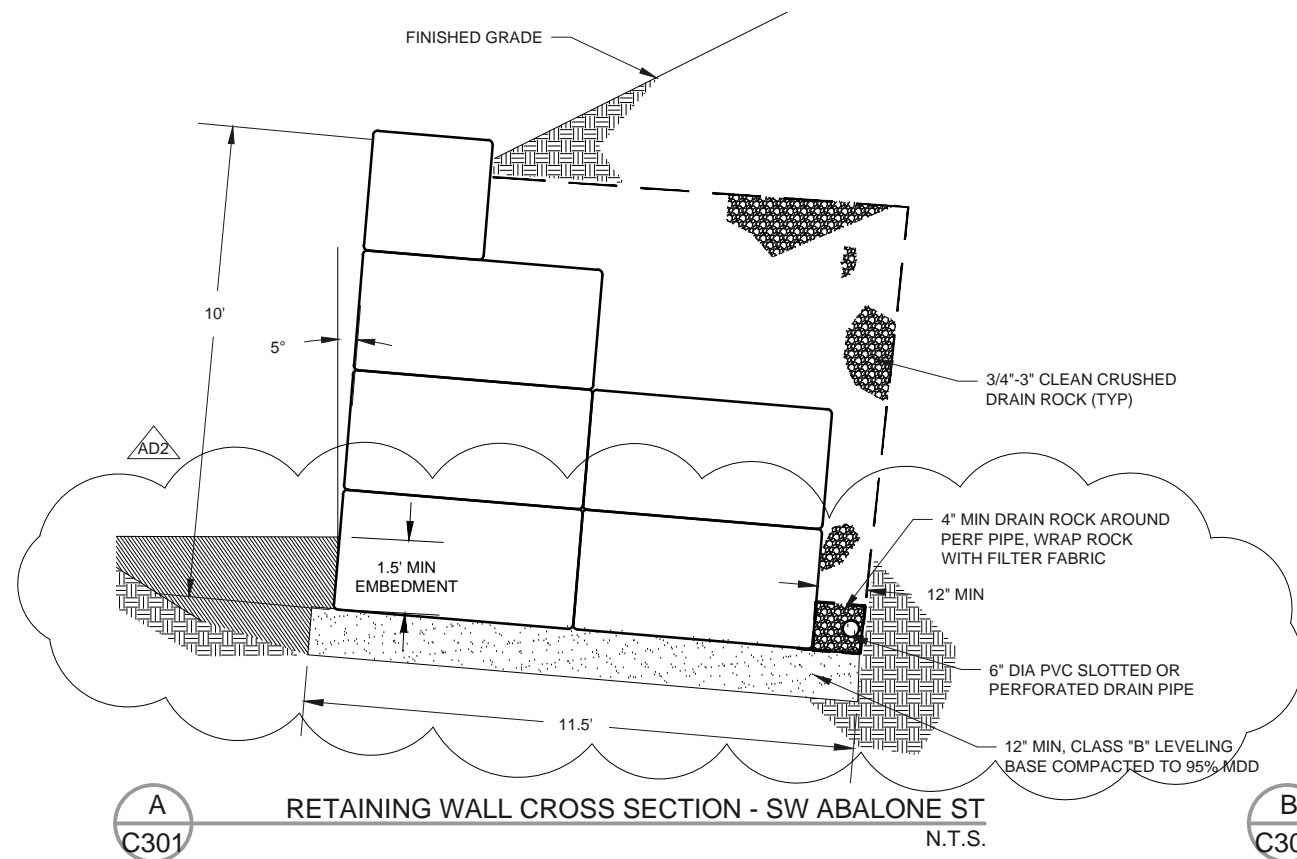


REV.	DATE	DESCRIPTION	BY

Designed By: RWIW Drawn By: ALF Checked By: MEH
 Project No.: 2302-027

Date _____ Sheet No. _____	CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	RETAINING WALL - SW 30TH ST
<div style="text-align: center;"> <h1>C302</h1> <h2>June 2015</h2> </div>			

DATE: 7/10/15 FILE: \\C:\CW_Projects\2302-027 Newport Abalone and 30th Roadway\ImpaDrawings\DWG\Abalone & 30th - Cross Sections & Ret. Walls.dwg



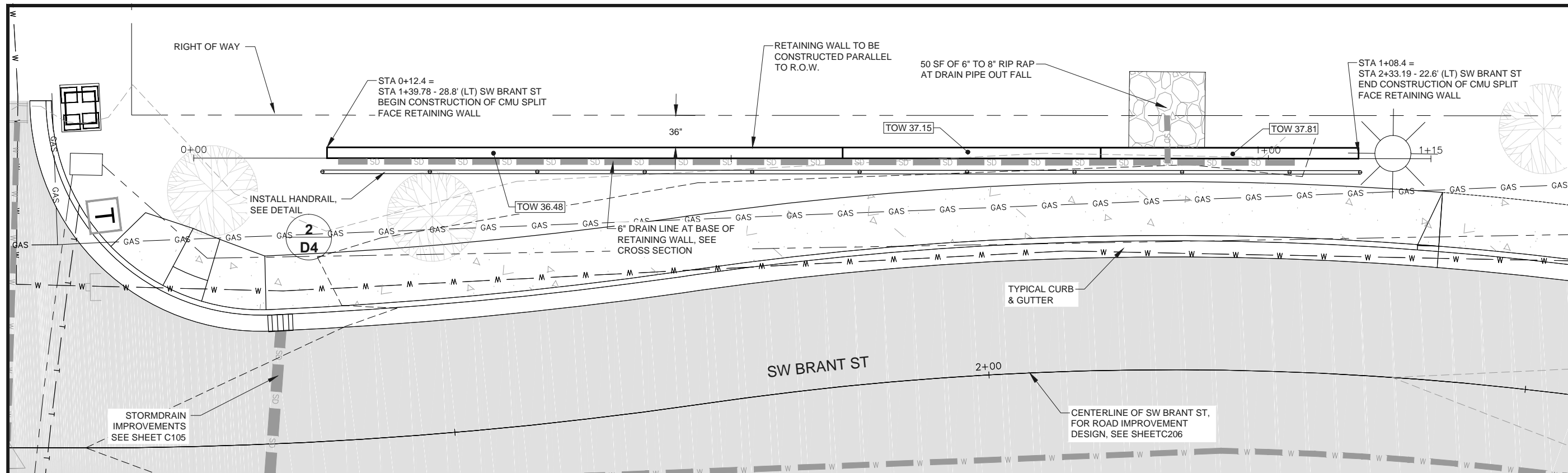
REV.	DATE	DESCRIPTION	BY
1	7/10/2015	ADDENDUM #2 (AD2)	

Designed By:	Drawn By:	Checked By:
RWW	ALF	MEH

Project No: 2302-027

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	RETAINING WALL - ABALONE & 30TH ST CROSS SECTIONS
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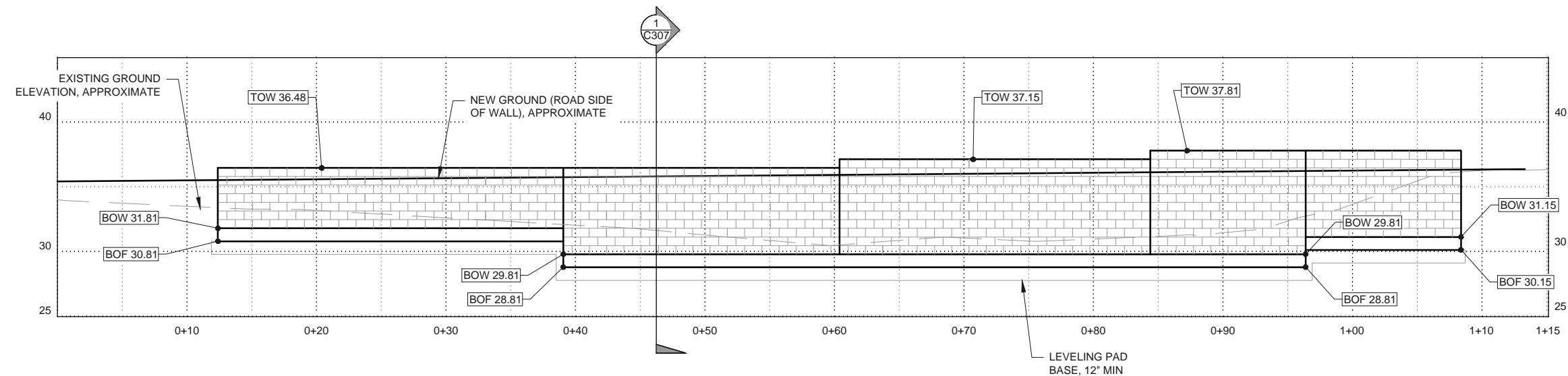
Sheet No:	C303
Date:	July 2015



1
C304

RETAINING WALL PLAN - SW BRANT ST (WEST SIDE)

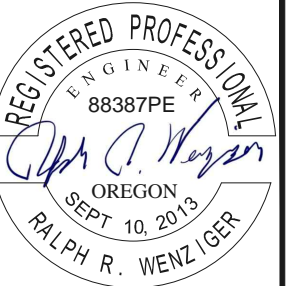
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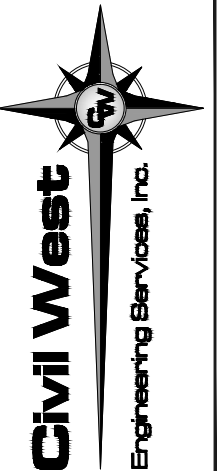
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C304

RETAINING WALL PLAN - SW BRANT ST (WEST SIDE)

SCALE: 1" = 10'



EXPIRATION DATE: 06/30/16



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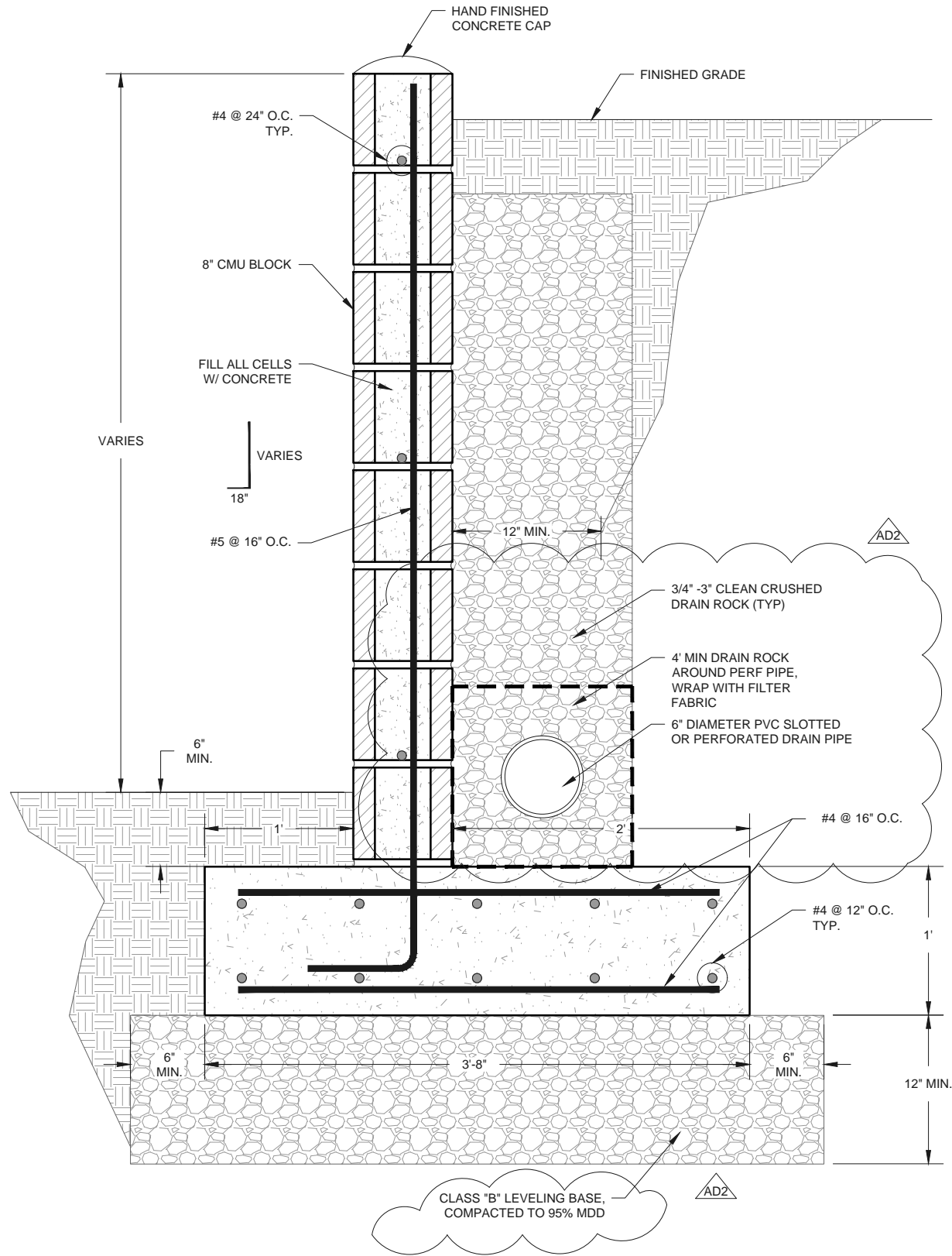
REV.	DATE	DESCRIPTION	BY

Designed By:	RWWV	Drawn By:	ALF	Checked By:	MEH
Project No: 2302-027					

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	RETAINING WALL - BRANT ST (WEST SIDE)
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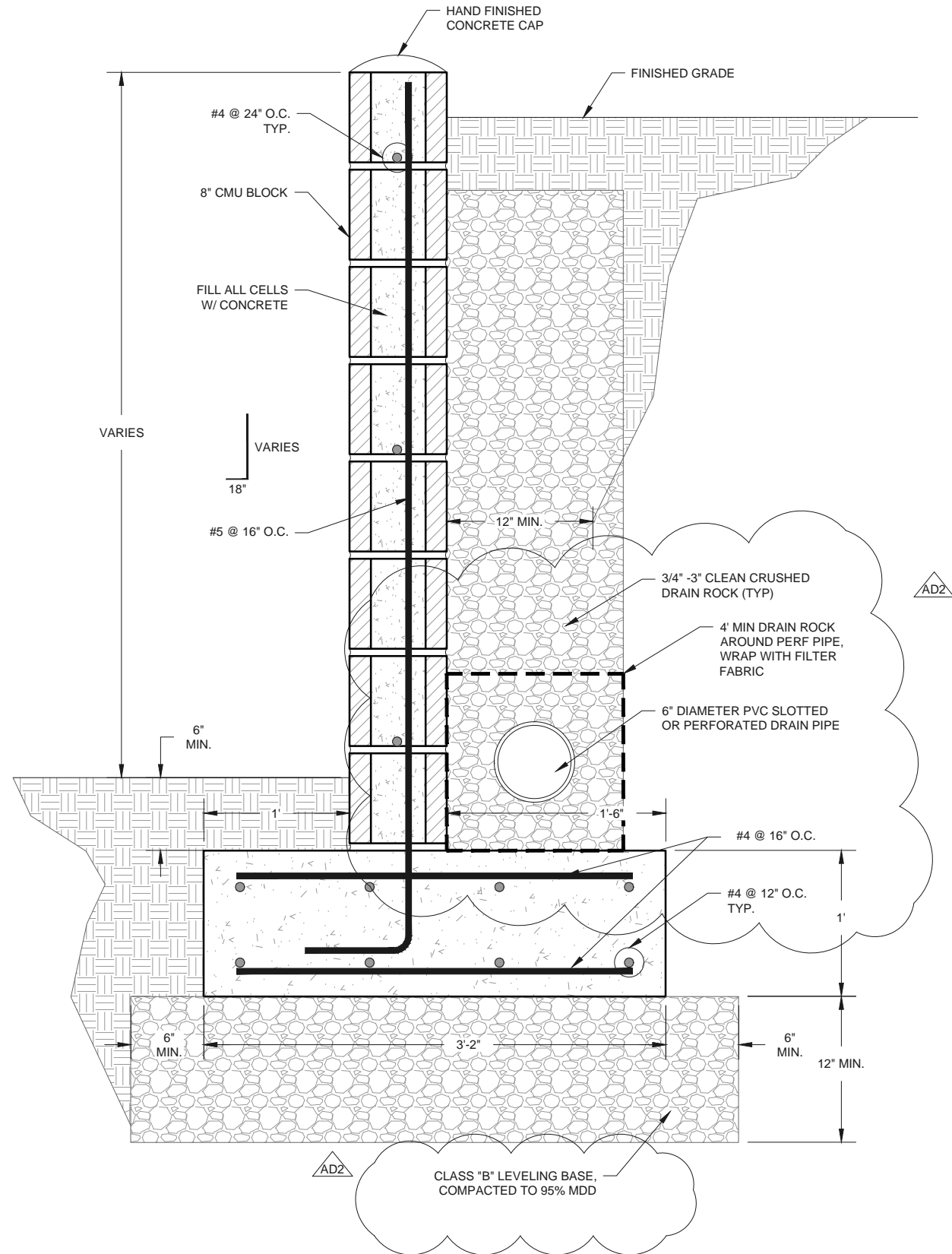
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Date	June 2015

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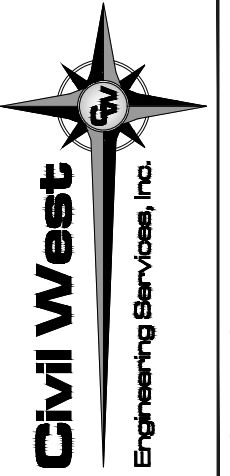
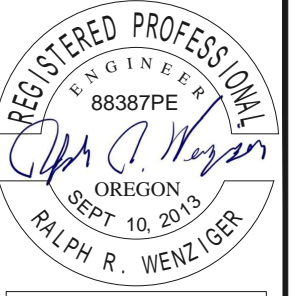
1
C304

CROSS SECTION - SW BRANT ST RETAINING WALL (WEST SIDE)
SCALE 1" = 1'



2
C306

CROSS SECTION - SW 27TH ST RETAINING WALL (NORTH SIDE)
SCALE 1" = 1'



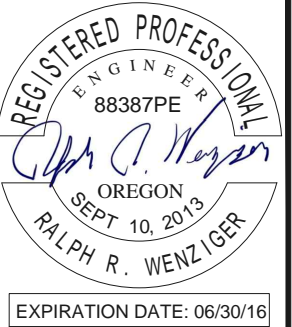
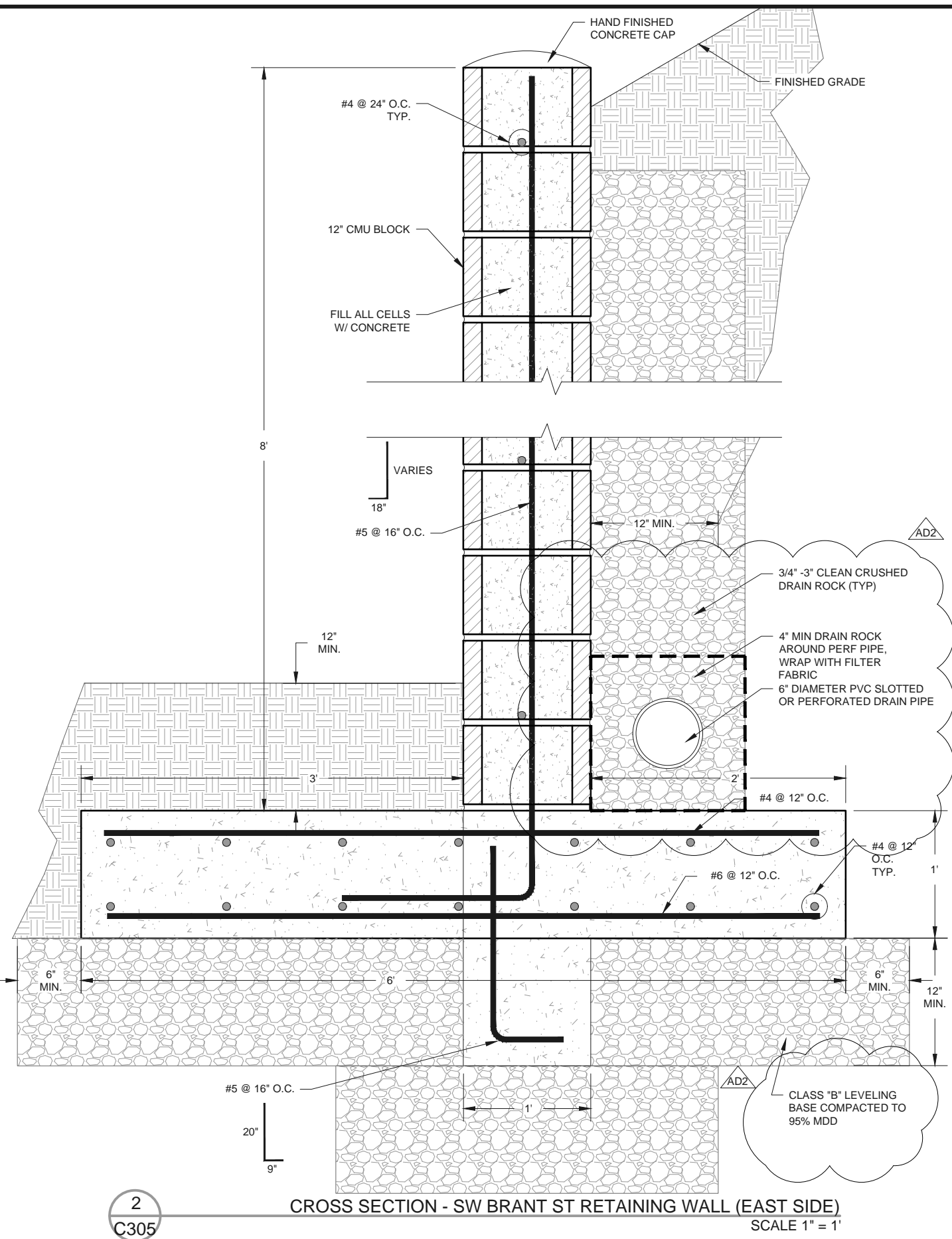
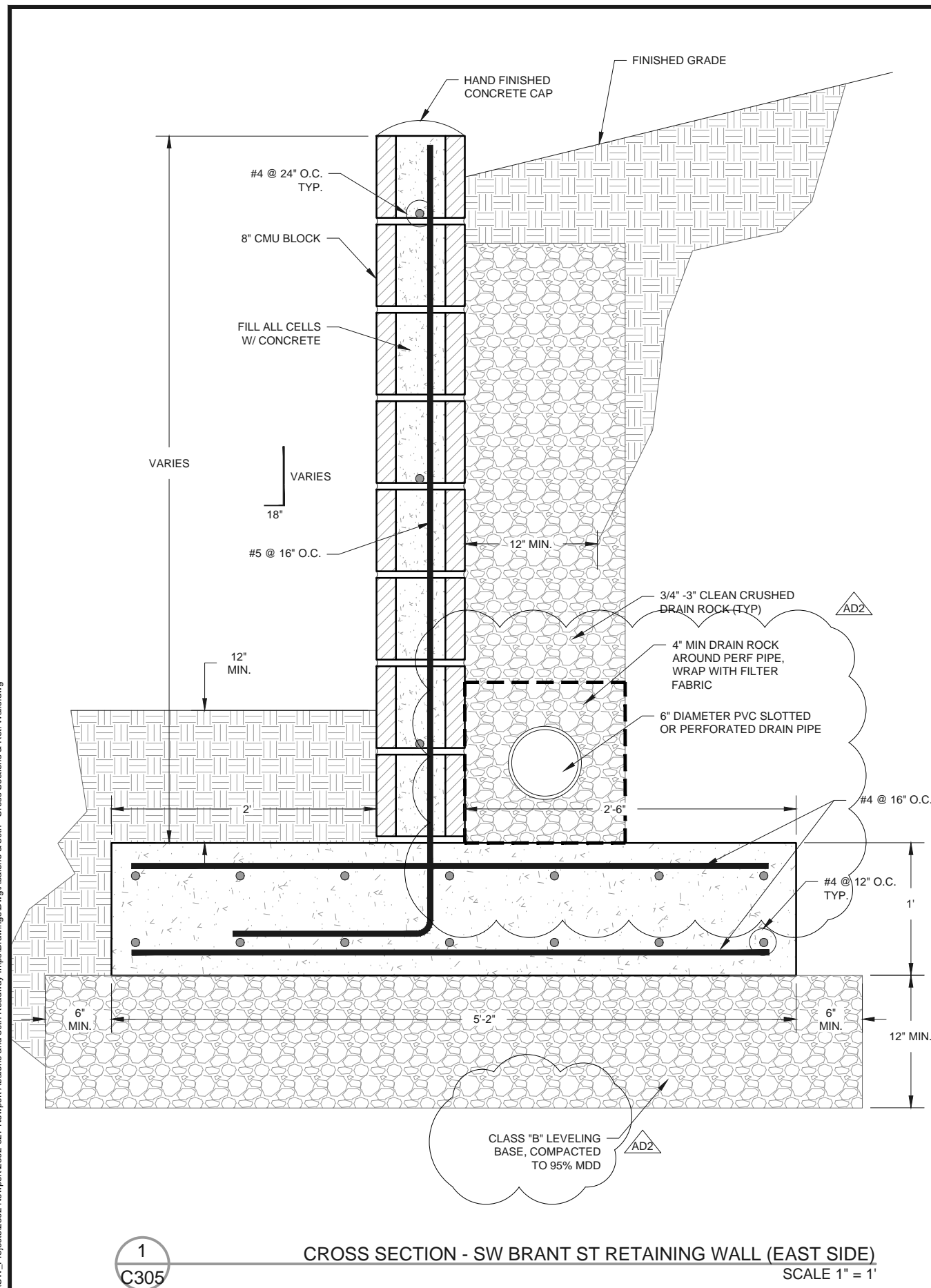
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1	7/10/2015	ADDENDUM #2 (AD2)	

Designed By:	Drawn By:	Checked By:
RWW	ALF	MEH

Project No: 2302-027

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	RETAINING WALL - SW BRANT ST & SW 27TH ST CROSS SECTIONS
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Sheet No:	C307
Date:	July 2015



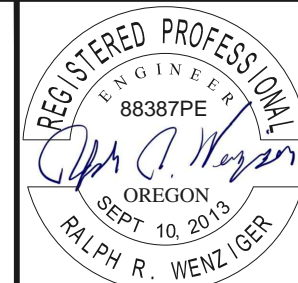
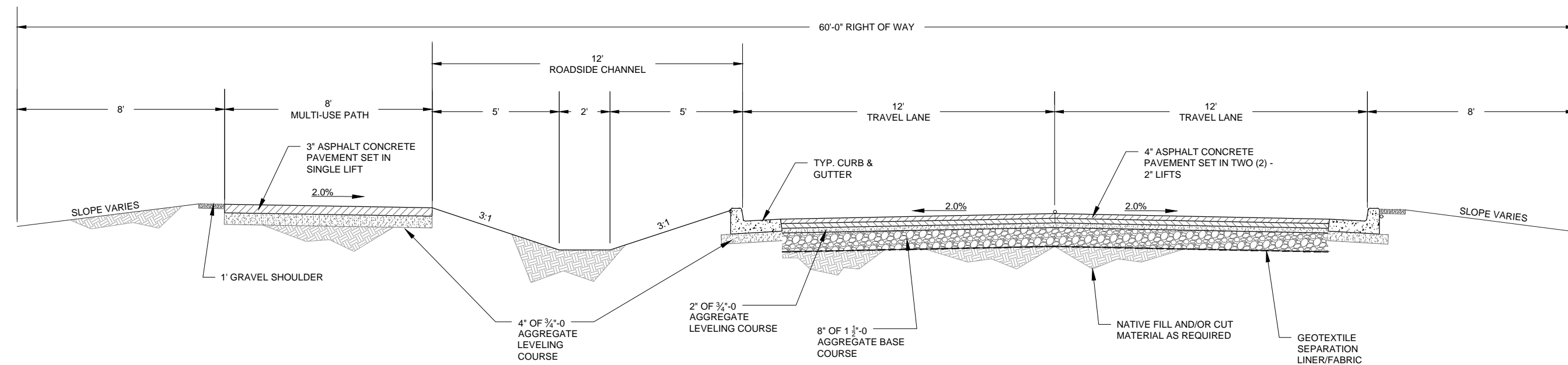
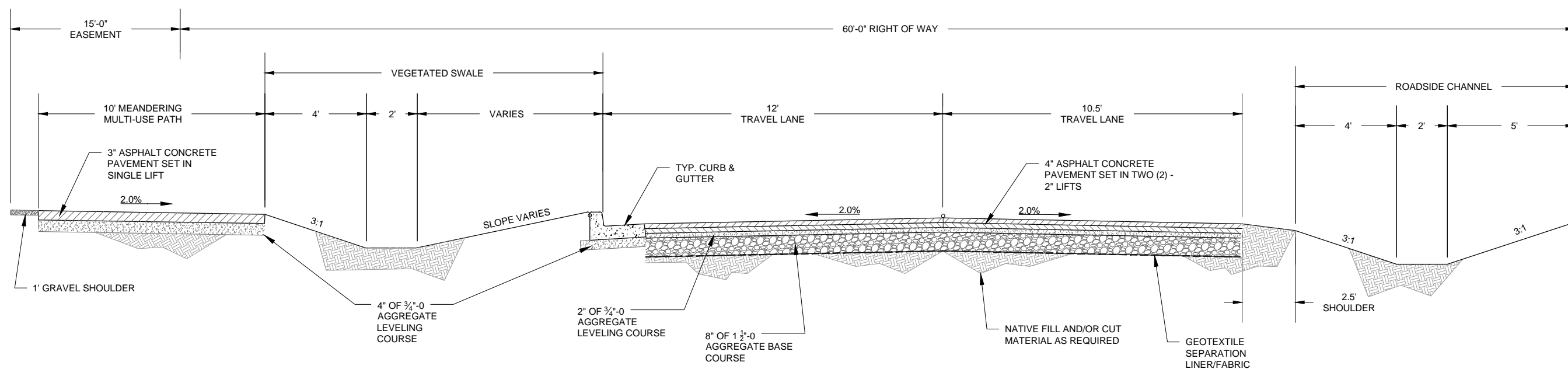
341-204-7040
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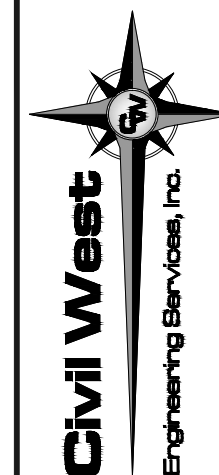
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1	7/10/2015	ADDENDUM #2 (AD2)	

Designed By:	RWW	Drawn By:	ALF	Checked By:	MEH
Project No: 2302-027					

Date	Sheet No.	CITY OF NEWPORT LINCOLN COUNTY, OREGON
		SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS
		RETAINING WALL - SW BRANT ST CROSS SECTIONS (EAST SIDE)



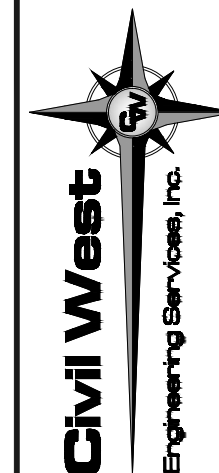
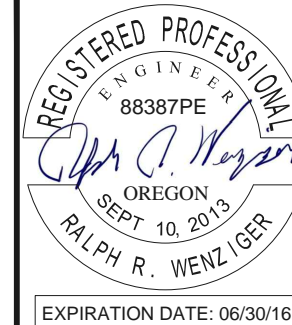
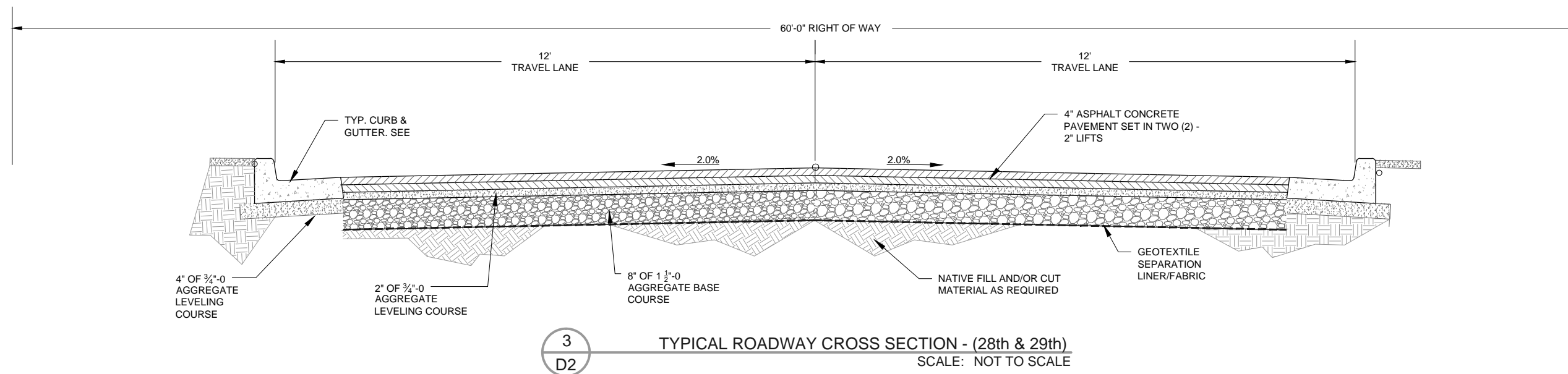
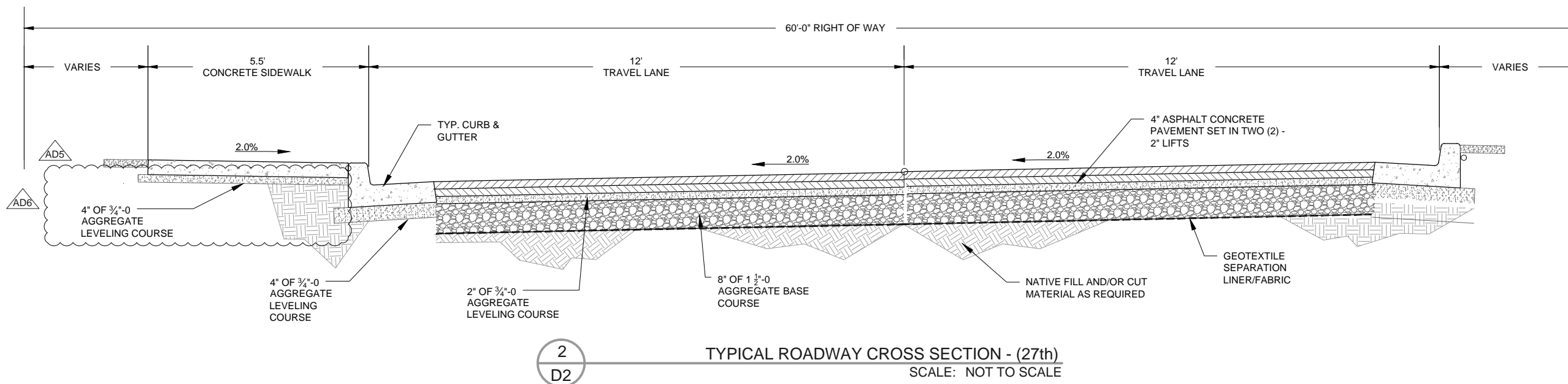
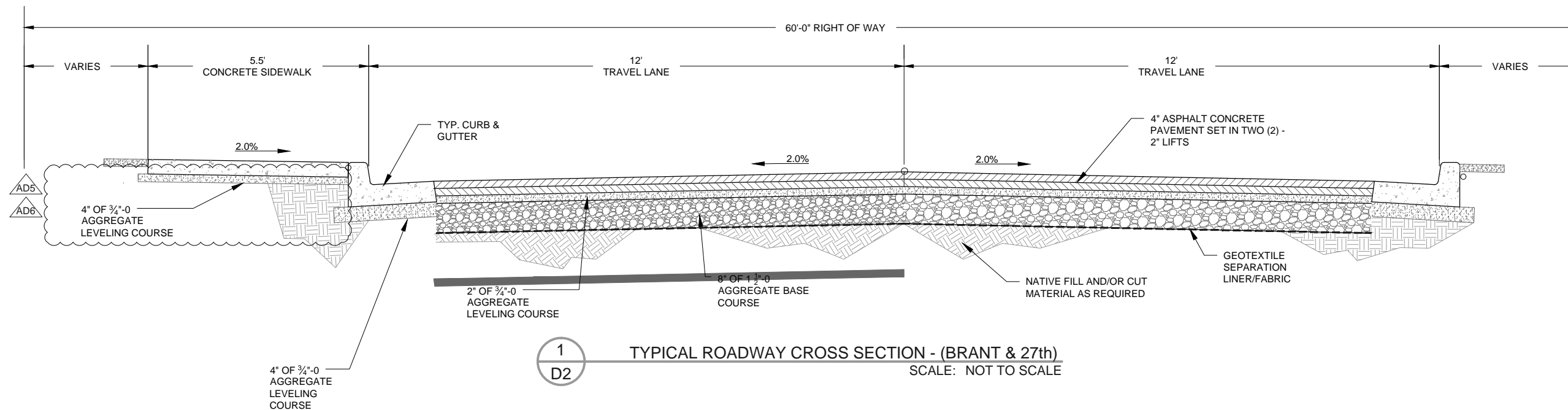
EXPIRATION DATE: 06/30/16

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CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	ROADWAY CROSS SECTIONS - ABALONE ST & 30TH ST
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Sheet No.	D1
Date	June 2015

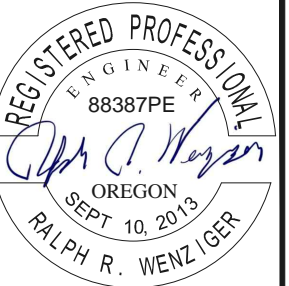
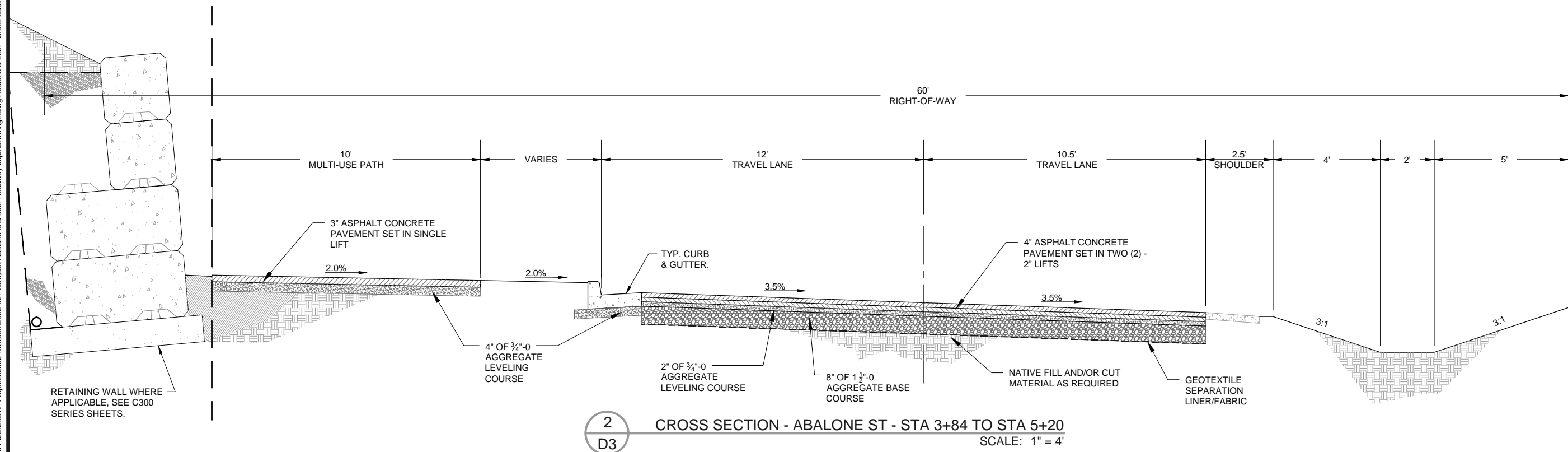
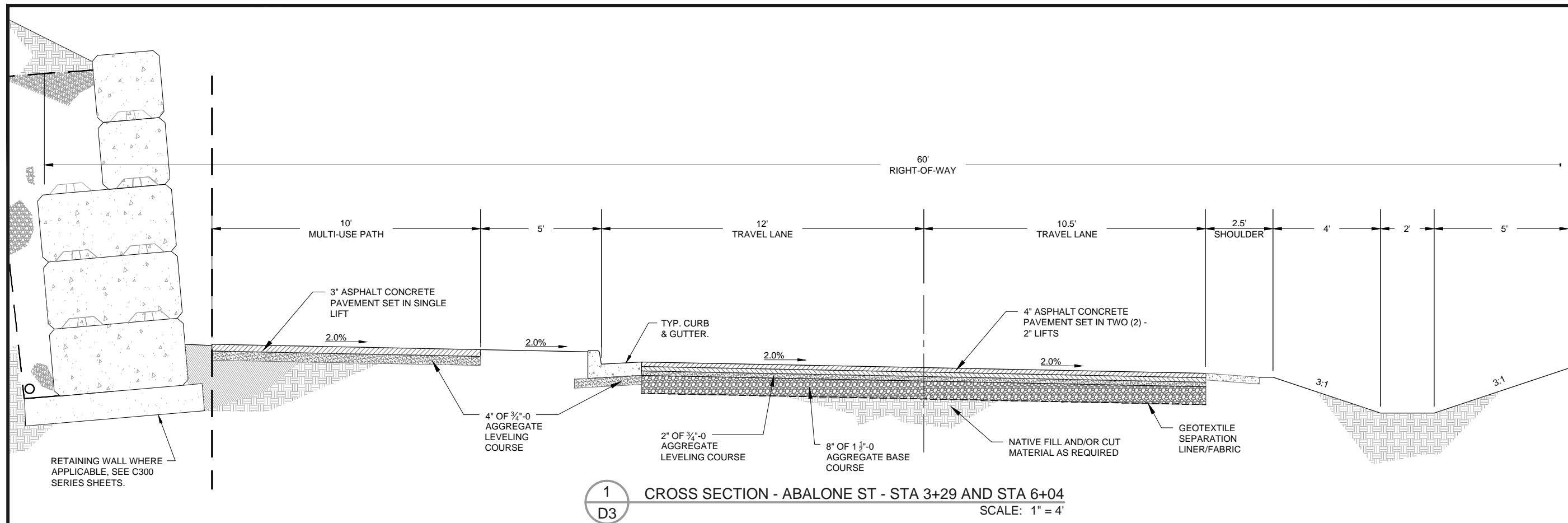
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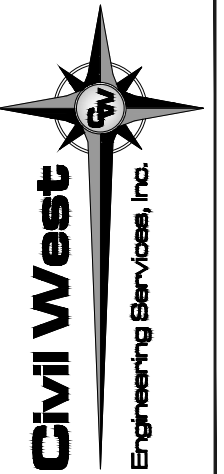
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REV.	DATE	DESCRIPTION	BY
1	7/16/15	ADDENDUM #5 (AD5)	RRW
2	7/17/15	ADDENDUM #6 (AD6)	RRW
Checked By: MEH			
Drawn By: ALF			
Designed By: RRW			
Project No: 2302-027			

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	ROADWAY CROSS SECTIONS - BRANT & 27th ST, 28th & 29th ST
Sheet No: D2	Date: July 2015	



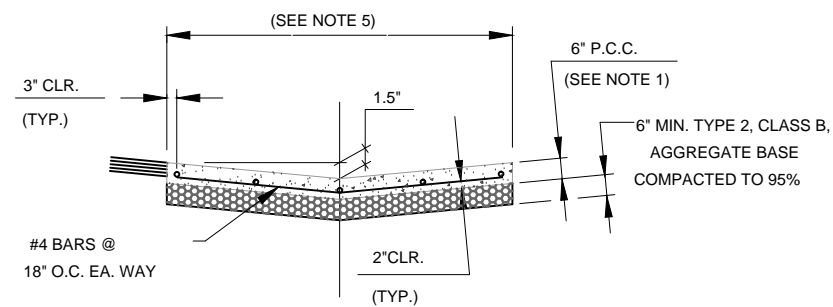
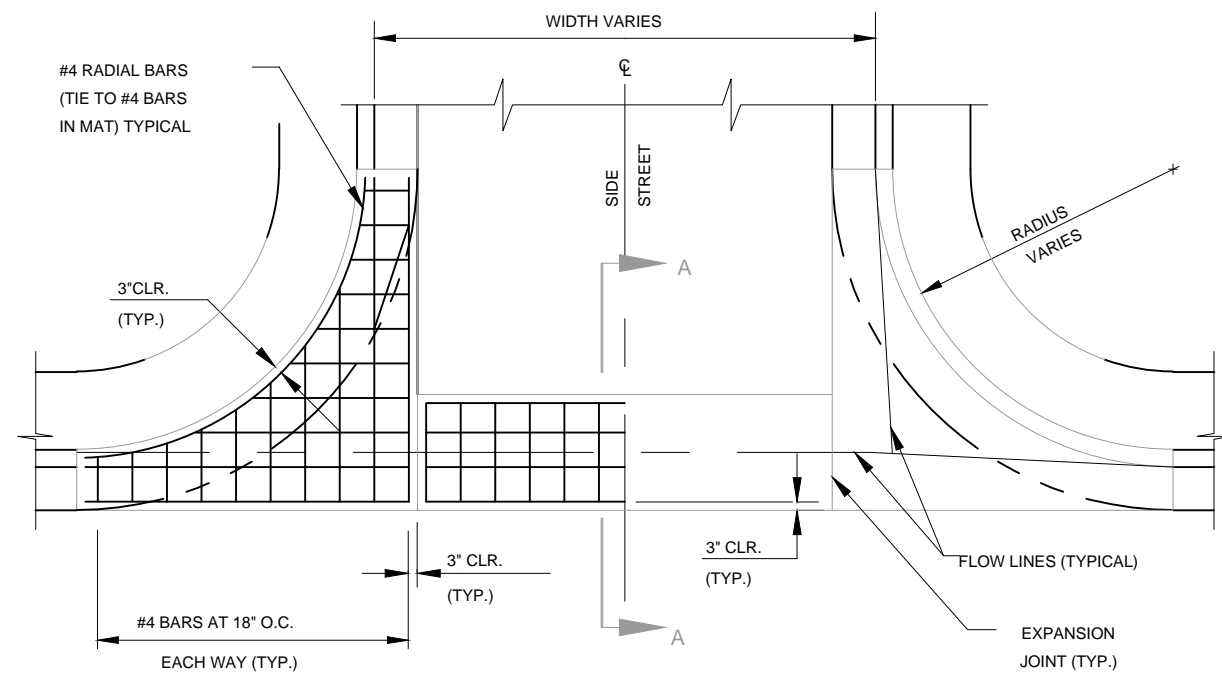
EXPIRATION DATE: 06/30/16



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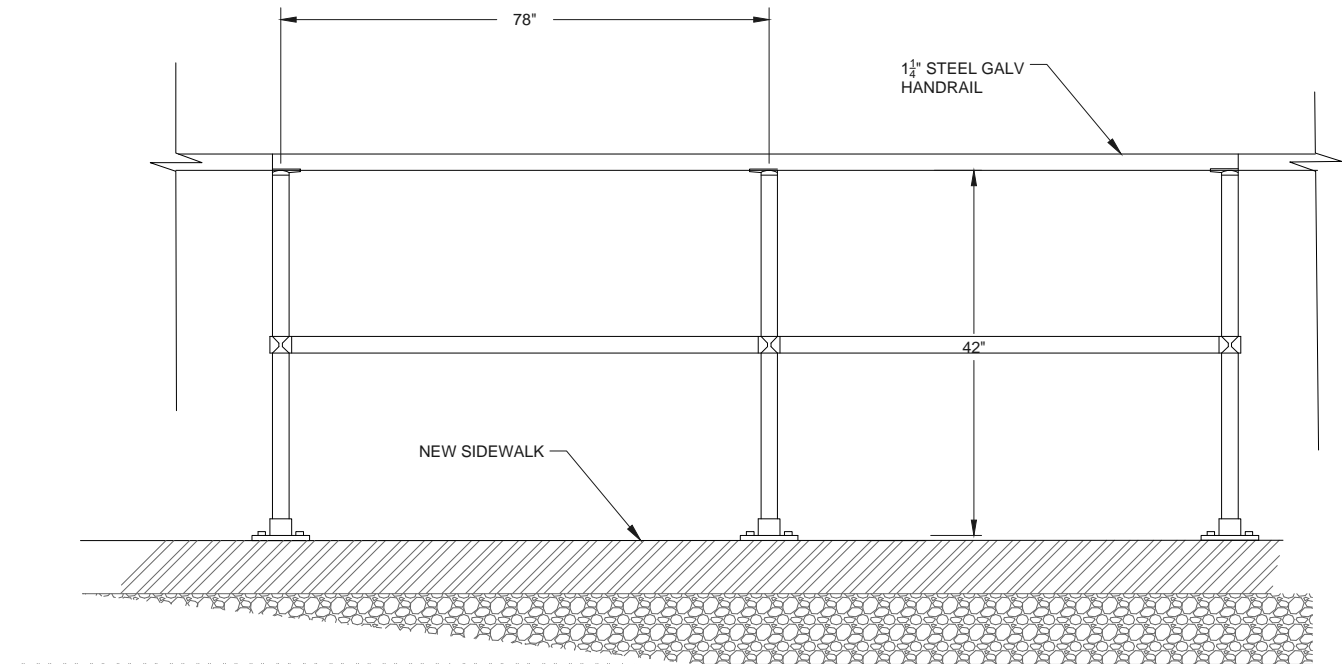
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	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS
	ROADWAY CROSS SECTIONS - SUPERELEVATED CURVES
	<div style="text-align: center; font-size: 2em; font-weight: bold;">D3</div> <div style="text-align: center; font-weight: bold;">June 2015</div>



SECTION A-A

NOTES:

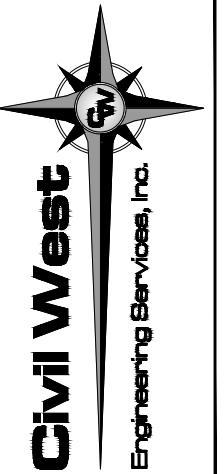
1. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: SYNTHETIC FIBER-REINFORCEMENT (AT A MINIMUM OF 1 POUND PER CUBIC YARD), 4000 PSI MIN. COMPRESSIVE STRENGTH @ 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH A MAX. WATER/CEMENT RATIO OF 0.45, AIR ENTRAINMENT 6% \pm 1.5%, SLUMP AT 1 TO 4 INCHES. ALL MATERIALS SHALL CONFORM TO SSPWC SECTION 202. POLYPROPYLENE FIBERS SHALL BE ADDED TO THE P.C.C. PER THE MANUFACTURER'S RECOMMENDATIONS.
2. FOR INDUSTRIAL AREAS, AN ENGINEERED DESIGN IS REQUIRED.
3. AGGREGATE BASE SHALL BE ENGINEERED TYPE 2, CLASS B WITH 95% COMPACTION.
4. VALLEY GUTTER SHALL HAVE WEAKENED PLANE JOINTS EVERY 10 FEET.
5. LOCAL AND COLLECTOR STREETS, WIDTH = 6 FEET. ARTERIAL STREETS (DRIVEWAYS ONLY), WIDTH = 10 FEET.
6. VALLEY GUTTER SECTIONS (SPANDRELS) ALONG CURB & GUTTER SHALL BE A MONOLITHIC POUR AS SHOWN.



HANDRAIL DETAIL
NOT TO SCALE

P.C.C. VALLEY GUTTER
NOT TO SCALE

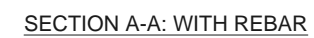
REGISTERED PROFESSIONAL
ENGINEER
88387PE
OREGON
SEPT 10, 2013
RALPH R. WENZIGER
EXPIRATION DATE: 06/30/16



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CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	PCC VALLEY GUTTER DETAIL & HANDRAIL DETAIL
Sheet No.	D4	
Date	June 2015	



- NOTES:**
1. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI
 2. SCORE MARKS ARE TO BE TOOLED INTO CONCRETE SURFACE TO FORM A SQUARE BUT NOT TO EXCEED 8" IN ANY DIRECTION.
 3. REINFORCING BAR IS TO BE HELD UP WITH NON-CORROSIVE MATERIALS (ROCK, CONCRETE) PRIOR TO BEING PLACED.
 4. CONCRETE IS TO RECEIVE A MEDIUM BROOM FINISH. BROOMING IS TO BE PERPENDICULAR TO THE CURB LINE.
 5. CURB JOINT SHALL BE A TROWELED JOINT WITH A MIN. 1/2" RADIUS ALONG THE BACK OF CURB.
 6. DRIVEWAY SHALL BE A MINIMUM 8" THICK.



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REV.	DATE	DESCRIPTION	BY
1	7/17/15	ADDENDUM #6 (AD6)	RRW
Designed By: RRW			Checked By: MEH
Project No:			2302-027

CITY OF NEWPORT
LINCOLN COUNTY, OREGON

SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

DRIVEWAY DETAILS

D5

August 2015

Isometric view of a sidewalk apron and curb detail. The diagram shows a concrete apron with a broom finish, a ramp with a 1:12 slope, and a standard curb and gutter. Key dimensions and features include:

- CONCRETE IN SIDEWALK AREA SHALL HAVE BROOM FINISH
- RAMP 1:12 SLOPE
- INSTALL 3" PLASTIC DRAIN PIPE AND COUPLING TO CONNECT ROOF DRAINS
- 4'-6" RESIDENTIAL
- 1' MIN.
- 60" RESIDENTIAL
- MANDATORY SAW CUTTING OF EXISTING CONCRETE
- 10' APRON SEE NOTE 2
- STANDARD CURB & GUTTER
- 60" RESIDENTIAL
- 12' MIN. 24' MAX. FOR 50' TO 100' OF FRONTAGE

Technical drawing of a driveway layout. The drawing shows a 5' residential driveway with a 2% to 10% slope. The driveway is composed of 6" of 3/4"-0 AGG BASE COMPACTED. A 1/2" - 1" LIP is shown at the end of the driveway. The drawing also includes a STANDARD CURB & GUTTER, a BREAKLINE, and a CAP. A note indicates "BREAK EXISTING CURB & CAP AS SHOWN". A warning symbol "AD6" is present.

NOTES

1. CONCRETE SHALL HAVE A MINIMUM BREAKING STRENGTH OF 3500 PSI AFTER 28 DAYS, FOR ALL DRIVEWAY APRONS (6") AND DRIVEWAY EXTENTIONS (4")
2. 10' CONCRETE APRON REQUIRED FOR ALL DRIVEWAYS WHERE NO SIDEWALK IS CONSTRUCTED.
3. 4" THICKNESS OF CONCRETE FROM BACK OF SIDEWALK OR DRIVEWAY APRON TO PROPERTY LINE WHERE PRIVATE DRIVE IS CONCRETE
4. DRIVEWAY MUST MEET EXISTING STREET GRADE
5. CITY APPROVAL REQUIRED FOR DEVIATION FROM WIDTH STANDARDS
6. PERMITS AND INSPECTIONS ARE REQUIRED ON NEW AND EXISTING STRUCTURES. THE INSPECTION WILL BE MADE AFTER THE FORMS HAVE BEEN SET
7. FOR SIDEWALKS REFER TO STANDARD SIDEWALK DETAIL, THIS SHEET.
8. FOR CURB & GUTTER REFER TO STANDARD CURB & GUTTER DETAIL, SHEET ##.

The diagram illustrates a cross-section of a road with a driveway. Key features and labels include:

- Variable Recommended 4' Min.**: Dimension for the setback area.
- AS SPECIFIED**: Label for the driveway section.
- 1'-0" MIN.**: Dimension for the setback area.
- 2%**: Slope specification for the driveway section.
- 2:1 MAX SLOPE**: Slope specification for the shoulder area.
- MIN. 3/4" CRUSHED ROCK OR SAND AS APPROVED BY THE CITY WITH A MIN. DEPTH OF 4"**: Material and depth specification for the base layer.
- ROAD CURB & DETAIL T-301**: Label for the curb detail.
- DRIVEWAY SECTION**: Label for the driveway section.
- R/W**: Right of Way line.
- AD6**: Triangle symbol indicating a specific detail or section.

TYPICAL SETBACK TYPE CROSS SECTION

CONTRACTION JOINTS AT APPROXIMATE 15' INTERVALS

TOOLED "DUMMY" JOINTS AT APPROXIMATE 5' INTERVALS

CONTRACTION JOINT (EXPANSION JOINT IF AGAINST EXISTING SIDEWALK)

CONTINUE PATTERN THROUGH DRIVEWAY APRON

STANDARD DRIVEWAY (OPTIONAL)

STANDARD CURB & GUTTER

EXPANSION JOINT REQUIRED IF ABUTTING AGAINST EXISTING CONCRETE SIDEWALK

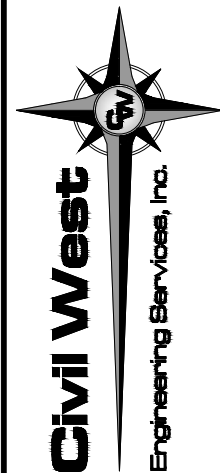
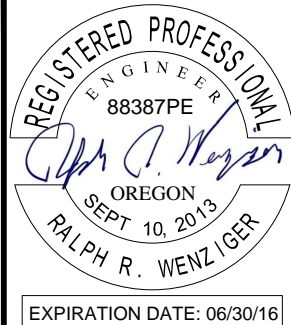
JOINT IN SIDEWALK TO MATCH JOINT IN CURB

PLACE BOND BREAK BETWEEN CURB AND SIDEWALK

SIDEWALKS 8' AND WIDER SHALL HAVE A LONGITUDINAL CONTRACTION JOINT AT THE MIDPOINT

NOTES:

1. CONCRETE SHALL BE 3500 PSI IN 28 DAYS
2. STANDARD SIDEWALK CROSS SLOPE SHALL BE 2%. WHEN THE LOT IS BELOW THE TOP OF THE CURB AND SLOPE DOWN FROM CURB , A MINUS 2% SLOPE MAY BE REQUIRED
3. CONCRETE DEPTH FOR SIDEWALKS SHALL BE A NOMINAL 4" MIN. DRIVEWAY SECTIONS INCLUDING SIDEWALKS THROUGH DRIVEWAYS SHALL BE NOMINAL 6" MIN FOR LOCAL STREETS AND 8" FOR ARTERIALS AND COMMERCIAL ROADWAYS
4. CONTRACTION JOINTS SHALL BE INSTALLED AT APPROXIMATE INTERVALS OF 15' BY CUTTING AT LEAST $\frac{1}{2}$ OF THE DEPTH OF THE CONCRETE. "DUMMY" JOINTS SHALL BE INSTALLED AT APPROXIMATE INTERVALS OF 5'. EXPANSION JOINTS WITH PRE-MOLDED FILLER SHALL BE INSTALLED BETWEEN DRIVEWAYS & SIDEWALKS AT THE DIRECTION OF THE ENGINEER, SEE DRIVEWAY STANDARD.
5. INSTALL A BOND BREAKER OR ISOLATION JOINT BETWEEN BACK OF CURB & SIDEWALK, & AROUND ANY OBSTRUCTION WITHIN SIDEWALK AREA
6. DRAIN BLOCKOUTS IN THE CURB SHALL BE EXTENDED TO THE BACK OF THE SIDEWALK WITH A 3" DIAMETER. PLASTIC PIPE AT A 2% SLOPE. A CONTRACTION JOINT SHALL BE PLACED OVER THE PIPE
7. SEE STANDARD WHEELCHAIR/BICYCLE RAMP DETAILS FOR SIDEWALK PATTERNS AT INTERSECTION CURB RETURNS
8. SEE, ALSO, STANDARD DETAILS FOR DRIVEWAYS, THIS SHEET
9. A MIN. SEPARATION OF 4' BETWEEN CURB & SIDEWALK IS RECOMMENDED FOR LANDSCAPE STRIP MAINTENANCE
10. SIDEWALKS 8" AND WIDER SHALL HAVE A LONGITUDINAL CONTRACTION JOINT AT THE MIDPOINT

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REV.	DATE	DESCRIPTION	BY
1	7/17/15	ADDENDUM #6 (AD6)	RRW

Designed By:	RRW	Drawn By:	ALF	Checked By:	MEH
Project No: 2302-027					

CITY OF NEWPORT
LINCOLN COUNTY, OREGON

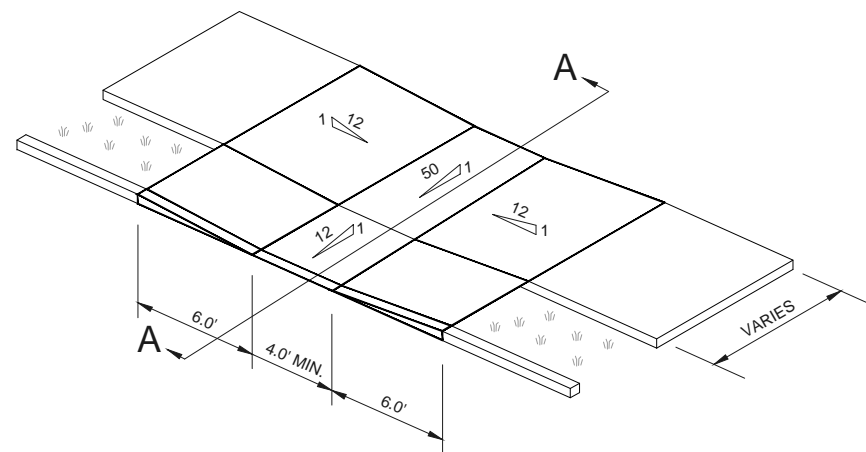
SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

STANDARD SIDEWALK & DRIVEWAY APPROACH DETAILS

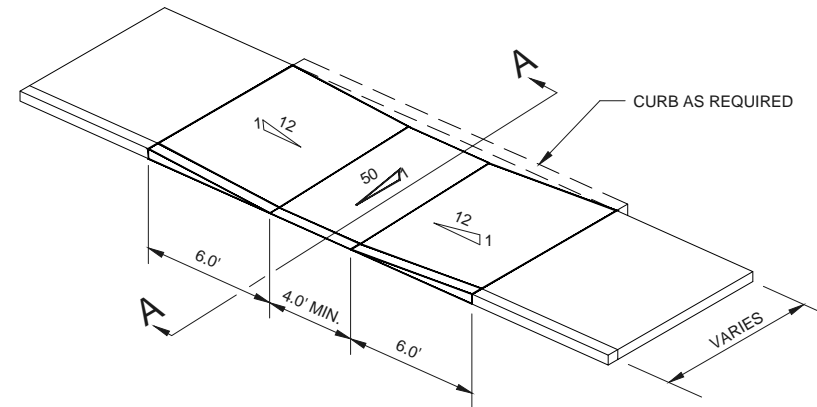
D6

July 2015

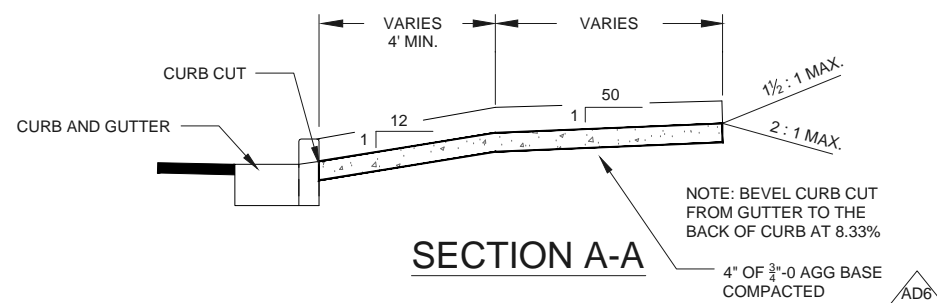
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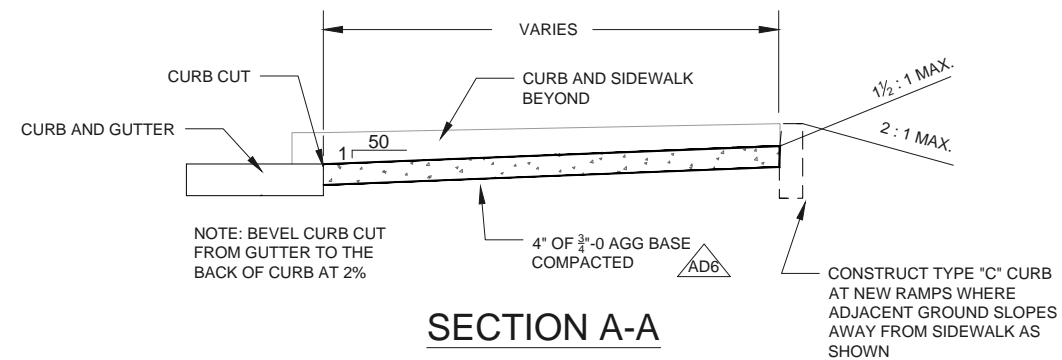
SIDEWALK RAMP DETAIL



SIDEWALK RAMP DETAIL



SECTION A-A



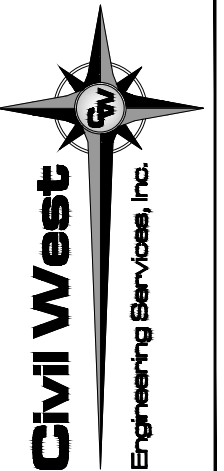
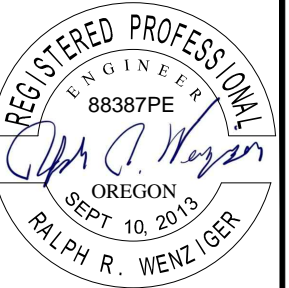
SECTION A-A

1
D7

SIDEWALK AND RAMP DETAIL W/ PLANTER STRIP
NOT TO SCALE

2
D7

SIDEWALK AND RAMP DETAIL W/O PLANTER STRIP
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REV.	DATE	DESCRIPTION	BY
1	7/17/15	ADDENDUM #6 (AD6)	rw
Designed By: RWW Drawn By: ALF Checked By: MEH Project No: 2302-027			

CITY OF NEWPORT
LINCOLN COUNTY, OREGON
SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS
TYPICAL SIDEWALK & RAMP
DETAILS

Sheet No.
D7
Date
July 2015



SCALE: NOT TO SCALE


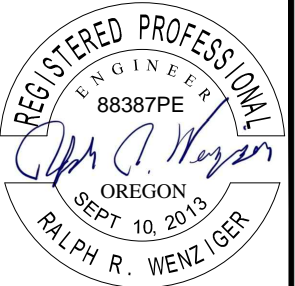


SCALE: NOT TO SCALE



-
- Diagram illustrating the cross-section of a pile structure:
- ROUND CONCRETE CAP**: The top section of the pile, shown with a rounded top.
 - 6" STEEL PIPE PAINTED YELLOW FILLED W/ CONCRETE**: The main body of the pile, which is a 6-inch steel pipe filled with concrete and painted yellow.
 - 12" ROUND CONCRETE FOOTING**: The base section of the pile, shown as a 12-inch round concrete footing.
- Dimensions indicated:
- 4'**: Height of the Round Concrete Cap.
 - 3'**: Height of the 6" Steel Pipe section.
 - 4"**: Diameter of the 12" Round Concrete Footing.

4
D9



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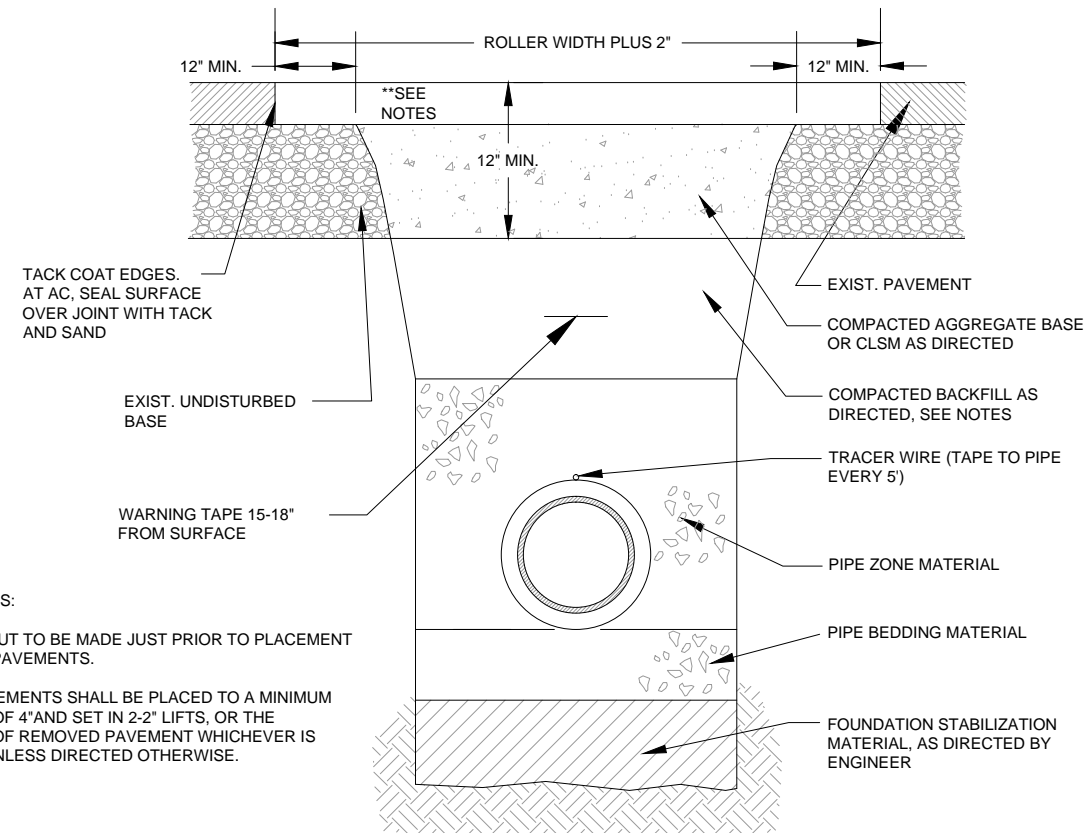
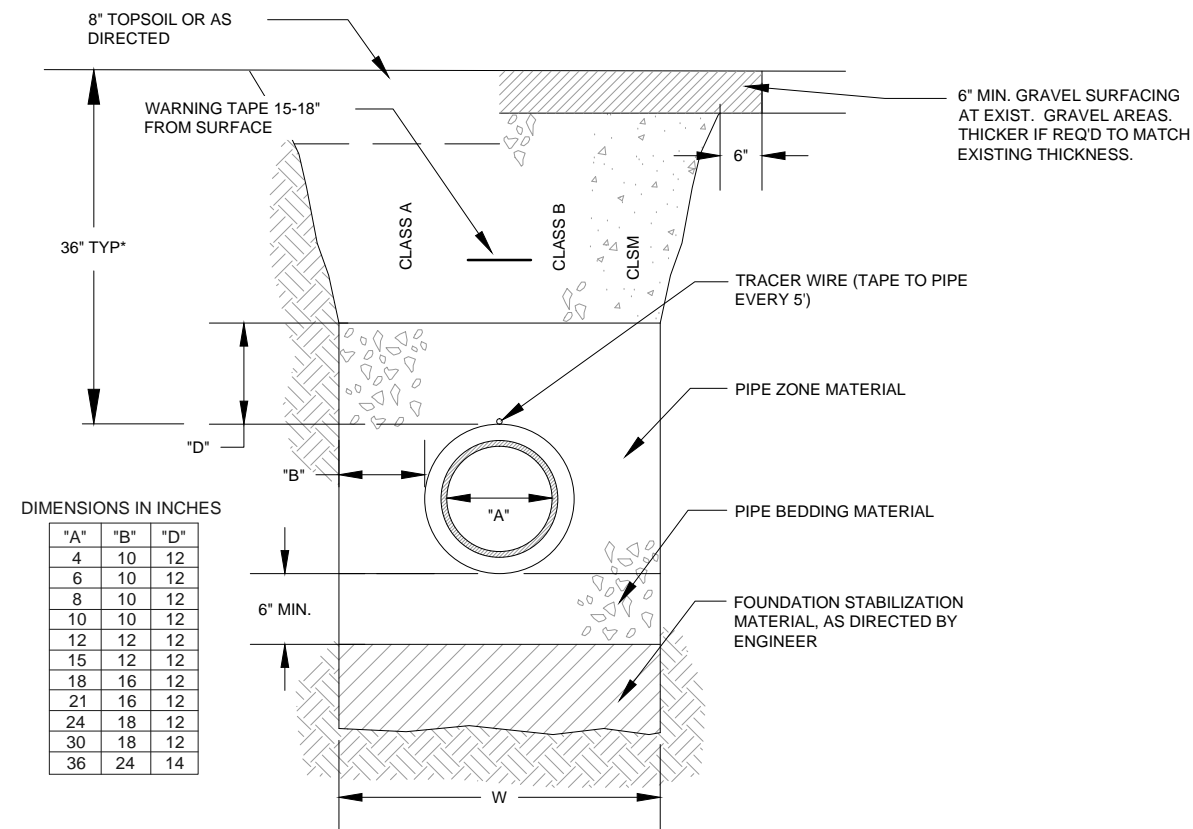
SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

CURB & GUTTER DETAILS AND BOLLARD DETAIL

Date	Sheet No.
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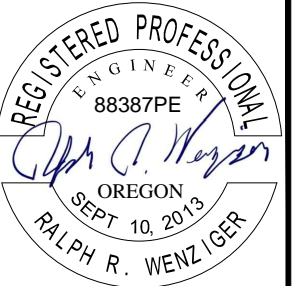
D9

June 2015

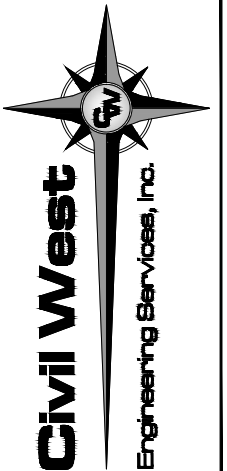


NOTES:

1. TRENCH EXCAVATION SHALL BE CONDUCTED IN A SAFE MANNER WITH ALL NECESSARY BRACING AND SHORING PROVIDED FOR COMPLIANCE WITH OSHA.
2. FOUNDATION STABILIZATION SHALL BE PROVIDED WHEN MATERIAL AT BOTTOM OF TRENCH IS UNSUITABLE IN THE OPINION OF THE CITY TO PROVIDE A STABLE TRENCH BASE.
3. CONSTRUCTION IN PUBLIC RIGHT-OF-WAY SHALL COMPLY WITH PUBLIC STANDARDS.
4. CLASS B BACKFILL REQUIRED IN ALL ROADWAYS AND TRAFFIC PATHS EXCEPT WHERE CLSM BACKFILL IS REQUIRED BY COUNTY, ODOT, OR CITY.
5. TONING WIRE REQUIRED AT SERVICE LATERALS. WIRE SHALL BE 12 GA. MINIMUM SOLID COPPER WIRE WITH GREEN 30 MIL THICK HDPE INSULATION RATED FOR DIRECT BURY. USE APPROVED WATERPROOF SPLICE AT ALL CONNECTIONS.
6. WARNING TAPE SHALL BE 6" WIDE, 4 MIL THICK, APWA GREEN, READING "CAUTION SEWER LINE BURIED BELOW".
7. MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY SANITARY SEWER DESIGN STANDARDS MANUAL.
8. COMPACT ALL FILL AS SPECIFIED.



EXPIRATION DATE: 06/30/16



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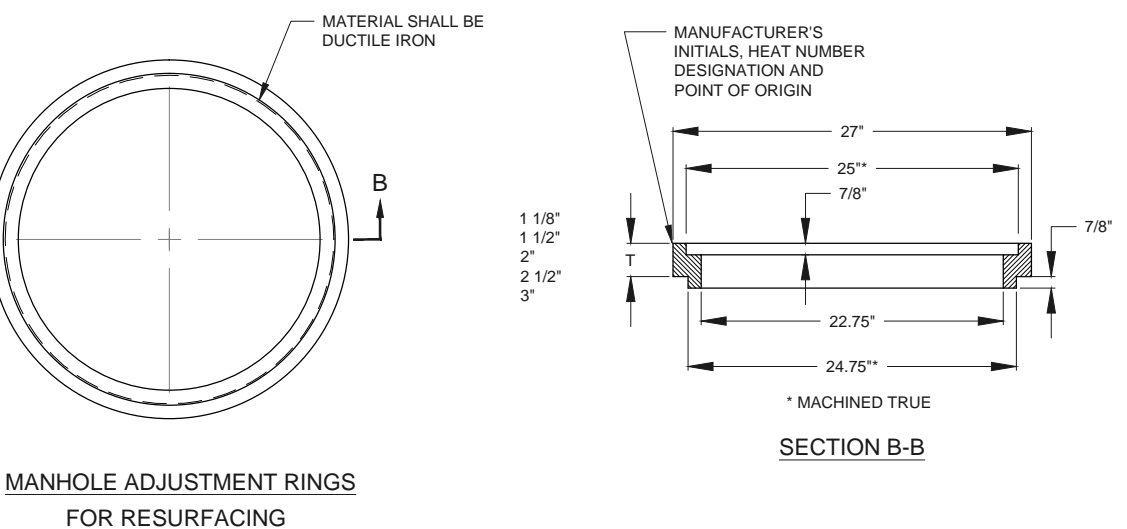
CITY OF NEWPORT
LINCOLN COUNTY, OREGON

SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

TYPICAL TRENCH DETAILS

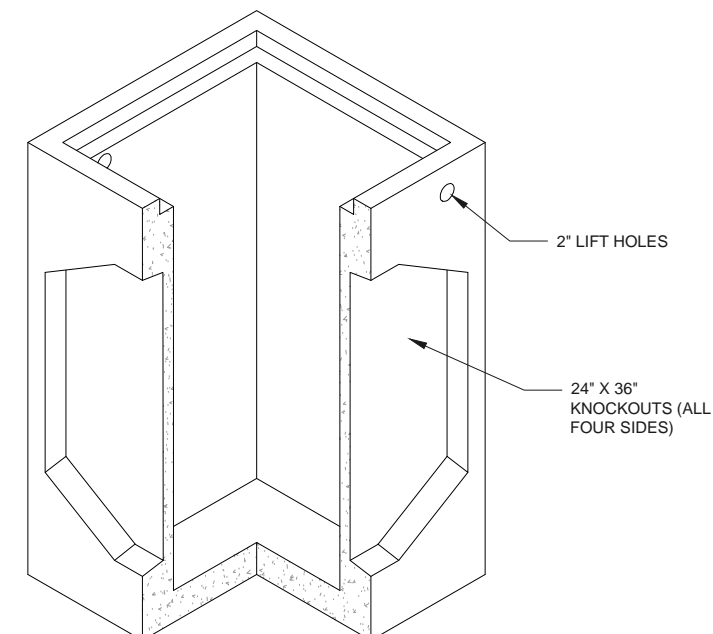
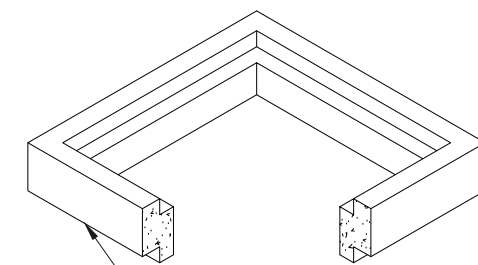
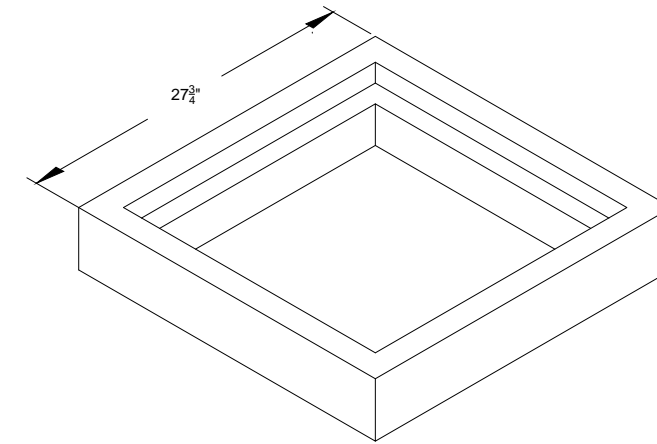
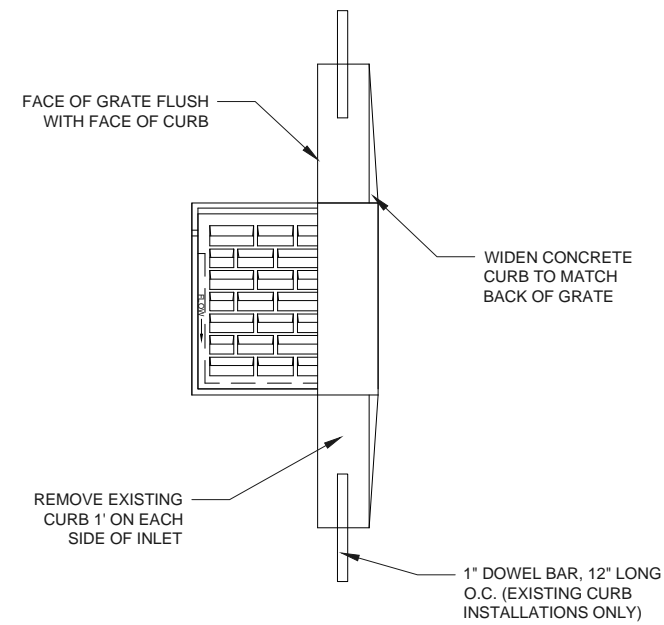
D10

June 2015



CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	MANHOLE FRAME ADJUSTMENT DETAILS
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Sheet No.	D11
Date	June 2015



- SHEET NOTES:**
1. SET GRATE TO GRADE WITH CONCRETE ADJUSTMENT RINGS AND MORTAR; GROUT BETWEEN GRATE AND BOX TO SMOOTH FINISH.
 2. POUR CONCRETE COLLAR AROUND GRATE AND BOX.
 3. MAXIMUM VERTICAL ADJUSTMENT OF 2' $\frac{1}{2}$ " WITHOUT RISER RINGS; MAX HORIZONTAL OFFSET = 3".
 4. MAXIMUM OF 12" OF CONCRETE ADJUSTMENT RINGS ALLOWED WITH FULL BED OF MORTAR BETWEEN EACH.
 5. VOIDS AROUND PIPE DIAMETER SHALL BE GROUTED ON THE INSIDE AND OUTSIDE OF THE BOX.
 6. LOCATE WIRES (SEE PIPE INSTALLATION STANDARD DRAWINGS FOR DETAILS) SHALL BE EXTENDED TO THE TOP OF THE STRUCTURE.
 7. OPTIONAL BAFFLE BOX (NOT SHOWN) FOR 6", 8", 10", OR 12" PIPES ARE AVAILABLE; AS REQUIRED BY ENGINEER.
 8. USE ADVANTAGE PRECAST 24" SQUARE CATCH BASIN OR PRE-APPROVED EQUAL. USE D&L FOUNDRY I-3559 CURB INLET OR PRE-APPROVED EQUAL.

1
D13

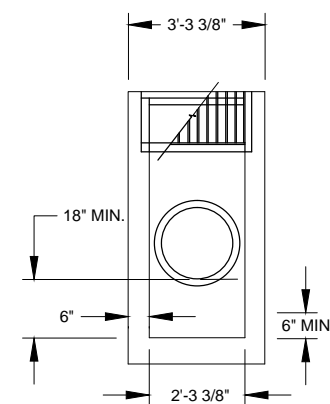
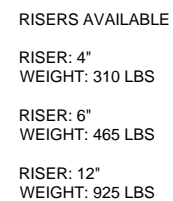
TYPICAL CATCH BASIN DETAIL
NOT TO SCALE

REGISTERED PROFESSIONAL
ENGINEER
88387PE
Ralph R. Wenziger
OREGON
SEPT 10, 2013
RALPH R. WENZIGER
EXPIRATION DATE: 06/30/16

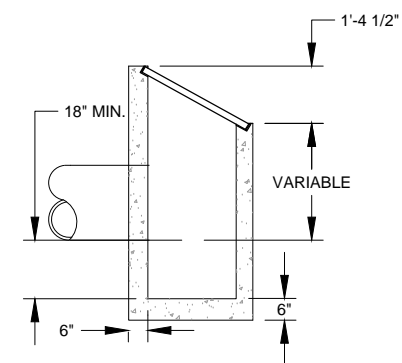
REV.	DATE	DESCRIPTION	BY
Designed By: RWW		Drawn By: ALF	Checked By: MEH
Project No:		2302-027	

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	TYPICAL CATCH BASIN DETAILS
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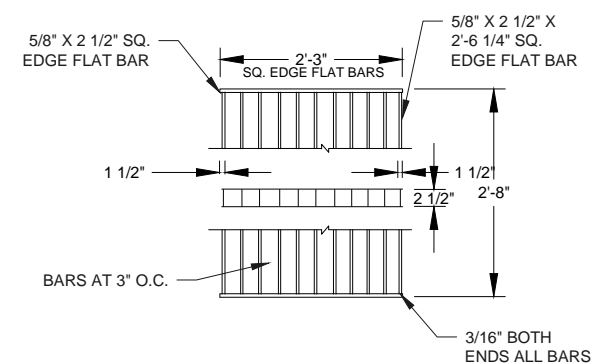
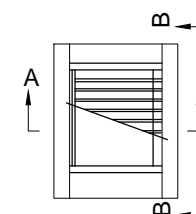
Sheet No.	D13
Date	June 2015



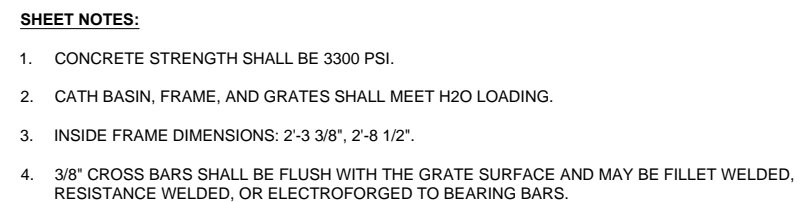
SECTION B-B



SECTION A-A

DITCH INLET

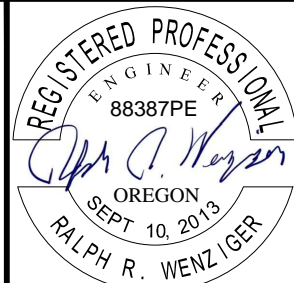
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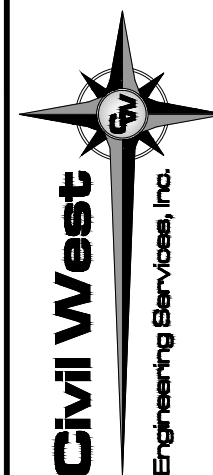
1
D14

TYPICAL DITCH INLET AND GRATE DETAIL

NOT TO SCALE



EXPIRATION DATE: 06/30/16



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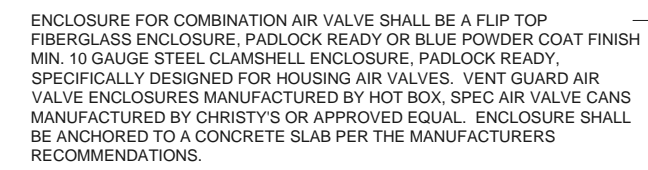
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LINCOLN COUNTY, OREGONSW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

DITCH INLET & GRATE DETAIL

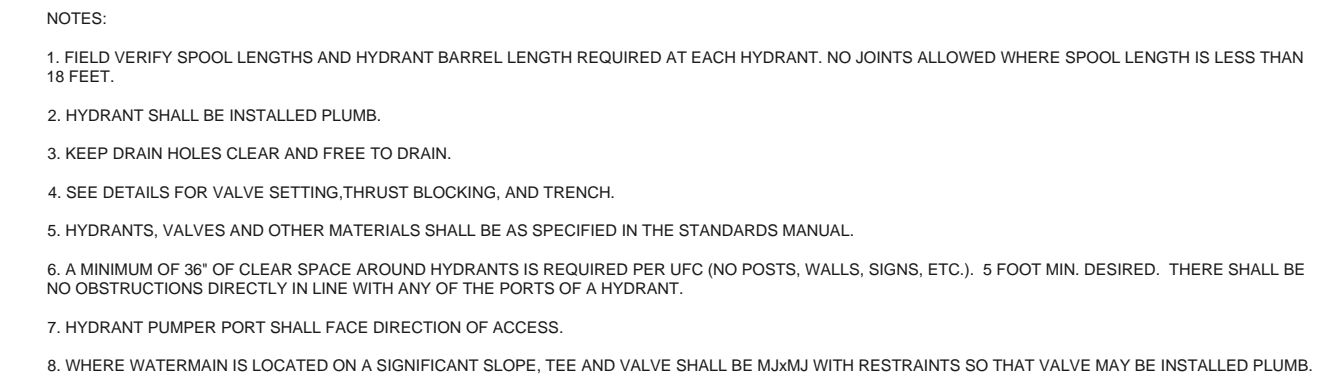
Date	Sheet No.
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D14

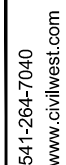
June 2015



1 AIR RELEASE VALVE ASSEMBLY
D15 NOT TO SCALE



2 STANDARD FIRE HYDRANT ASSEMBLY
D15 NOT TO SCALE



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Sheet No.	
Date	
<p>CITY OF NEWPORT LINCOLN COUNTY, OREGON</p> <p>SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS</p> <p>FIRE HYDRANT & ARV ASSEMBLIES DETAILS</p>	
<p>D15</p> <p>June 2015</p>	

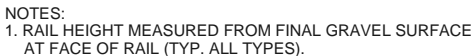
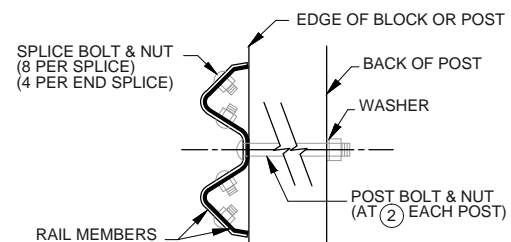


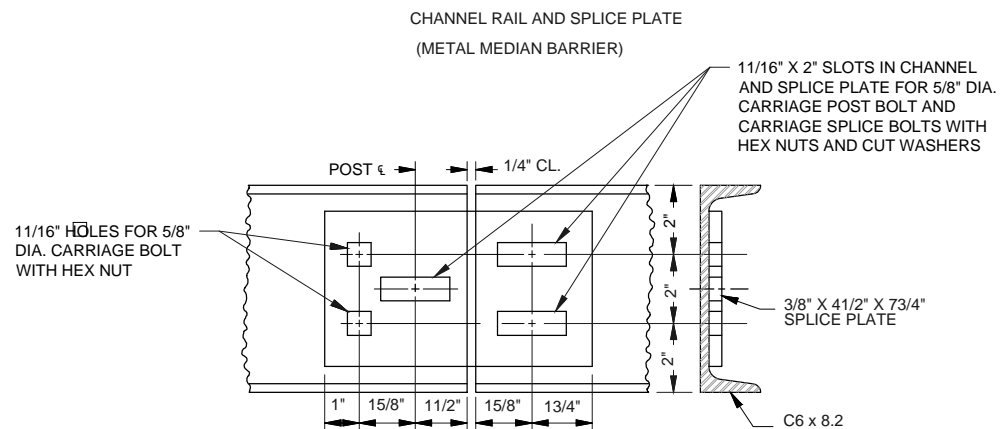
TABLE OF POST SPACING				
TYPE	1	2A	3	4
SPACING	12'-6"	6'-3"	3'-11/2"	6'-3"

NORMAL RAIL ELEMENT DATA			
Type	Rail	Effective Lengths	Gauges
1, 2A, 3	W beam	6.25', 12.5', 25'	10 & 12
4	Thrie beam	6.25', 12.5', 25'	10 & 12



1. WHEN REQUIRED BY THE PLANS, POST BOLTS TO EXTEND BEYOND THE TIGHTENED NUTS WITHIN LIMITS OF 1/4" TO 1/2".
2. WHEN STEEL POSTS ARE USED SEE "POSTS" FOR MODIFIED BOLT DETAIL, STD. DRG. RD415.
3. ALL POST BOLT THREADS TO BE SET AFTER ASSEMBLY FOR WRENCH REMOVAL ONLY.

FITTINGS

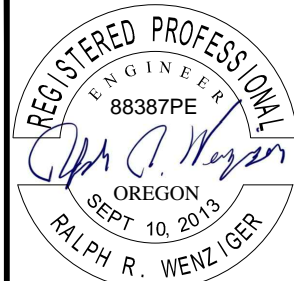


1
D16

TYPE 2A GUARDRAIL
NOT TO SCALE

GENERAL NOTES FOR ALL DETAILS:

1. MAINTAIN 4' DISTANCE BETWEEN FACE OF GUARDRAIL AND FACE OF SOLDIER PILE WALL.
2. FOR GUARDRAILING END ANCHOR DETAILS SEE SHEET D4.



EXPIRATION DATE: 06/30/16



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REV.	DATE	DESCRIPTION	BY

Designed By: RWW	Drawn By: ALF	Checked By: MEH
Project No: 2302-027		

CITY OF NEWPORT
LINCOLN COUNTY, OREGON

SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS


GUARDRAIL DETAIL SHEET

Sheet No.

D16

June 2015

Civil West



Engineering Services, Inc.

109 SW Hurbert Street
Newport, Oregon 97365

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REV.	DATE	DESCRIPTION	BY
1	7/16/2015	ADDENDUM #3 (AD3)	RRW

Designed By:	RRW	Drawn By:	ALF	Checked By:	MEH
Project No: 2302-027					

STRIPING & SIGN PLAN

July 2015

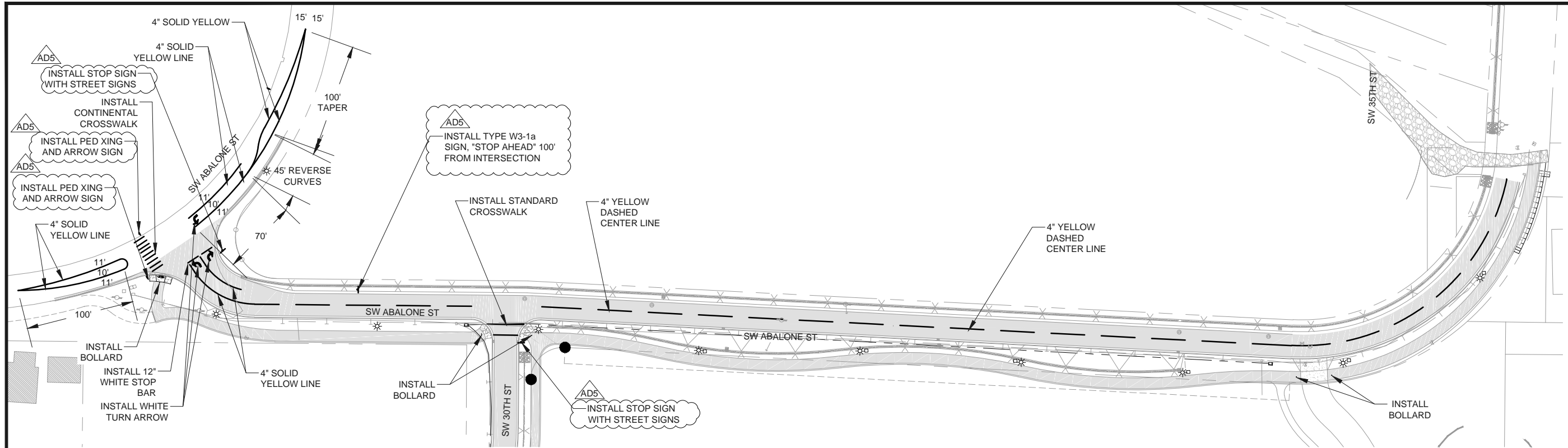


1
D17

STRIPING & SIGN PLAN

$$1'' = 100'$$

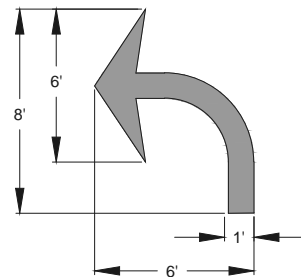
DATE: 7/16/15 FILE: \\C:\CW_Projects\2302 Newport\2302-027 Newport Abalone and 30th Roadway\Imps\Drawings\Imps\Abalone & 30th - DESIGN.dwg



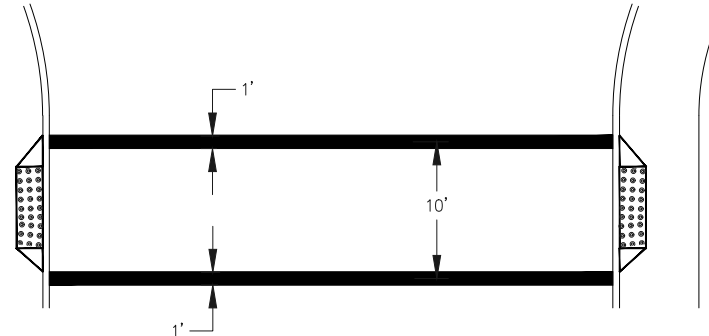
1
D18

STRIPING & SIGN PLAN

1" = 100'



1
D18
WHITE TURN ARROW DETAIL
NTS

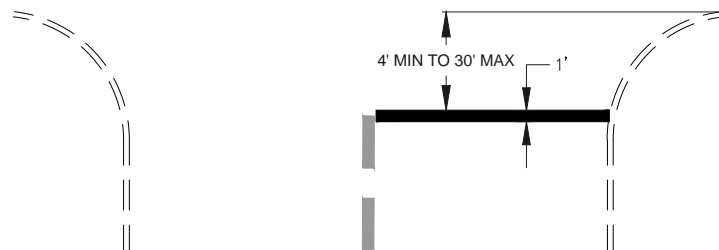


2
D18
STANDARD CROSSWALK TWO 1' WHITE BARS
NTS

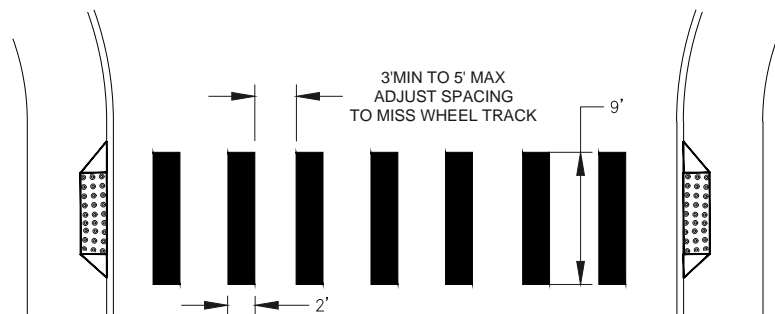
AD5



5
D18
SIGN #1, 36x36 TYPE W3-1a
NTS



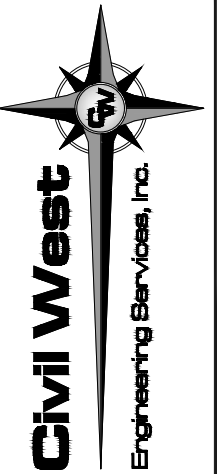
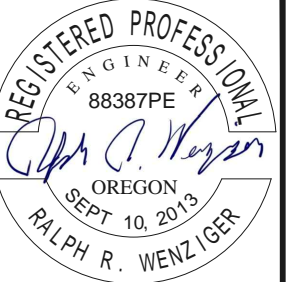
3
D18
12" WHITE STOP BAR
NTS



4
D18
STAGGERED CONTINENTAL CROSSWALK 2' BARS
NTS



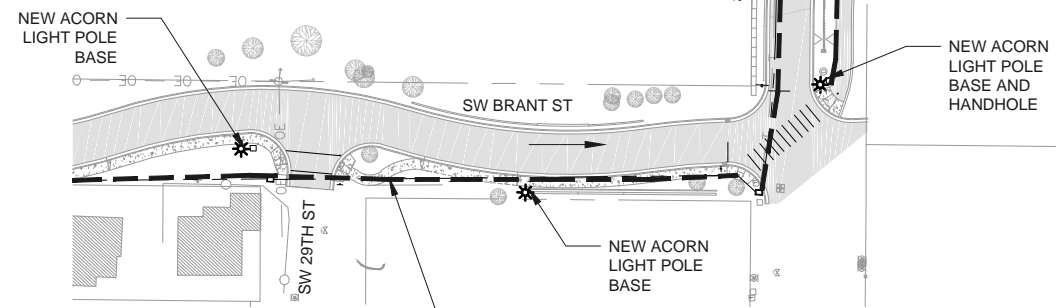
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D18
SIGN #2, 30x30 TYPE W11-2 & W16-7pR
NTS





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REV.	DATE	DESCRIPTION	BY
1	7/16/2015	ADDENDUM #5 (AD5)	RRW
Designed By: RRW Drawn By: ALF Checked By: MEH			
Project No: 2302-027			

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	STRIPING & SIGN DETAILS
Sheet No: Date:	D18 July 2015	


$$1'' = 100'$$

<u>LEGEND</u>	
— E — E —	NEW 2" ELECTRICAL CONDUIT
- OE - OE - OE - OE -	EXISTING ELECTRICAL
— T — T —	2" PIONEER TELEPHONE CONDUIT
— T — T —	EXISTING TELEPHONE LINE
— - - - -	2" COAST COMM CONDUIT
— C — T —	EXISTING COAST COMM LINE
— - - - -	UTILITY TRENCH
	STREET LIGHT
	HANDHOLE JUNCTION BOX



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REV.	DATE	DESCRIPTION	BY

Designed By: RWW Drawn By: ALF Checked By: MEH
 Project No: 2302-027

CITY OF NEWPORT
LINCOLN COUNTY, OREGON

SW ABALONE ST. & 30TH ST.
ROADWAY IMPROVEMENTS

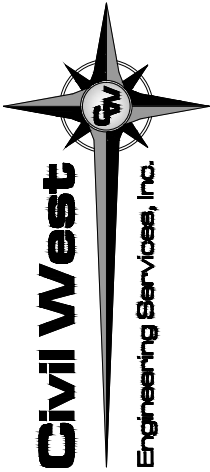
SW 30TH STREET & SW ABALONE STREET
LIGHTING AND CONDUIT PLAN

E1

June 2015



LIGHTING AND CONDUIT PLAN

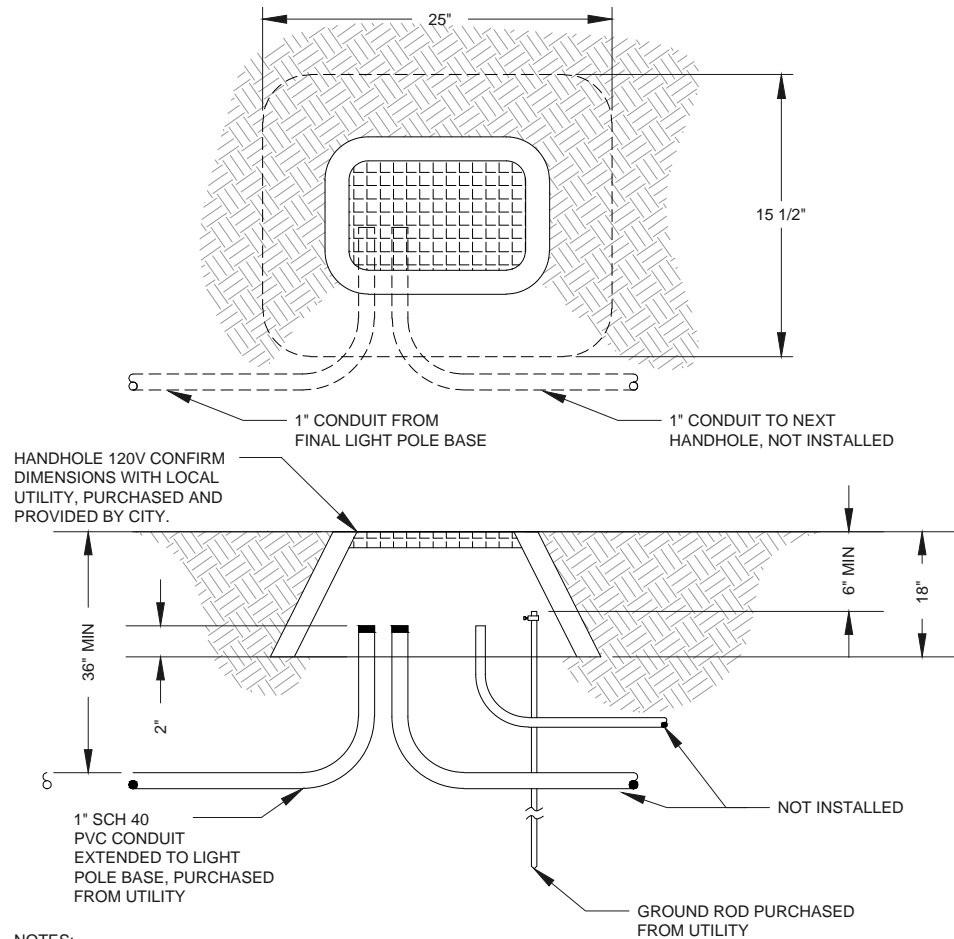
609 SW Hubert Street
Newport, Oregon 97365

Project No: 2302-027

SW 27TH STREET & SW BRANT LIGHTING AND CONDUIT PLAN

June 2015

DATE:7/10/15 FILE:Y:\CW_Projects\2302-027 Newport Abalone and 30th Roadway\Imps\Drawings\DWG\Detail Sheets_addendum.dwg



NOTES:

HANDHOLE TYPE AND SIZE PROVIDED BY CITY.

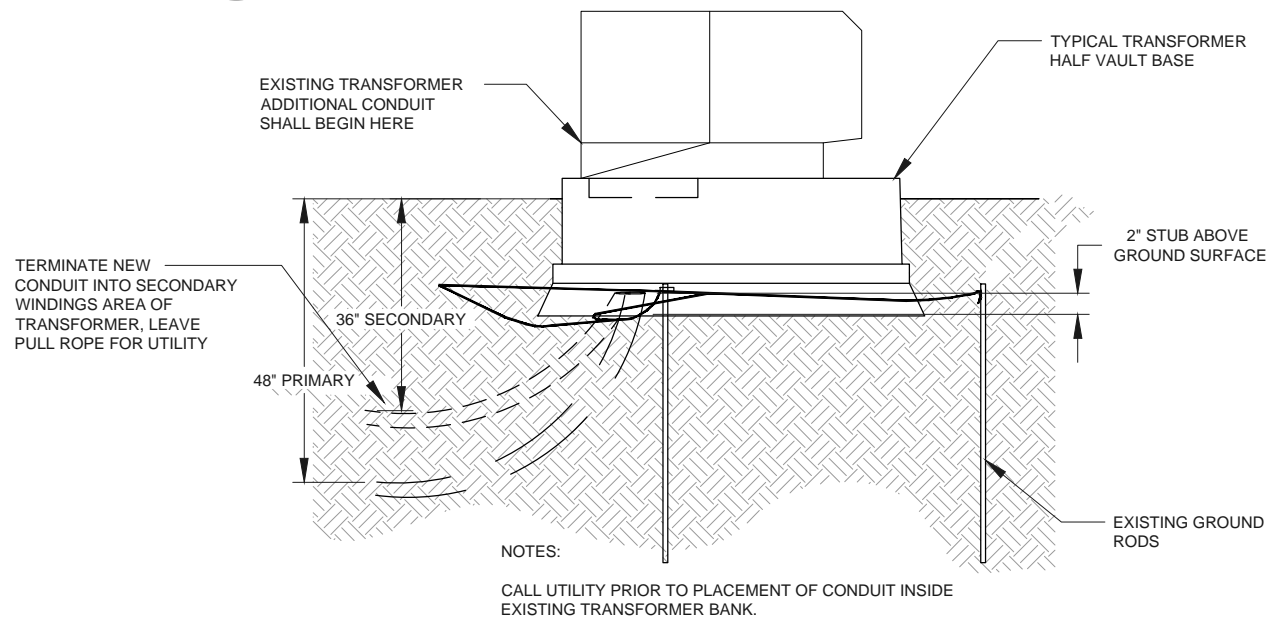
INSTALL GROUND ROD AND PVC CONDUIT. WIRE AND LIGHT POLE CONNECTIONS WILL BE COMPLETED BY THE UTILITY.

LOCATE HANDHOLES PER STREET PLAN DRAWINGS ON COMPACTED BASE.

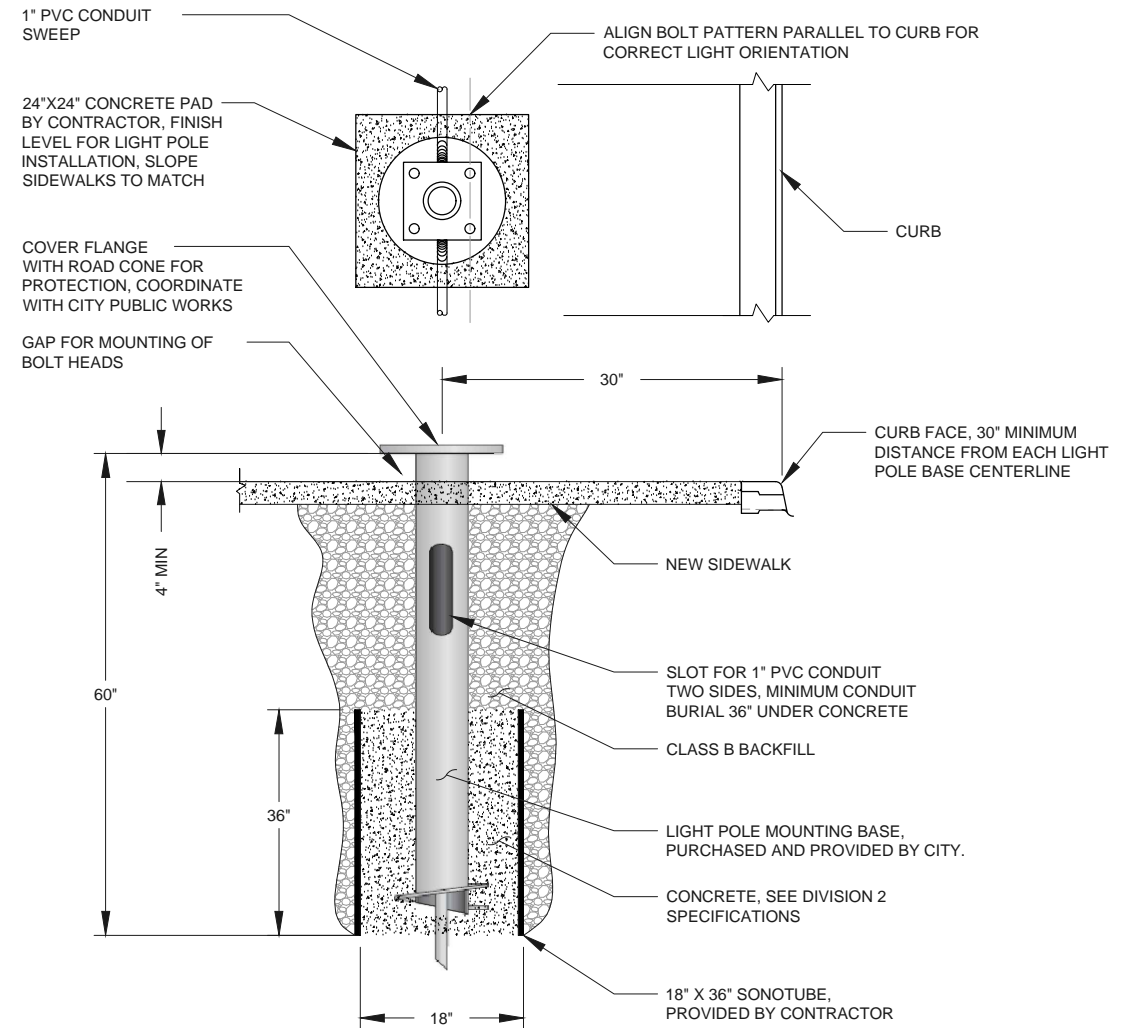
HANDHOLES SHALL BE FLUSH WITH GROUND SURFACE TO AVOID TRIPPING HAZARDS TO PEDESTRIANS.

ONE (1) PULL ROPE SHALL BE LOCATED IN EACH CONDUIT. PULL ROPE SIZE AND STYLE SHALL BE PURCHASED FROM UTILITY.

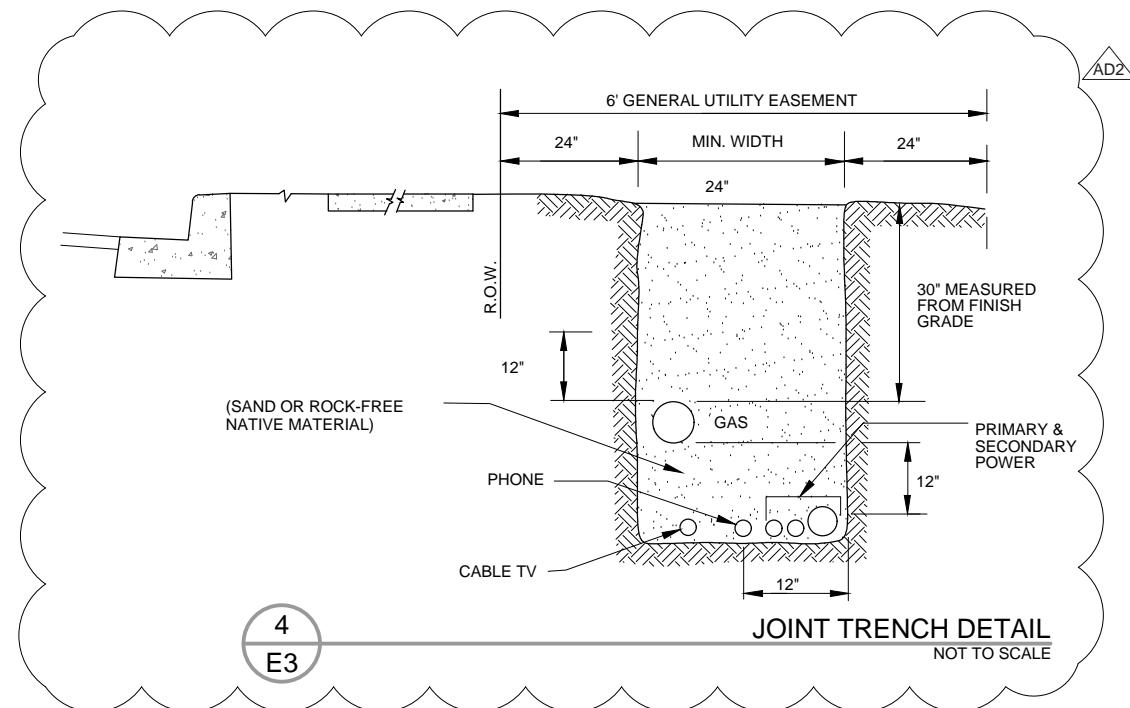
1 LIGHTING HANDHOLE DETAIL
E3 NOT TO SCALE



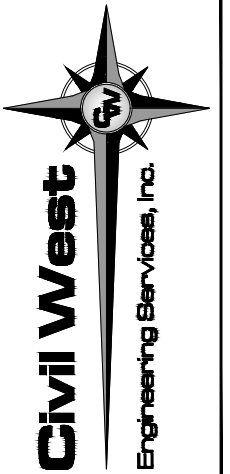
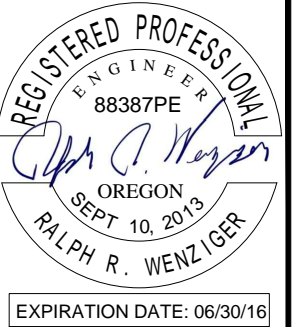
2 TRANSFORMER DETAIL
E3 NOT TO SCALE



3 LIGHTING POLE BASE
E3 NOT TO SCALE



4 JOINT TRENCH DETAIL
E3 NOT TO SCALE



REV.	DATE	DESCRIPTION	BY
1	7/10/2015	ADDENDUM #2 (AD2)	
Designed By: RWW Drawn By: ALF Checked By: MEH			
Project No: 2302-027			

CITY OF NEWPORT LINCOLN COUNTY, OREGON	SW ABALONE ST. & 30TH ST. ROADWAY IMPROVEMENTS	ELECTRICAL DETAILS - LIGHTING
Sheet No: E3	Date: July 2015	