



PLANNING COMMISSION WORK SESSION AGENDA
Monday, August 11, 2025 - 6:00 PM
City Hall, Council Chambers, 169 SW Coast Hwy, Newport, OR 97365

All public meetings of the City of Newport will be held in the City Council Chambers of the Newport City Hall, 169 SW Coast Highway, Newport. The meeting location is accessible to persons with disabilities. A request for an interpreter, or for other accommodations, should be made at least 48 hours in advance of the meeting to the City Recorder at 541.574.0613, or cityrecorder@newportoregon.gov.

All meetings are live-streamed at <https://newportoregon.gov>, and broadcast on Charter Channel 190. Anyone wishing to provide written public comment should send the comment to publiccomment@newportoregon.gov. Public comment must be received four hours prior to a scheduled meeting. For example, if a meeting is to be held at 3:00 P.M., the deadline to submit written comment is 11:00 A.M. If a meeting is scheduled to occur before noon, the written comment must be submitted by 5:00 P.M. the previous day. To provide virtual public comment during a city meeting, a request must be made to the meeting staff at least 24 hours prior to the start of the meeting. This provision applies only to public comment and presenters outside the area and/or unable to physically attend an in person meeting.

The agenda may be amended during the meeting to add or delete items, change the order of agenda items, or discuss any other business deemed necessary at the time of the meeting.

1. **Call to Order**
Bill Branigan, Bob Berman, Jim Hanselman, Gary East, Braulio Escobar, John Updike, Robert Bare, and Dustin Capri.
2. **Unfinished Business**
- 2.A **City Center Design Review District Form Based Code Concepts.**
[Memorandum](#)
[City Center Form Based Code Markup, dated 8.11.25](#)
[Urbsworks PowerPoint Presentation](#)
[Newport City Center Form Based Code Part 1](#)
[Newport City Center Form Based Code Part 2](#)
[City Center Parking Option Map](#)
3. **New Business**

3.A South Beach Island Annexation Boundary Map and Proposed Zoning.

[Memorandum](#)

[Project Sheets From South Beach Urban Renewal Plan](#)

[Draft Annexation Map](#)

[Draft Annexation Map With Aerial](#)

[Draft Annexation Legal Descriptions](#)

[Comprehensive Plan Designation Map](#)


[Proposed Zoning Map](#)

3.B Planning Commission Work Program Update.

[PC Work Program 8-6-25](#)

4. Adjournment

Memorandum

To: Planning Commission/Commission Advisory Committee
 From: Derrick Tokos, Community Development Director 
 Date: August 7, 2025
 Re: City Center Design Review District Form Based Code Concepts

This is a continuation of the July 28, 2025 work session, where Marcy McInelly, AIA, with Urbsworks, Inc., presented a form-based code concept for the City Center area. The new form based code has been tailored to complement the identified transportation solutions and support mixed-use development, with housing of various types, to create a walkable, live-work environment. Marcy will attend this work session virtually.

For the work session, I have placed the code revisions recommended by Urbsworks Inc. into our typical mark-up format. As a baseline, I used the draft that you reviewed for the housekeeping amendments, as it is the most current and will be adopted before the form based code makes its way to the City Council. There were several areas of overlap and I want to make sure we don't inadvertently delete recently adopted code changes because we were using an outdated base set of information. I also added a few additional amendments that were not picked up by Urbsworks, Inc.

In addition to the mark-up, your packet includes the slide presentation that Urbsworks, Inc. put together for the last work session, and the two files that collectively make up the form based code. I deleted the form based code appendices, since it is duplicative of the mark-up. Lastly, the packet includes a map that will be used as a discussion point for determining how the City Center Parking District should be amended to align, or not, with the new Design District.

The mark-up includes yellow highlighted language intended to flag discussion items. Please pay particular attention to these, as I hope to touch upon all of them at the work session.

Attachments

City Center Form Based Code Markup, dated 8.11.25
 Urbsworks PowerPoint Presentation
 Newport City Center Form Based Code Part 1
 Newport City Center Form Based Code Part 2
 City Center Parking Option Map

(Unless otherwise specified, new language is shown in double underline, and text to be removed is depicted with ~~strike through~~. Staff comments, in *italics*, are for context and are not a part of the revisions. **Highlighted language** denotes work session discussion items)

CHAPTER 14.03 ZONING DISTRICTS

14.03.050 Residential Uses.

The following list sets forth the uses allowed within the residential land use classification. Uses not identified herein are not allowed. Short-term rentals are permitted uses in the City of Newport's R-1, R-2, R-3 and R-4 zone districts subject to requirements of [Section 14.25](#).

"P" = Permitted uses.

"C" = Conditional uses; permitted subject to the approval of a conditional use permit.

"X" = Not allowed.

| A. | Residential | R-1 | R-2 ⁸ | R-3 | R-4 |
|----|--|-----|------------------|-----|-----|
| | 1. Single-Family | P | P | P | P |
| | 2. Two-family ⁷ | P | P | P | P |
| | 3. Townhouse | X | P | P | P |
| | 4. Single Room Occupancy ^{4, 6} | P | P | P | P |
| | 5. Cottage Cluster | X | X | P | P |
| | 6. Multi-family | X | X | P | P |
| | 7. Manufactured Homes ¹ | P | P | P | P |
| | 8. Manufactured Dwelling Park | X | P | P | P |
| B. | Accessory Dwelling Units | P | P | P | P |
| | (B. was added on the adoption of Ordinance No 2055 on June 17, 2013; and subsequent sections relettered accordingly. Effective July 17, 2013.) | | | | |
| C. | Accessory Uses | P | P | P | P |
| D. | Home Occupations | P | P | P | P |
| E. | Community Services | | | | |
| | 1. Parks | P | P | P | P |
| | 2. Publicly Owned Recreation Facilities | C | C | C | C |
| | 3. Libraries | C | C | C | C |
| | 4. Utility Substations | C | C | C | C |
| | 5. Public or Private Schools | C | C | C | P |
| | 6. Family Child Care Home | P | P | P | P |
| | 7. Child Care Center | C | C | C | C |
| | 8. Religious Institutions/Places of Worship | C | C | C | C |

| | | | | | |
|----|--|---|---|---|---|
| | 9. Emergency Shelter ⁵ | P | P | P | P |
| F. | Residential Care Homes | P | P | P | P |
| G. | Nursing Homes | X | X | C | P |
| H. | Motels and Hotels ^{3.} | X | X | X | C |
| I. | Professional Offices | X | X | X | C |
| | | | | | |
| J. | Beauty and Barber Shops | X | X | X | C |
| K. | Colleges and Universities | C | C | C | C |
| L. | Hospitals | X | X | X | P |
| M. | Membership Organizations | X | X | X | p |
| N. | Museums | X | X | X | P |
| O. | Condominiums ² | X | P | P | P |
| P. | Hostels | X | X | X | C |
| Q. | Golf Courses | C | C | C | X |
| R. | Recreational Vehicle Parks | X | X | X | C |
| S. | Necessary Public Utilities and Public Service Uses or Structures | C | C | C | C |
| T. | Residential Facility* | X | X | P | P |
| U. | Movies Theaters** | X | X | X | C |
| V. | Assisted Living Facilities*** | X | P | P | P |
| W. | Bicycle Shop**** | X | X | X | C |
| X. | Short-Term Rentals (subject to requirements of Chapter 14.25) | P | P | P | P |
| Y. | Transportation Facilities | P | P | P | P |

¹ Manufactured homes may be located on lots, parcels or tracts outside of a manufactured dwelling park subject to the provisions listed in NMC 14.06.020.

² Condominiums are a form of ownership allowed in all zones within dwelling types otherwise permitted pursuant to subsection (A).

³ Hotels/motels units may be converted to affordable housing provided they are outside of the Tsunami Hazard Overlay Zone.

⁴ A building with four to six units on a lot or parcel in an R-1 or R-2 zone district, or a combination of buildings of at least four units each subject to the density limitations of an R-3 or R-4 zone district.

⁵ Subject to a public hearing before the Newport City Council to establish compliance with the requirements of ORS 197.782.

⁶ Density limit shall be three times the maximum number of multi-family units for lots or parcels where five or more multi-family units are allowed.

⁷ If one or more of the units is an accessible or affordable unit, then one additional attached or detached dwelling unit shall be permitted notwithstanding the density limits of the zone district.

⁸ Cottage Clusters, Multi-family, and Single Room Occupancy are permitted within the City Center Design Review District (CCDRD), subject to the form based provisions of the CCDRD.

Staff: City Center Revitalization Plan Revisions. R-2 is limited to Benton/10th Street and SE 2nd Street.

14.03.070 Commercial and Industrial Uses.

The following list sets forth the uses allowed within the commercial and industrial land use categories.

“P” = Permitted uses.

“C” = Conditional uses; allowed only after the issuance of a conditional use permit.

“X” = Not allowed.

| | | C-1 | C-2 ¹ | C-3 | I-1 | I-2 | I-3 |
|----|---|-----|------------------|-----|-----|-----|-----|
| 1. | Office | P | X | P | P | P | X |
| 2. | Retails Sales and Service | | | | | | |
| | a. Sales-oriented, general retail | P | P | P | P | P | C |
| | b. Sales-oriented, bulk retail | C | X | P | P | P | C |
| | c. Personal Services | P | C | P | P | C | X |
| | d. Entertainment | P | P ² | P | P | C | X |
| | e. Repair-oriented | P | X | P | P | P | X |
| 3. | Major Event Entertainment | C | C | P | P | C | X |
| 4. | Vehicle Repair | C | X | P | P | P | X |
| 5. | Self-Service Storage ⁶ | X | X | P | P | P | X |
| 6. | Parking Facility | P | P | P | P | P | P |
| 7. | Contractors and Industrial Service ⁶ | X | X | P | P | P | P |
| 8. | Manufacturing and Production | | | | | | |
| | a. Custom Creative Work ⁸ | P | P | P | P | C | C |
| | b. Light Manufacturing | X | X | C | P | P | P |

| | | | | | | | |
|-----|---|---|---|---|---|---|---|
| | c. Heavy Manufacturing | X | X | X | X | C | P |
| 9. | Warehouse, Freight Movement, & Distribution | X | X | P | P | P | P |
| 10. | Wholesale Sales | X | X | P | P | P | P |
| 11. | Waste and Recycling Related | C | C | C | C | C | C |
| 12. | Basic Utilities ³ | P | P | P | P | P | P |
| 13. | Utility Corridors | C | C | C | C | C | C |
| 14. | Community Service ^{7,8} | P | C | P | P | C | X |
| 15. | Family Child Care Home | P | P | P | X | X | X |
| 16. | Child Care Center | P | P | P | P | P | X |
| 17. | Educational Institutions | | | | | | |
| | a. Elementary & Secondary Schools | C | C | C | X | X | X |
| | b. College & Universities | P | X | P | X | X | X |
| | c. Trade/Vocational Schools/Other | P | X | P | P | P | P |
| 18. | Hospitals | C | C | C | X | X | X |
| 19. | Courts, Jails, and Detention Facilities | X | X | P | C | X | X |
| 20. | Mining | | | | | | |
| | a. Sand & Gravel | X | X | X | X | C | P |
| | b. Crushed Rock | X | X | X | X | X | P |
| | c. Non-Metallic Minerals | X | X | X | X | C | P |
| | d. All Others | X | X | X | X | X | X |
| 21. | Communication Facilities ⁴ | P | X | P | P | P | P |
| 22. | Residences on Floors Other than Street Grade ⁹ | P | P | P | X | X | X |
| 23. | Affordable Housing ⁵ | P | P | P | P | X | X |
| 24. | Transportation Facilities | P | P | P | P | P | P |

1. Any new or expanded outright permitted commercial use in the C-2 zone district that exceeds 2,000 square feet of gross floor area. New or expanded uses in excess of 2,000 square feet of gross floor area may be permitted in accordance with the provisions of Chapter 14.34, Conditional Uses. Residential uses within the C-2 zone are subject to special zoning standards as set forth in Section 14.30.100.

2. Recreational Vehicle Parks are prohibited on C-1, C-2, and C-3 zoned property within the Historic Nye Beach and City Center Design Review Districts.

3. Small wireless facilities shall be subject to design standards as adopted by City Council resolution.

4. Communication facilities located on historic buildings or sites, as defined in Section 14.23, shall be subject to conditional use review for compliance with criteria outlined in Sections 14.23 and 14.34.

5. Permitted as outlined in Chapter 14.15 or, in the case of hotels/motels, the units may be converted to affordable housing provided they are outside of the Tsunami Hazard Overlay Zone defined in NMC Chapter 14.50.

6. Self-service storage use; salvage or wrecking of heavy machinery, metal and building materials; towing and vehicle storage; and auto and truck salvage and wrecking are prohibited within the South Beach Transportation Overlay Zone, as defined in Section 14.43.020.

7 For emergency shelters subject to ORS 197.782, city staff shall determine if standards listed under ORS 197.782 have been satisfied when the shelter is located in a zone where community service uses are listed as permitted. In those zones where community service uses are listed as conditional, a public hearing shall be held by the Newport City Council to establish compliance with statutory requirements.

8. Transitional housing as defined in ORS 197.746 must be operated by a public or non-profit entity, with residential tenancy limited to a period of time that is not more than 30 days.

9 Residential uses, including on the ground floor of buildings, is permitted on C-1 and C-3 zoned land within the CDDR, subject to the form based provisions of the CDDR

Staff: Change include allowance for residential use, including at street grade for C-1 and C-3 property inside the City Center Design Review District. Recreational vehicle parks, which are an entertainment oriented commercial use, will be prohibited in both the Nye Beach and City Center Design Review Districts.

14.03.100 Public Uses

The following list sets forth the uses allowed within the public land use classification. Uses not identified herein are not allowed.

"P" = Permitted Uses.

"C" = Conditional uses; permitted subject to the approval of a conditional use permit.

"X" = Not allowed.

| | | P-1 ¹ | P2 | P-3 |
|-----|--|------------------|----|-----|
| 1. | Public Parks | P | P | P |
| 2. | Public Open Space | P | P | P |
| 3. | Public Schools, Colleges, or Universities | P | X | X |
| 4. | Any Building or Structure Erected by a Governmental Entity | P | X | X |
| 5. | Community Buildings | P | X | X |
| 6. | Fairgrounds | P | X | X |
| 7. | Public Cemeteries | P | P | X |
| 8. | Water & Wastewater Treatment Plants | P | X | X |
| 9. | Performing Arts Centers | P | X | X |
| 10. | Visual Arts Centers | P | X | X |
| 11. | Senior Centers | P | X | X |
| 12. | Airport and Accessory Structures | P | X | X |
| 13. | Public Golf Courses | P | P | X |
| 14. | City Halls | P | X | X |
| 15. | County Courthouses | P | X | X |
| 16. | Jails and Juvenile Detention Facilities | P | X | X |
| 17. | City or County Maintenance Facilities | P | X | X |
| 18. | Publicly Owned Recreational Vehicle Parks | C | C | X |
| 19. | Public Museums | P | X | X |
| 20. | Public Restrooms | P | P | X |
| 21. | Recreation Equipment | P | P | X |
| 22. | Post Office | P | X | X |
| 23. | Parking Lots | P | P | X |
| 24. | Public Hospitals | P | X | X |
| 25. | Transportation Facilities | P | P | P |
| 26. | Water Storage Facilities | P | X | X |
| 27. | Public Libraries | P | X | X |
| 28. | Fire Stations | P | X | X |
| 29. | Police Stations | P | X | X |
| 30. | Accessory Structures for Any of the Above | P | P | P |

¹ Residential uses are permitted within the P-1 zone in the City Center Design Review District (CCDRD), subject to the form based provisions of the CCDRD.

Staff: Adds a footnote indicating that residential uses are permitted on P-1 zoned property subject to the form based provisions of the CCDRD.

CHAPTER 14.06 MANUFACTURED DWELLINGS, PREFABRICATED STRUCTURES, SMALL HOMES AND RECREATIONAL VEHICLES

14.06.060 Recreational Vehicle Parks

Recreational vehicle parks are allowed conditionally in an R-4 or I-2 zone district, and conditionally if publicly owned in the P-1 and P-2 zoning districts (excluding those P-1 properties within the Historic Nye Beach Design Review District and the Traditional Downtown Zone area within the City Center Design Review District), subject to subsections A through D below and in accordance with [Section 14.52](#), Procedural Requirements. Recreational vehicle parks are allowed outright in C-1, C-2, C-3, and I-1 zoning districts (excluding those C-2 properties within the Historic Nye Beach Design Review District and the City Center Design Review District), subject to the subsections A through D as follows:

Staff: Language prohibits the establishment of new recreational vehicle parks in the commercial zoned portions of the CCDRD, and public zoned areas within the traditional downtown. P-1 zoned areas outside of the traditional downtown, such as the Fairgrounds, would still be available for recreational vehicle park development.

CHAPTER 14.09 TEMPORARY USES

14.09.050 Mobile Food Units

Notwithstanding any other restrictions and prohibitions in this code, a mobile food unit, not associated with a special event, may be located within the City of Newport subject to the following:

- C. The lot, parcel or tract upon which the mobile food unit will be placed is located outside of the Nye Beach, **City Center**, or Bayfront parking districts, the geographic boundaries of

which are defined in NMC Section 14.14.100, unless the use is a Mobile Food Unit Pod; and

Staff: Added language makes limitations on Mobile Food Units the same for the City Center Parking District as they currently are for Nye Beach and the Bayfront, where they are only permitted as Pods. This might work best if the limitation is tied to the traditional downtown area of the City Center Design District rather than the parking district. It depends upon whether or not the

CHAPTER 14.13 DENSITY LIMITATIONS

14.13.010 Density Limitations

A residential building structure or portion thereof hereafter erected shall not exceed the maximum living unit density listed in Table A, as hereinafter set forth, for the zone indicated, unless an exception is granted via an adjustment or variance as described in Section 14.33 of this Code, and consistent with Section 14.52, Procedural Requirements.

(BY THIS REFERENCE, THERE IS INCLUDED HEREIN AND MADE A PART HEREOF, A TABLE OF DENSITY AND OTHER REQUIREMENTS, DESIGNATED "TABLE A".)

NMC 14.13.020

Table "A"

| Zone District | Min. Lot Area (sf) | Min. Width | Required Setbacks ^{3,7} | | | Lot Coverage (%) | Max. Building Height | Density (Land Area Required Per Unit (sf)) |
|-------------------|-----------------------|------------|--|-------------|-------|------------------|-----------------------------|--|
| | | | Front/2 nd Front ¹ | Side | Rear | | | |
| R-1 | 7,500 sf | 65-ft | 15-ft / 15-ft or 20-ft / 10-ft | 5-ft & 8-ft | 15-ft | 54 % | 30-ft | SFD - 7,500 sf ² Duplex - 3,750 sf ² |
| R-2 ¹⁰ | 5,000 sf ³ | 50-ft | 15-ft / 15-ft or 20-ft / 10-ft | 5-ft | 10-ft | 57% | 30-ft | SFD - 5,000 sf ² Duplex - 2,500 sf ² Townhouse - 2,500 sf ³ |
| R-3 | 5,000 sf ³ | 50-ft | 15-ft / 15-ft or 20-ft / 10-ft | 5-ft | 10-ft | 60% | 35-ft or 40-ft ⁹ | 1,250 sf ³ |

| | | | | | | | | |
|------------------------|-----------------------|-------|--|------|-------|---------------------|------------------------------------|--------------------------|
| R-4 ^{4, 10} | 5,000 sf ³ | 50-ft | 15-ft / 15-ft or 20-ft / 10-ft | 5-ft | 10-ft | 64% | 35-ft or 40- ft ⁹ | 1,250 sf ^{3, 5} |
| C-1 ¹⁰ | 5,000 sf | 0 | 0 or 15-ft from US 101 ⁸ | 0 | 0 | 85-90% ⁶ | 50-ft ⁶ | n/a |
| C-2 ⁴ | 5,000 sf | 0 | 0 or 15-ft from US 101 ⁸ | 0 | 0 | 85-90% ⁶ | 50-ft ⁶ | n/a |
| C-3 ¹⁰ | 5,000 sf | 0 | 0 or 15-ft from US 101 ⁸ | 0 | 0 | 85-90% ⁶ | 50-ft ⁶ | n/a |
| I-1 | 5,000 sf | 0 | 15-ft from US 101 | 0 | 0 | 85-90% ⁶ | 50-ft ⁶ | n/a |
| I-2 | 20,000 sf | 0 | 15-ft from US 101 | 0 | 0 | 85-90% ⁶ | 50-ft ⁶ | n/a |
| I-3 | 5 acres | 0 | 15-ft from US 101 | 0 | 0 | 85-90% ⁶ | 50-ft ⁶ | n/a |
| W-1 | 0 | 0 | 0 | 0 | 0 | 85-90% ⁶ | 40-ft ⁶ | n/a |
| W-2 | 0 | 0 | 0 | 0 | 0 | 85-90% ⁶ | 35-ft ⁶ | n/a |
| E-C, E-D and E-N | 0 | 0 | 0 | 0 | 0 | 100% | 40-ft ⁶ | n/a |
| P-1 ¹⁰ | 0 | 0 | 0 | 0 | 0 | 100% | 50-ft | n/a |
| P-2 | 0 | 0 | 0 | 0 | 0 | 100% | 35-ft | n/a |
| P-3 | 0 | 0 | 0 | 0 | 0 | 100% | 30-ft | n/a |

¹ Front and second front yards shall equal a combined total of 30-feet. Garages and carports shall be setback at least 20-feet from the access street for all residential structures.

² Density limitations apply where there is construction of more than one single-family dwelling (SFD) or duplex on a lot or parcel.

³ Density limitations for townhouses and cottage clusters is the minimum area required per townhouse or cottage cluster unit; whereas, minimum lot area, minimum lot width, and setbacks, apply to the perimeter of the lot, parcel, or tract dedicated to the townhouse or cottage cluster project.

⁴ Special Zoning Standards apply to R-4 and C-2 zoned property within the Historic Nye Beach design Review District as outlined in NMC 14.30.100.

⁵ Density of hotels, motels, and non-residential units shall be one unit for every 750 sf of land area.

⁶ Height limitations, setbacks, and lot coverage requirements for property adjacent to residential zones are subject to the height and yard buffer requirements of NMC Section 14.18.

⁷ Front and 2nd front setbacks for a townhouse project or cottage cluster project shall be 10-feet except that garages and carports shall be setback a distance of 20-feet.

⁸ The 15-foot setback from US 101 applies only to land situated south of the Yaquina Bay Bridge.

⁹ The 40-ft height allowance is limited to multi-family uses with pitched roof construction, where the predominate roof pitch is 4:12 or steeper, and where no adjustments are being sought under the provisions of NMC Chapter 14.51.

¹⁰ Special Zoning Standards apply to C-1, C-3, P-1, R-2, and R-4 zoned property within the City Center Design Review District as outlined in Section 14.30.200.

Staff: New footnote has been added to note that standards in Section 14.30.200 will govern if they differ from the provisions in Table A

CHAPTER 14.14 PARKING AND LOADING REQUIREMENTS

14.14.100 Special Area Parking Requirements

A. The boundary of the special areas are defined as follows:

1. City Center. That area bounded by SW Fall Street, SW 7th Street, SW Neff Way, SW Alder Street, SW 2nd Street, SW Nye Street, Olive Street, SE Benton Street, SW 10th Street, SW Angle Street, SW 11th Street, SW Hubert Street, and SW 10th Street.

Staff: Corrected bad street reference. Planning Commission Discussion is needed to right-size the parking district. Should it be the same size as the Design District or smaller, such as the boundary of just the traditional downtown component of the Design District? A map showing the differences is enclosed.

CHAPTER 14.15 RESIDENTIAL USES IN NONRESIDENTIAL ZONING DISTRICTS

14.15.010 Purpose

It is the intent of this section to regulate the placement of residences in nonresidential zoning districts.

14.15.020 Residential Uses in Nonresidential Zoning Districts

Residences shall be allowed in nonresidential zones as follows:

- A. **C-1 zones:** Residences are prohibited at street grade, except in the City Center Design Review District, where they are a use permitted outright, subject to the form based provisions of the CCDRD. For floors other than street grade, residences are allowed as an outright permitted use.
- B. **C-2 zones:** For areas outside of the Historic Nye Beach Design Review District, residences are prohibited at street grade. For floors other than street grade, residences are allowed as an outright permitted use. On lands zoned C-2 that are within the Historic Nye Beach Design Review District, residential uses shall be allowed as specified in Chapter 14.30, Design Review Standards.
- C. **C-3 zones:** Same as the C-1 zone.
- D. **For all I zones:** One residence for a caretaker or watchman as an accessory use is allowed as a permitted use.
- E. **W-2 zones:** Residences are prohibited at street grade. For floors other than street grade, residences are allowed subject to the issuance of a conditional use permit in accordance with the provisions of [Section 14.34](#), Conditional Uses, and [Section 14.52, Procedural Requirements](#).
- F. **For all other nonresidential zones:** Residences are prohibited.

G. **Affordable Housing:** Notwithstanding other provisions of this section, Affordable Housing shall be permitted at street grade provided:

1. It is situated outside of the Tsunami Hazards Overlay Zone; and
 - a. The property is zoned for commercial or public use, as outlined in Section 14.03.020; or
 - b. The property is zoned I-1, is publicly owned, and is adjacent to land zoned for residential use or a school.
2. Development standards for Affordable Housing under this sub-section shall be the same as those that apply to the adjacent residentially zoned property. If there is no adjacent land zoned for residential use, then the development standards of the R-4 zone shall apply.
3. Affordable Housing on property within the Historic Nye Beach Design Review District, shall satisfy the development standards specified in Chapter 14.30, Design Review Standards.

Staff: Change to Section 14.15.020(A) allows residential use outright at the street grade subject to the Design District standards.

CHAPTER 14.18 SCREENING AND BUFFERING BETWEEN RESIDENTIAL AND NONRESIDENTIAL ZONES

14.18.010 Height Buffer

Non-residential sites of districts abutting or having any portion located adjacent to any residential zone shall have a height limitation beginning at a height of ten feet at the property line abutting the residential zone and increasing at a slope of 1:2 for R-1 property, 1:1 for R-2 property, 2:1 for R-3 property, and 3:1 for R-4 property until intersecting the height limit otherwise established in that district.

14.18.020 Adjacent Yard Buffer

On any portion of a site in a non-residential zone that abuts a residential zone, a minimum interior yard of 10 feet planted and maintained as a landscaped screen shall be required.

14.18.030 Separated Yard Buffer

On any portion of a non-residential site that is opposite from a residential district and separated therefrom by a street, alley, creek, drainage facility, or other open area, a minimum yard of ten feet shall be required. The minimum yard shall be planted and maintained as a landscape screen (excluding areas required for access to the site).

Staff: Discussion point. Do these provisions make sense for the new City Center Design District. If not, then language should be added making it clear that they do not apply.

CHAPTER 14.19 LANDSCAPING REQUIREMENT

14.19.020 Definitions

For purposes of this section, the following definitions shall apply. Where no definition is given, the common usage of the word shall be used. If there is a conflict between the definitions contained in this section and the more general definitions contained in the definitions section of this Ordinance, this section shall apply.

D. City Center. The area of the city defined in Section 14.14.100(C) of this Code.

Staff: City Center, Bayfront and Nye Beach allow alternative landscaping, like planter and window boxes as opposed to typical green strips due to their more compact development form. This item is flagged since it relates to how the boundary of the City center area should be redefined.

CHAPTER 14.30 DESIGN REVIEW STANDARDS

14.30.010 Purpose

Design review districts may be adopted by the City of Newport in accordance with applicable procedures to ensure the continued livability of the community by implementing standards of design for both areas of new development and areas of redevelopment. Design review is an important exercise of the power of the City to regulate for the general welfare by focusing on how the built environment shapes the character of the community.

The Newport Comprehensive Plan identifies five potential urban design districts within the Newport Peninsula including the City Center District, Waterfront District, Nye Beach District, Upland Residential District, and the Oceanfront Lodging/Residential District. Additionally, neighborhood plans may be adopted for other areas of Newport that include as an objective the implementation of design review to maintain and/or provide a flexible approach to development by offering two methods of design review from which an applicant can choose. One method of design review is under clear and objective design standards and procedures to allow development that is consistent with the standards to occur with certainty in a timely and cost effective manner. A second alternative method of design review is review under design guidelines, which are a more flexible process for proposals that are creative/innovative and meet the identified guidelines of the applicable design review district.

It is further the purpose of these standards to:

- A. Preserve the beautiful natural setting and the orientation of development and public improvements in order to strengthen their relationship to that setting.
- B. Enhance new and redeveloping architectural and landscape resources to preserve and strengthen the interconnected, historic, scenic and/or aesthetic character and function of each district and/or neighborhood setting.
- C. Improve the vehicular and pedestrian networks in order to improve safety, efficiency, continuity, and relationships connecting Newport neighborhoods.

- D. Facilitate a built form that is compatible with, and complements, planned highway projects.
- E. Improve cohesion and livability of each district and/or neighborhood setting by enhancing its function, character, and relationship to its natural setting and orientation.
- F. Preserve and strengthen the ability of Newport institutions to continue as centers of employment.
- G. Strengthen Newport's economic vitality by improving its desirability through improved appearance, function, and efficiency.
- H. Improve the built environment in order to strengthen the visual appearance and attractiveness of developed areas.
- I. Preserve and enhance the existing housing supply, encourage the increase of affordable housing, and balance the need for all types of additional housing.
- J. Implement the goals and objectives of the adopted neighborhood plans.

Staff: This section has been amended to pickup the updated Comprehensive Plan language the Planning Commission recommended for approval at it 7/28/25 meeting. It also identifies that a discretionary process, with guidelines, will be an option in addition to clear and objective standards. Urbsworks is developing a set of guidelines for your review at the August 25, 2025 work session.

14.30.020 Design Review Districts: Overlay Zones Established

The following:

- A. Historic Nye Beach Design Review District. The Historic Nye Beach Design Review District Overlay Zone shall be indicated on the Zoning Map of the City of Newport with the letters HNBO and is the area described as follows:

Beginning at the northeasterly corner of SW Hubert Street and SW 2nd Street; thence westerly along the north line of SW 2nd Street to the west line of SW Dolphin Street, said point also being the southeast corner of Lot 1, Block B, Barlow Blocks

Addition to the City of Newport; thence north along the west line of SW Dolphin Street to 10 feet beyond the north line of Lot 7, said Barlow Blocks Addition; thence westerly, 10 feet north of and parallel with said north line of Lot 7 to the Pacific Ocean; thence northerly along the Pacific Ocean to the south line of NW 12th Street; thence east along the south line of NW 12th Street to the east line of an alley between NW Spring Street and NW Hurbert Street; thence south along the east line of said alley way to the north line of NW 10th Street; thence southwesterly to the southwest corner of the intersection of NW 10th Street and NW Brook Street; thence south along the west line of NW Brook Street to the south line of NW 8th Street; thence east along the south line of NW 8th Street to the west line of NW Hurbert Street; thence south along the west line of NW Hurbert Street to the north line of NW 6th Street; thence east to the northeast intersection of NW 6th Street and NW Hurbert Street; thence south along the east line of NW Hurbert Street and SW Hurbert Street to the north line of SW 2nd Street and the point of beginning.

B. City Center Design Review District. The City Center Design Review District Overlay Zone shall be indicated on the Zoning Map of the City of Newport with the letters CCDRD and is the area described as follows:

The City Center Design Review District is US Highway 101 and adjacent properties between the north end of the Yaquina Bay Bridge and US Highway 20, and US Highway 20 and adjacent properties between US Highway 101 and the eastern city limits.

Staff: This legal description will be updated prior to the hearing such that it has the same level of detail as the above description for the Nye Beach Design District.

14.30.030 Adoption of Design Review: Guidelines and Standards

The following design guidelines and standards are hereby adopted by reference and made a part hereof:

A. The document entitled “Newport Design Review: Guidelines and Standards” dated July 29, 2015. The guidelines and standards contained therein shall be the guidelines and standards applicable to the Historic Nye Beach Design Review District.

B. The document entitled “City Center Design Review District Form Based Code ” dated [September 2025]. The Regulating Plan, Public Realm Standards, and Development and Design Standards, contained therein shall be the standards applicable to the City Center Design Review District.

Staff: This section has been amended to reference the clear and objective standards that will apply in City Center Design Review District. It is the June 2025 draft that you are reviewing at this work session, which will be updated to address Planning Commission comments from the 7/28/25 and 8/11/25 work sessions.

14.30.040 Design Review Required

The following development activities in an established design review district are required to obtain a design review permit under the design standards in an identified design review district or, in the alternative, to apply for a design review permit and to obtain approval under the design guidelines for that design review district:

- A. New construction, substantial improvement, or relocation of one or more dwelling units.
- B. New construction, substantial improvement, or relocation of a commercial or public/institutional building.
- C. New construction, substantial improvement, or relocation of a residential accessory structure that contains more than 200 square feet of gross floor area and is not more than 10 feet in height.
- D. New construction, substantial improvement, or relocation of a commercial accessory structure that contains more than 120 square feet of gross floor area.
- E. An addition that increases the footprint of an existing building by more than 1,000 square feet.

Staff: No changes are recommended; however, the Planning Commission should confirm that these thresholds, which currently apply to Nye Beach, are also appropriate for the City Center Design District.

14.30.050 Exemptions

The following activities are exempt from the provisions of this chapter:

- A. Development activity that is subject to the provisions of Newport Municipal Code [Chapter 14.23](#), Historic Buildings and Sites.
- B. Any project for improvement of a structure to comply with existing state or local health, sanitary, or safety code specifications that are solely necessary to assure safe living conditions.
- C. Development that does not involve the construction, substantial improvement, or relocation of a dwelling unit, commercial or public/institutional building, or accessory structure.
- D. Conversion of space within an existing structure into an accessory dwelling unit.

Staff: No changes are recommended; however, the Planning Commission should confirm that these thresholds, which currently apply to Nye Beach, are also appropriate for the City Center Design District.

14.30.060 Approval Authority

The following are the initial review authorities for a Design Review application:

- A. Community Development Director. For projects subject only to the design standards specified in [the following documents, entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015. The](#) approval or denial of a Design Review application by the Community Development Director is a ministerial action performed concurrent with City review of a building permit.

1. Entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015.
2. Entitled “City Center Design Review District Form Based Code,” dated [September 2025].

Staff: Establishes that approval or denial of a Design Review application using the form based code’s clear and objective standards is a ministerial action.

- B. Planning Commission. For projects that require design review under the design guidelines contained in the document entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015, including the following:

1. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure that is over 65 feet in length or 35 feet in height; or
2. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure that does not meet the design standards contained in the document entitled “Newport Design Review: Guidelines and Standards” dated July 29, 2015; or
3. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure that involves a conditional use, a variance, or any other type of land use permit for which a Type III Land Use Action decision process is required, pursuant to [Chapter 14.52](#), Procedural Requirements.

Staff: These are the current thresholds that require discretionary guideline review in the Nye Beach Design District. The Planning Commission should consider whether or not they are appropriate for the City Center Design District.

14.30.100 Special Zoning Standards in Design Review Districts

All zoning standards and requirements applicable under Ordinance No. 1308 (as amended) in the subject zoning district shall apply, except that the following additional zoning standards are applicable for the design review district as applicable in the underlying zoning designation and shall be modified for each district as specified.

City Center Design Review District

1. No drive through windows are allowed within the area identifies as the Traditional Downtown Zone as identified in the City Center Design Review District Form Based Code.
2. Adjustments to Chapter 14.12 (Minimum Size) and Chapter 14.13 (Density Limitations, Table "A") are allowed within the City Center Design Review District as detailed in the City Center Design Review District Form Based Code, subsection 5, Development and Design Standards.

Staff: Adds the City Center Design District to areas subject to special zoning standards and references the form based code. Additionally, the changes allow for adjustments and prohibit drive through windows in the Traditional Downtown portion of the District.

CHAPTER 14.44 TRANSPORTATION STANDARDS

14.44.060 Streets, Pathways, Accessways, and Trails

- A. Street Width and Cross Sections. Right-of-way and roadway widths shall conform to the Minimum Street Cross-Sections in the Transportation System Plan and the standards in Table 14.44.060-A, except as modified in the document entitled "City

**Center Design Review District Form Based Code ” dated
[September 2025].**

Table 14.44.060-A. Minimum Right of Way and Roadway Widths

| Functional Classification | Minimum Right-of-Way Width | Minimum Roadway Width |
|---------------------------|----------------------------|-----------------------|
| Major Collector | 70-ft | 48-ft |
| Neighborhood Collector | 50-ft | 36-ft |
| Local | 50-ft | 36-ft |
| Yield Street | 40-ft | 24-ft |
| Shared Street | 30-ft | 16-ft. ¹ |

¹. A 12-ft width may apply to local streets that carry fewer than 150 vehicles per day.

B. Travel Lane and On-Street Parking. Travel lanes and on-street parking areas shall be sized in accordance with the standards in Table 14.44.060-B, except as modified in the document entitled “City Center Design Review District Form Based Code ” dated [September 2025].

Table 14.44.060-B. Minimum On-Street Parking and Roadway Widths

| Roadway Classification | Arterial Street ¹ | Major Collector | Neighborhood Collector | Local Street | Yield / Shared Street ² |
|------------------------------|------------------------------|---------------------|------------------------|----------------------|------------------------------------|
| Through Lanes | 2 to 4 | 2 | 2 | 2 | 1 |
| Min. Lane Width | 11-12 ft. ³ | 10 ft. ⁴ | 10 ft. ⁴ | 10 ft. | 12 – 16 ft. |
| Median/Center Turn Lane | 11-14 ft. ⁶ | 11 ft. ⁷ | 11 ft. ⁷ | None | None |
| Min. On-Street Parking Width | Context Dependent, 7-8 ft. | 8 ft. | 8 ft. | 7-8 ft. ⁸ | 7-ft one side ⁸ |

¹. Although guidance is provided for arterial streets, these are under State jurisdiction. Values presented in this table are consistent with ODOT’s urban design guidance. For detailed design recommendations on US 101 and US 20, the identified urban contexts for Newport are provided in the appendix and ODOT’s urban design guidance is publicly available.

². For use along low volume local streets in residential areas only, where no adjustments are being sought to off-street parking requirements as authorized under ORS Chapter 197A, and implemented in NMC Chapter 14.51. Yield streets are an option for new streets, while shared streets are an option for existing streets. Requires intermittent on-street parking on at least one side to allow for vehicle queuing and passing opportunities. For blocks of no more than 300 ft. in length, and with fire access roads at both ends, a 16 ft. width may apply to local streets that carry fewer than 500 vehicles per day, or a 12 ft. width may apply to local streets that carry fewer than 150 vehicles per day. For blocks longer than 300 feet, this also requires

30 ft. long pullouts/no parking zones every 150 ft. to allow for 20 ft. wide clear areas (excluding drainage swales) or 26 ft. wide clear areas near fire hydrants.

3. 11 ft. travel lanes are preferred for most urban contexts within Newport. 11 ft. travel lanes are standard for central business district areas in ODOT's urban design guidance. Adjustments may be required for freight reduction review routes. Final lane width recommendations are subject to review and approval by ODOT.
4. Travel lanes widths of 11-12 ft. are required along designated local truck routes.
5. A minimum 8-ft.-wide pedestrian refuge should be provided at marked crossings. Otherwise, a median can be reduced to a minimum of 4 ft. at midblock locations that are more than 150 ft. from an arterial (i.e., US 101 and US 20), before widening at intersections for left-turn lanes (where required or needed).
6. ODOT's urban design guidance recommends a 14 ft. lane for speeds above 40 mph. Final lane width recommendations are subject to review and approval by ODOT.
7. Center turn lane required at and within 150 ft. of intersections with arterials (i.e., US 101 and US 20). Otherwise, it is optional and should be used to facilitate turning movements and/or street crossings; minimum 8-ft-wide median required where refuge is needed for pedestrian/bicycle street crossings.
8. On-street parking is preferred along all City streets where block spacing, and system connectivity standards are met. An 8 ft. width is required in most areas, with a 7 ft. width only allowed along local streets in residential areas. Local yield/shared streets require intermittent on-street parking on at least one side to allow for vehicle queuing and passing opportunities, with an 8 ft. width required when on only one side, and 7 ft. width allowed when on both sides. Shoulders totaling 8 ft. in collective width may also be provided in lieu of parking

- I. **Sidewalks.** Sidewalks in conformance with the city's adopted sidewalk design standards are required as outlined in the adopted Transportation System Plan and Table 14.44.060(C) below, except as modified in the document entitled "City Center Design Review District Form Based Code" dated **September 2025**. Any modifications to the sidewalk standards require approval pursuant to the requirements of Section 14.33.100 - Transportation Mitigation Procedure. Requests for modifications involving ODOT facilities will require review and approval by ODOT.

Table 14.44.060-C. Minimum Sidewalk Widths

| Roadway Classification | Arterial Street ¹ | Major Collector (Commercial) | Major Collector (Non-Commercial) | Neighborhood Collector | Local/Yield Street ³ |
|------------------------|------------------------------|------------------------------|----------------------------------|------------------------|---------------------------------|
|------------------------|------------------------------|------------------------------|----------------------------------|------------------------|---------------------------------|

| | | | | | |
|---|-----------------------|--------------------|-------|---------|---------|
| Edge | 1-4 ft. | 0 ft. | 0 ft. | 0 ft. | 0 ft. |
| Pedestrian Throughway | 5-10 ft. | 8 ft. ⁴ | 6 ft. | 6 ft. | 5 ft. |
| Furnishings/Landscape (including curb) | 5.5-6.5 ft. | 3 ft. | 3 ft. | 0.5 ft. | 0.5 ft. |
| Min. Walkway Width | Variable ⁵ | 11 ft. | 9 ft. | 6.5 ft. | 5.5 ft. |
| Minimum Buffer (Pedestrian Throughway to Vehicle Travel Way) ² | Variable ⁵ | 3 ft. | 3 ft. | 0.5 ft. | 0.5 ft. |

¹. Minimum widths may be expanded in areas with enhanced pedestrian activity, or when identified as a project in the TSP or subsequently adopted refinement plan. For instance, the edge zone may need to be expanded to accommodate outdoor seating for the adjacent land use.

². Includes width of on-street parking, bike facilities, and furnishing/landscape zone.

³. Local streets that are also constructed as shared/yield streets do not require curbs and may include a 5 ft. shoulder walkway at street level, with the travel lanes and shoulders satisfying pedestrian needs. In constrained cases, the shoulder walkway may be provided on only one side, or eliminated.

⁴. In highly constrained locations, the landscape buffer may be eliminated to meet the required 8 ft. pedestrian throughway with approval from the City Engineer, City Engineer's designee or Community Development Director.

⁵. Desired walkway and buffer width for ODOT facilities depends on the urban context and are subject to review and approval by ODOT.

O. Shared Use Paths. A shared use path must be a minimum of 10 feet wide within 14 feet of right-of-way. In areas with significant walking or biking demand, as identified in the Newport Transportation System Plan (e.g., Nye Beach Area, Oregon Coast Bike Route) or on state facilities, the path must be 12 feet wide within a right-of-way of 16 feet (see Figure 14.44.060-A), unless an alternative width is shown in the Newport Transportation System Plan and approved by ODOT. A shared use path may be narrowed to 8 feet over short distances to address environmental or right-of-way constraints.

Figure 14.44.060-A, Pedestrian Trail, Accessway, & Shared Use Paths

| PEDESTRIAN TRAIL DESIGN | ACCESSWAY OR LOW USE SHARED USE PATH DESIGN ¹ | TYPICAL SHARED USE PATH DESIGN ² |
|---|---|---|
|  |  |  |

¹. For short segments, a low use shared use path can be as narrow as 8-feet wide, with a 1-foot shoulder on each side and a total right-of-way of 10 feet.

². Unless otherwise authorized by ODOT, a shared use path width of 12 feet is required parallel to ODOT facilities and may be applied in other areas with significant walking or biking demand (e.g. Nye Beach, Oregon Coast Bike Route).

Staff: The above Changes to Chapter 14.44 implement the “public realm” component of the document entitled “City Center Design Review District Form Based Code.”

City Center Design Review District Form Based Code

Planning
Commission Work
Session

July 28



AGENDA

- Orientation to the packet
- What is and why use a Form Based approach?
- Overview of the Form Based Code
- Newport Municipal Code (NMC) Amendments
- Discussion questions

ORIENTATION TO THE PACKET

- Discussion questions – page 6 of your packet
- Form Based Approach (introduction) – page 11 of your packet
- Applicability – page 13
- Regulating Plan – page 15
- Public Realm Standards (Street Cross Sections) – page 21
- Development and Design Standards – page 29
- Amendments to the Newport Municipal Code – page 66

Discussion questions – page 6

This document, the City Center Design Review District Form Based Code, represents the Newport Municipal Code (NCM) amendments necessary to implement the City of Newport City Center Revitalization Plan, and fulfills the TGM grant-funded project tasks related to code amendments.

Discussion

This document will be presented to the Newport Planning Commission at their July 2025 work session. On this and the following pages are issues for work session discussion.

“Retail Ready” Frontage Standard

The Regulating Plan (Section 3) currently shows “Retail-Ready” Frontage Type required on Hwy 101 and SW 9th, at all corners within the Traditional Downtown Zone. The “retail-ready Frontage Types is detailed on pages 40-43. Two alternative options for discussion are:

- » Only on side streets which have been identified as retail-focused streets– SW Hurbert, SW Alder, and SW Lee Streets—for the full length of the side street and wrapping the corners (e.g., 25 linear feet from each corner, or to the nearest property line, whichever is less).
- » On Hwy 101 only, (not on SW 9th) where 101 intersects with SW Hurbert, SW Alder, and SW Lee Streets and wrapping the corners (e.g., 25 linear feet from each corner or to nearest property line, whichever is less).

DISCUSSION QUESTIONS

“Retail Ready” Frontage Standard. The Regulating Plan (Section 3) currently shows “Retail-Ready” Frontage Type required on Hwy 101 and SW 9th, at all corners within the Traditional Downtown Zone. The “retail-ready” Frontage Types are detailed on pages 40-43. Two alternative options for discussion are:

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2. On Hwy 101 only, (not on SW 9th) where 101 intersects with SW Hurbert, SW Alder, and SW Lee Streets and wrapping the corners (e.g., 25 linear feet from each corner or to nearest property line, whichever is less).

DISCUSSION QUESTIONS

Encroachments. The CCDRD proposed provisions say: “Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.” See Urban Shopfront Frontage Type, page 40, for an example.

Encroachments of the kind mentioned are desirable for urban vitality and interest, especially in the Traditional Downtown Zone. The city permits encroachments on a very limited basis.

Discuss how the city allows encroachments and how the intent for architectural features (such as bay windows), and weather protection (awnings, marquees), can be allowed within the CCDRD.

DISCUSSION QUESTIONS

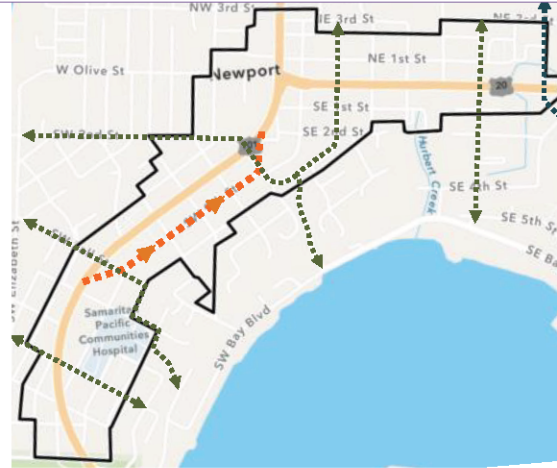
Architectural Features.

The CCDRD General Design Standards contain menus for architectural features but they are generic (see “Detailed Design,” page 34, for an example). The “Newport Design Review Glossary and Illustrations,” used for Historic Nye Beach Design Review District contains a more comprehensive listing along with illustrations. Besides being more comprehensive, these have the benefit of being in place for some time and are familiar to the community. While they are historic features many of them are timeless architectural features that can be rendered in a variety of architectural styles, from traditional to modern.

Options for discussion:

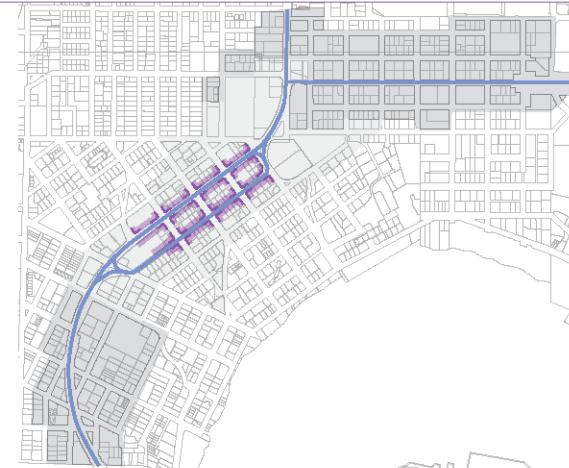
1. Keep the current generic listing (see “Detailed Design,” page 34).
2. Replicate or adapt certain features from the Newport Design Review Glossary and Illustrations, such as balcony, bay window, and/or turret. Combine these with the current generic listing.

Form Based Approach – page 11



City Center Design Review District (CCDRD)

The City Center Design Review District (CCDRD) is US Highway 101 and adjacent properties between the north end of the Yaquina Bay Bridge and US Highway 20, and US Highway 20 and adjacent properties between US Highway 101 and the eastern city limits.



CCDRD Form Based Code (FBC)

This document translates the vision created during the public involvement process into a series of land use, streetscape, and urban design strategies, using a form based approach.

Form based codes (FBCs) are development regulations used by local government agencies that emphasize the physical character of development and deemphasize the categorization of land uses and activities. They provide greater predictability about the look and feel of development and offer developers a clearer understanding of what the community seeks. In return, FBCs can make it easier for citizens to help create the physical development they want, which will more likely lead to their acceptance of development and street designs in their community.

The new 14.30 City Center Design Review District and amendments to the municipal code and Comprehensive Plan will:

FBC Approach

Promote a “form based” approach that emphasizes building shape, size, and relationship to streets and open spaces and de-emphasizes land use.

Use a “Regulating Plan” to translate the CCARP vision into a plan and map of the project area. The regulating plan will show where different development and design standards apply.

Utilize clear and objective dimensional standards. An example of a dimensional standard is “front yard setback – 20 feet.”

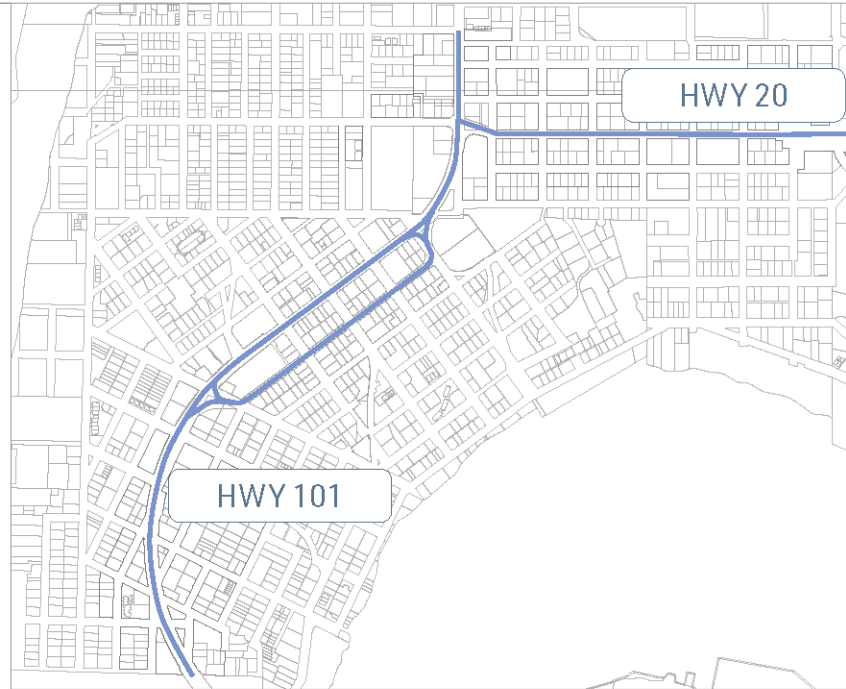
In general, provide as much information in tables as possible to aid clarity of the provisions and use graphics to communicate the intent of the development standards and provisions..

Display development and design standards in tables. The standards in the table will be keyed to the Regulating Plan, Frontage Types, or other diagrams.

Provide purpose or intent statements and visual examples to help explain the intended outcome.

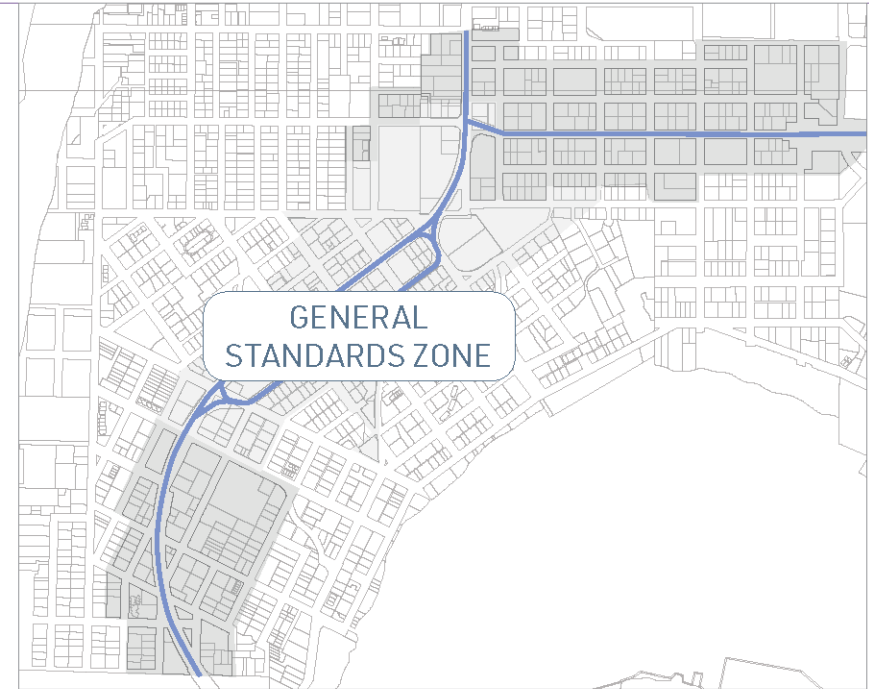
APPLICABILITY

Applicability – page 13



Public Realm Standards

Street Cross Sections (Section 04)

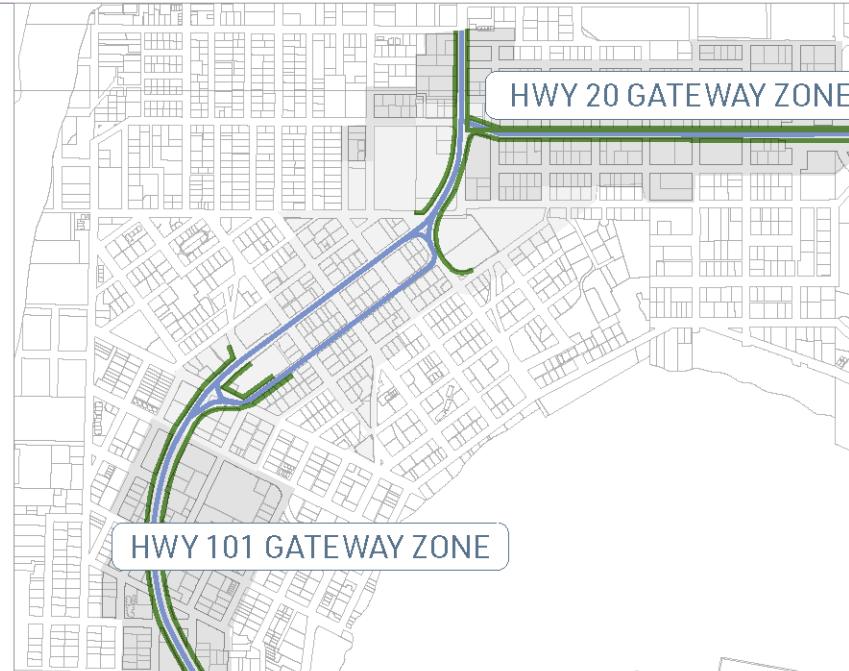


General Standards

Street Cross Sections (Section 04)

General Standards (Section 05)

APPLICABILITY

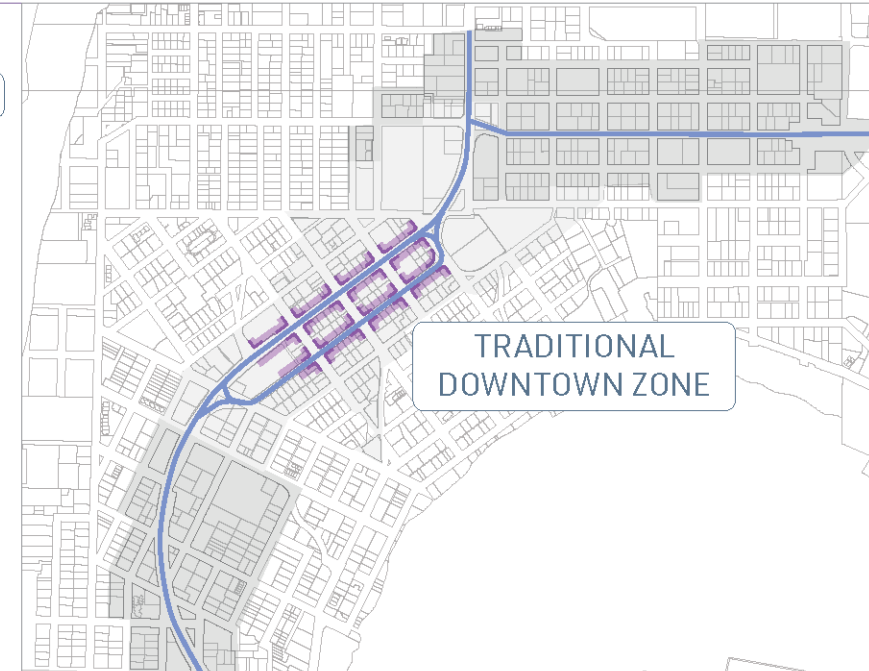


Gateway Frontage Types

Street Cross Sections (Section 04)

General Standards (Section 05)

Gateway Frontage Types (Section 5)



Traditional Downtown Frontage Types

Street Cross Sections (Section 04)

General Standards (Section 05)

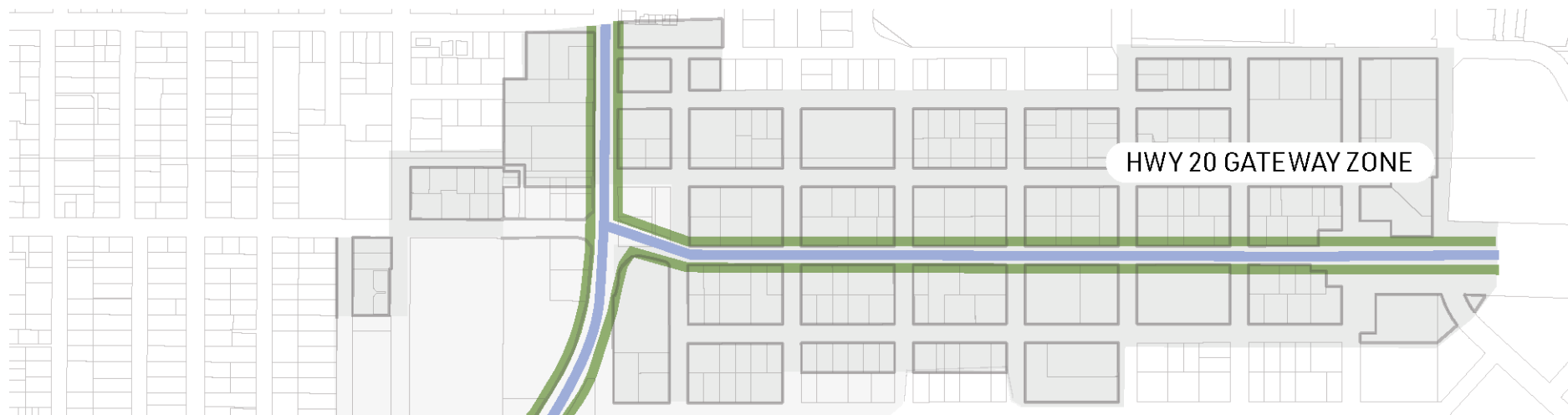
Gateway Frontage Types (Section 5)

Traditional Downtown Frontage Types (Section 5)

HWY 20 GATEWAY ZONE

Regulating Plan

– page 15

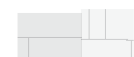


LEGEND

 URBAN FRONTAGE REQUIRED

 RETAIL-READY FRONTAGE REQUIREMENT

 LANDSCAPE FRONTAGE REQUIRED





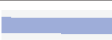
 GENERAL STANDARDS APPLY

 PRIMARY STREET

HWY 101 GATEWAY ZONE



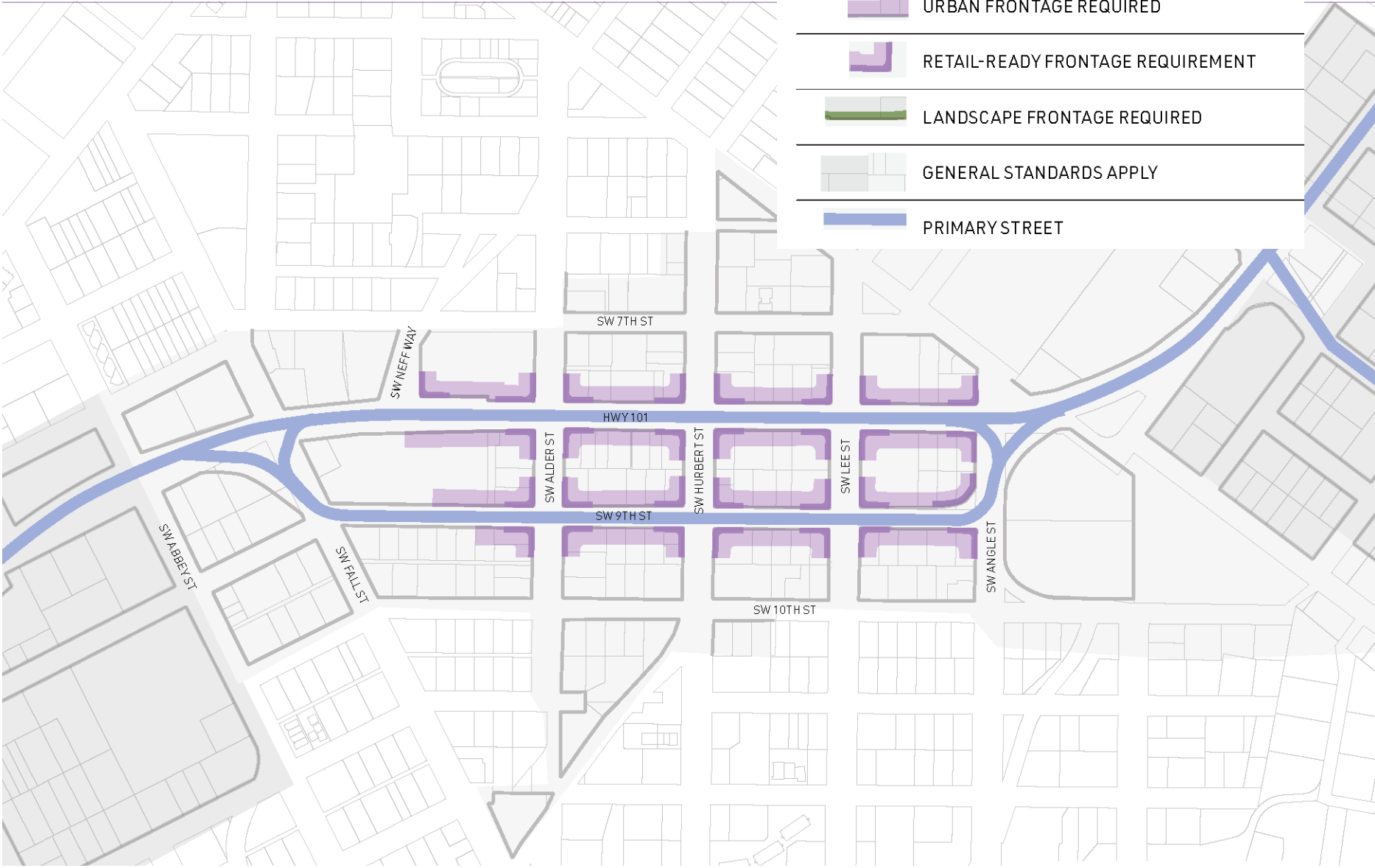
LEGEND

-  URBAN FRONTAGE REQUIRED
-  RETAIL-READY FRONTAGE REQUIREMENT
-  LANDSCAPE FRONTAGE REQUIRED
-  GENERAL STANDARDS APPLY
-  PRIMARY STREET

TRADITIONAL DOWNTOWN ZONE

LEGEND

- URBAN FRONTAGE REQUIRED
- RETAIL-READY FRONTAGE REQUIREMENT
- LANDSCAPE FRONTAGE REQUIRED
- GENERAL STANDARDS APPLY
- PRIMARY STREET



APPLICABLE ODOT URBAN BLUEPRINT STANDARDS

Public Realm Street Standards (Street Cross Sections) – page 22

| | CCARP CORE - TRADITIONAL DOWNTOWN | | CCARP GATEWAY - URBAN MIX | |
|--|-----------------------------------|-------------------|---------------------------|---------------|
| PUBLIC REALM STANDARDS | HWY 101 | SW 9TH ST | HWY 20 / SW OLIVE ST | |
| Pedestrian realm (feet) | 14 or greater | 12 or greater | 14 or greater (ideal) | 12 or greater |
| Frontage / Pedestrian / Buffer / Curb-gutter (feet) | <u>2 / 8 / 5 / .5</u> | 0 / 8 / 4 / 0 | 0 / <u>8</u> / 4 / 0 | 0 / 8 / 4 |
| Transition realm (feet) | 16 (one side) / 8 | 8 (one side only) | 0 | none |
| Bike / Buffer / Parking (feet) | <u>5 / 3 / 8 / 0</u> | 5 / 3 / 0 | 0 / 0 / 0 | |
| Travelway realm (feet) | 22 | 22 | 36 | 36 |
| Travel lane / Left turn lane and/or Raised Median (feet) | <u>11 / 11</u> | <u>11 / 11</u> | 12 / 12 / 12 | 12 / 12 / 12 |
| Totals (feet) | 74 | 54 | 60 | 60 |

Key | [Critical dimensions](#)

STREET CROSS SECTIONS

Hwy 101 – Southbound couplet

Traditional Downtown Main Street

Supports residential and retail uses at the ground level.

Aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



Cross Section illustrates Urban Frontage – Shopfront (left) and Urban Frontage – Residential Porch-Stoop-Terrace, (right), see page 40.

SW 9th St. – Northbound couplet

Traditional Downtown Secondary Street

Generally supports residential and some retail uses at the ground level.

Generally aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



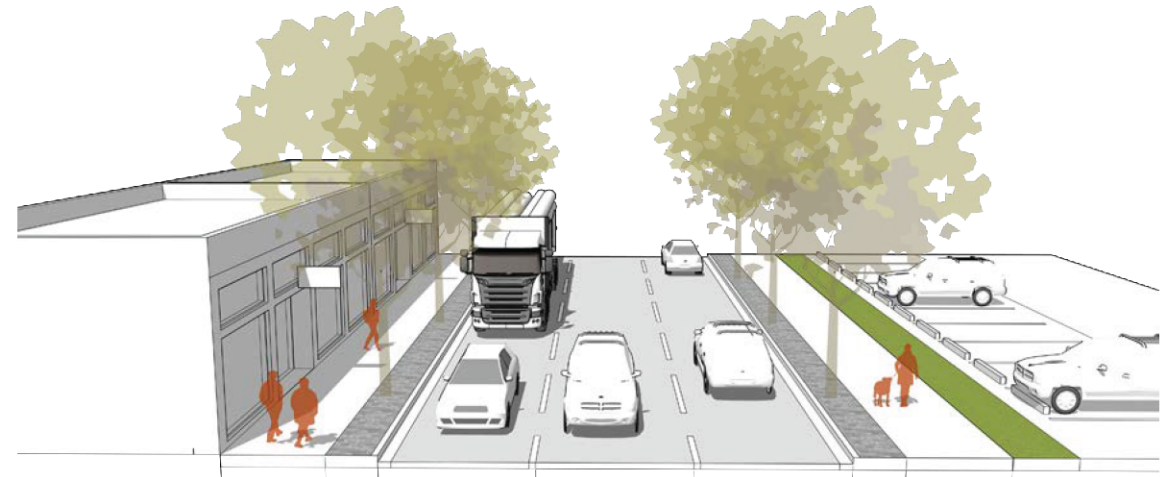
STREET CROSS SECTIONS

Hwy 20 – Olive St.

Improved pedestrian realm supports residential and some retail uses at the ground level.

Street environment and function will be improved with the consolidation of driveways on Olive Street; business access will be relocated to a single, central location on each block face or lot, and on side streets.

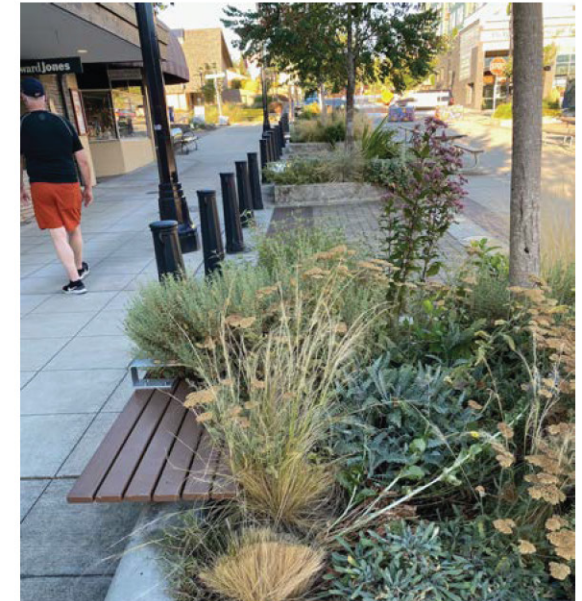
Generally aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



STREET CROSS SECTIONS

SW Hurbert St.

Parklet Street



STREET CROSS SECTIONS

SW Alder and SW Lee Streets

Festival Street or Plaza Street



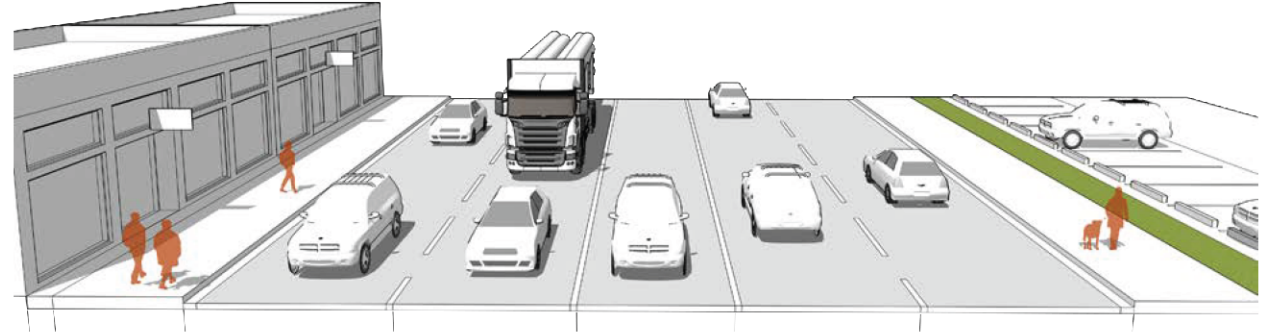
After

STREET CROSS SECTIONS

Hwy 101 –South of the couplet

Does not align with ODOT Blueprint for Urban Design Urban Mix recommendations.

Unimproved pedestrian realm and lack of on-street parking does not support retail, however, public realm and development requirements (e.g., FBC frontage types, General Standards) represent significant enhancements to Hwy 101 S.



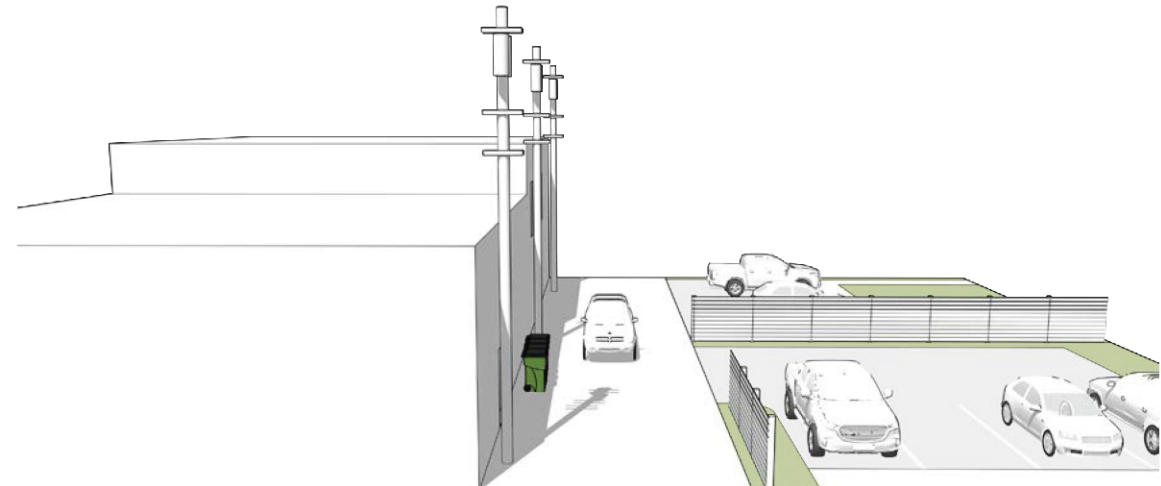
Cross Section showing existing conditions within existing right of way with Landscape Frontage Type applied (right) and General Design Standards applied (left).

Cross section enhancements and FBC frontage zone requirements represent opportunities for Hwy 101 Gateway Zone and hospital-related redevelopment enhancements.

STREET CROSS SECTIONS

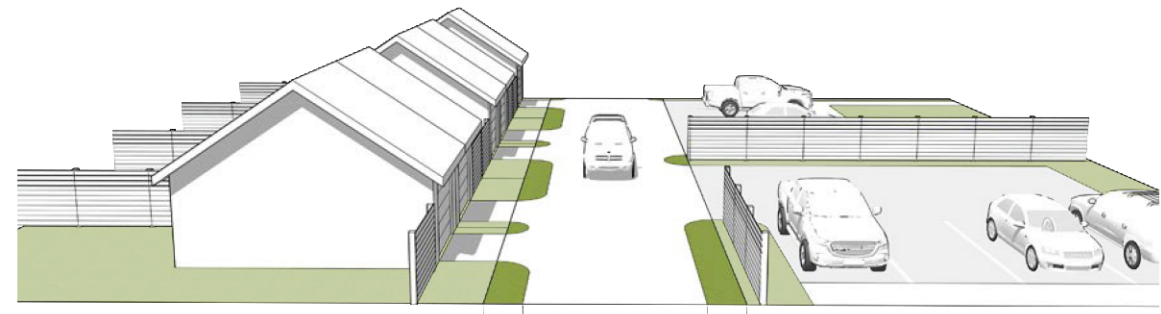
Commercial Alley

Private or public



Residential Alley

Private or public



Development and Design Standards – page 30

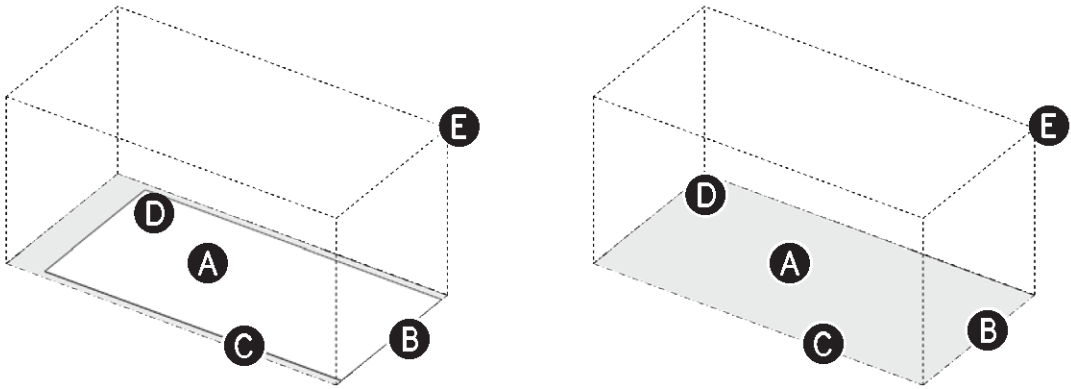
A suite of clear and objective standards:

- Development standards – lot size, width, setbacks, lot coverage, building height, and maximum density (land area required per dwelling unit);
- Clear and objective design standards for street-facing building façades or façades facing other public spaces (e.g., cottage cluster common courtyard), and
- Frontage Types - extra design standards for the first twenty feet of buildings within the Traditional Downtown (as defined by the Regulating Plan); includes options and choices for compliance.

SUMMARY OF BASE ZONE DEVELOPMENT STANDARDS

The base zone development standards will apply to new development along with the new form based standards.

Building envelope standards are summarized in the table.



| 14.13.010 DENSITY LIMITATIONS | C-1 AND C-3 | P-1 |
|---|-------------|-----------|
| A Minimum lot size (square feet) | 5,000 | 5,000 |
| B Minimum lot width (feet) | 25 | 25 |
| C Setbacks – Front and 2nd Front / Side / Rear (feet) | 0 / 0 / 0 | 0 / 0 / 0 |
| D Maximum lot coverage (%) | 85-90 | 100 |
| E Maximum building height (feet) | 50 | 50 |
| Density – Land Area Required per Unit (sq. feet)* | 1,250 * | 1,250 * |

Roof slopes shall be between 5:12 and 12:12 except for shed roof additions or shed dormers may be 3:12.

14.30.030 – CCDRD General Standards

A. Purpose

The design standards for the City Center Design Review District require a minimum level of design on every building. These standards are intended to promote district character, attention to detail, human-scale and pedestrian-oriented design, while affording flexibility to use a variety of architectural styles.

City Center development is intended to implement the CCARP community vision and must address the following design objectives:

- Articulation – All street-facing buildings must incorporate design elements that break up façades into smaller planes.
- Eyes on the street – A certain percentage of the area of each street-facing façade must be windows.
- Main entrance – On street-facing façades, at least 1 main entrance must meet standards for location, orientation, and visibility.
- Detailed design – All street-facing buildings must include several features selected from a menu.

B. Applicability

| Design Standard | Where it applies |
|---------------------------------|---|
| Articulation – 14.30.030(BC)(1) | Applicable to dwellings facing the street when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line. |
| Windows – 14.30(C)(2) | Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path. |
| Main entrance – 14.30(C)(3) | Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path. |
| Detailed design – 14.30(C)(4) | Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path. |

| Design Standard | Where it applies |
|--------------------------------------|---|
| Transitional space – 14.30(C)(5) | Applicable to ground floor dwellings with access from the street or shared open space (e.g. a common courtyard), and access entry door is: (a) Within 10 feet of the street-facing property line, or (b) Within the front yard setback, or (c) Within 10 feet of a shared open space common tract or easement. |
| Pedestrian circulation – 14.30(C)(6) | Applicable to the entire site, and Applicable only for new buildings. |
| Off-street parking – 14.30(C)(7) | Applicable to clustered parking where parking spaces are 5 or more. |

The design standards in this subsection apply to the types of development listed below when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line.

1. New dwellings.
2. Expansions of structures in that add area to any street-facing façade. The design standards for such expansions are applicable as follows:
 - a. Expansions that add 75 square feet or less of street-facing façade area are exempt from all design standards.
 - b. Expansions that add more than 75 square feet and less than 200 square feet of street-facing façade area

FRONTAGE TYPES

BUILD-TO LINE / FRONTAGE LINE

FRONT,

EXAMPLES

SHOPFRONT



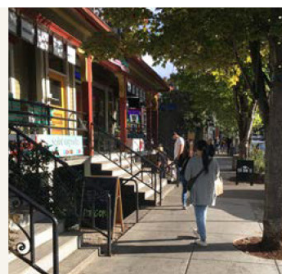
Shopfront frontage is characterized by a façade that is aligned close to the frontage line with the building entrance at sidewalk grade.

FORECOURT



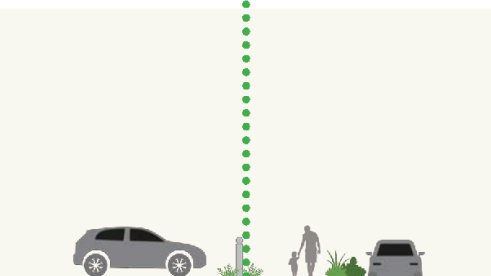
Forecourt frontage is characterized by portions of the façade close to the frontage line alternated by portions of the façade which are set back.

PORCH-STOOP-TERRACE



Porch-stoop-terrace frontage is characterized by a façade that is aligned close to the frontage line with the first story set back from the frontage line with a porch, a stoop, or a terrace.

LANDSCAPE FRONTAGE 1, 2, AND 3



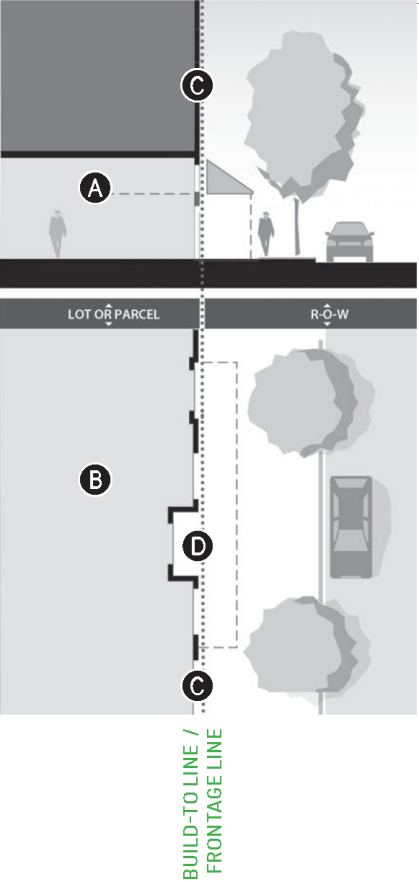
Landscape frontage 2 is characterized by a low wall, fence, and/or vertical landscaping such as a hedge that is aligned with the frontage line.

URBAN Frontages

Urban Frontage – Shopfront

FINAL DRAFT

FRONTAGE TYPES



URBAN FRONTAGE – SHOPFRONT

The Shopfront Frontage is characterized by a façade that is built up to the Build To Line or Frontage Line which is typically the front lot line.

The building entrance shall be at sidewalk grade. The Shopfront Frontage shall have substantial glazing on the ground floor. Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

- | | | |
|---|---|----|
| A | “Retail ready” minimum ground floor height (feet) | 18 |
| B | “Retail ready” minimum ground floor depth (feet) | 20 |
| C | Building Setback from Build-to Line (feet) | 0 |

Retail ready ground floor shall be constructed to meet commercial building standards.

- | | |
|---|--|
| D | Building entries shall provide a canopy or awning and/or be recessed behind the front building façade. |
|---|--|

Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.



Retail-ready shopfronts



Bay window and awning encroachments into the Right of Way

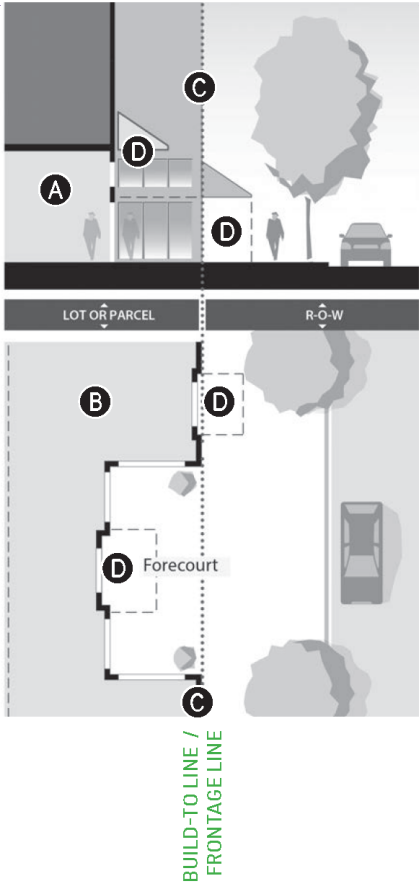


Recessed entry

Urban Frontage – Forecourt

FINAL DRAFT

FRONTAGE TYPES



URBAN FRONTAGE – FORECOURT

Forecourt Frontage is created by recessing a portion of the façade behind the Build To Line or Frontage Line. Forecourt Frontage shall be used in conjunction with the Shopfront Frontage. Forecourt Frontage is appropriate for commercial or residential uses, outdoor seating, and hardscaped plaza and/or landscaped gardens.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

A “Retail ready” minimum ground floor height (feet) 18

B “Retail ready” minimum ground floor depth (feet) 20

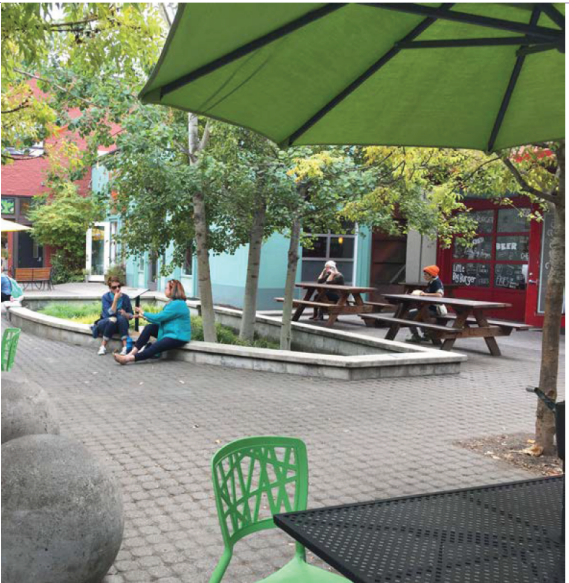
C Building Setback from Build-to Line for Shopfront Frontage (feet) 0

Retail ready ground floor shall be constructed to meet commercial building standards.

C Each dwelling unit which is accessed from the forecourt or the sidewalk shall comply with either the vertical or horizontal standard.

Vertical separation –
Distance from ground:
Minimum 18 inches;
maximum 3 feet;
Horizontal separation –
Distance from Build To
Line: Minimum 3 feet;
maximum 15 feet

D Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.



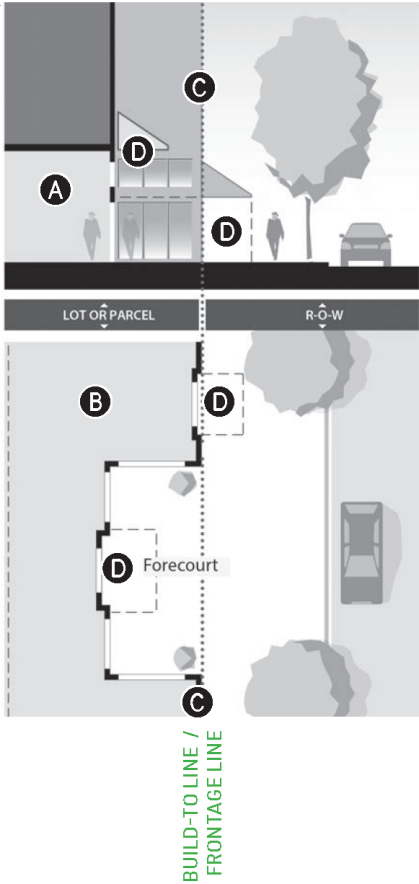
Forecourts shared by multiple retail establishments and restaurants



Urban Frontage – Forecourt

FINAL DRAFT

FRONTAGE TYPES



URBAN FRONTAGE – FORECOURT

Forecourt Frontage is created by recessing a portion of the façade behind the Build To Line or Frontage Line. Forecourt Frontage shall be used in conjunction with the Shopfront Frontage. Forecourt Frontage is appropriate for commercial or residential uses, outdoor seating, and hardscaped plaza and/or landscaped gardens.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

A “Retail ready” minimum ground floor height (feet) 18

B “Retail ready” minimum ground floor depth (feet) 20

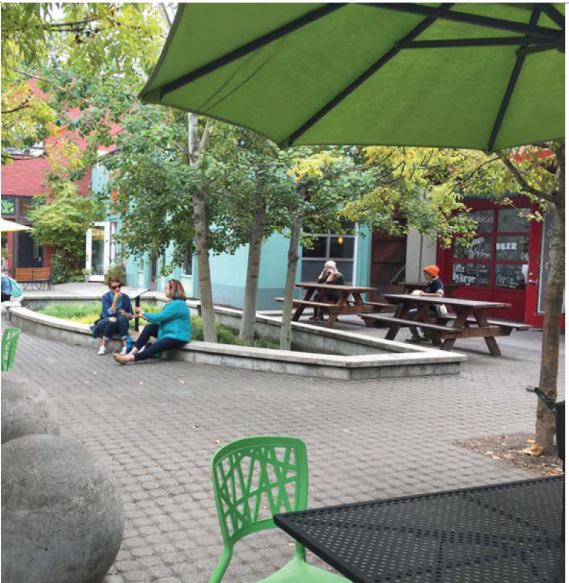
C Building Setback from Build-to Line for Shopfront Frontage (feet) 0

Retail ready ground floor shall be constructed to meet commercial building standards.

C Each dwelling unit which is accessed from the forecourt or the sidewalk shall comply with either the vertical or horizontal standard.

Vertical separation –
Distance from ground:
Minimum 18 inches;
maximum 3 feet;
Horizontal separation –
Distance from Build To
Line: Minimum 3 feet;
maximum 15 feet

D Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.

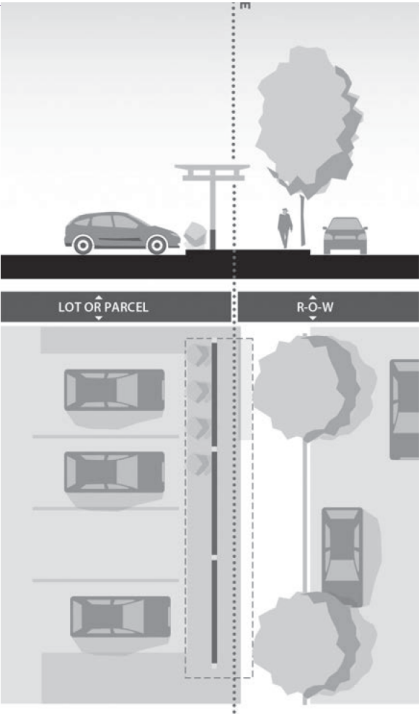


Forecourts shared by multiple retail establishments and restaurants



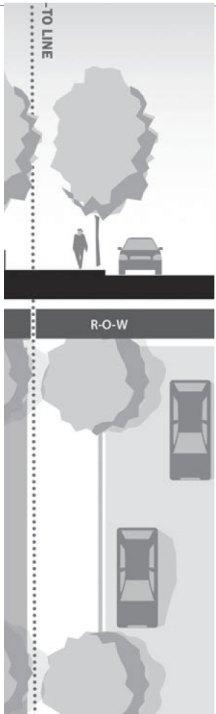
Landscape Frontages

FRONTAGE TYPES



LANDSCAPE FRONTAGE 1 - LOW WALL AND TRELLIS

| | |
|---|--|
| A Vertical Landscaping or Structure Setback from Build-to Line, maximum feet | 5 |
| B Height of Structure or Planting, feet | The underside of the Trellis portion of a Low Wall and Trellis shall be a minimum of 8 feet above grade and a maximum of 14 feet above grade. The Low Wall portion of a Low Wall and Trellis shall be a minimum of 1.5 feet and a maximum of 3 feet and have a minimum depth of 1.5 feet. |
| C Materials | The Trellis shall be heavy timber or steel (or a similar material) and shall consist of open structure with no decking or awning material. The Trellis shall have masonry, heavy timber, or steel (or similar metal) supporting columns spaced no more than 30 feet on center. The Low Wall shall be wood, masonry, and/or concrete. |
| D Openings | Openings in the Low Wall and Trellis are allowed for pedestrian pathways, sidewalks, plazas, and driveways. |
| E Surface Parking Setback | Surface Parking shall be set back a minimum of 3 feet from the Low Wall and Trellis. |
| F Ground Cover and Planting | The area between the Build-to Line and the Trellis shall be hardscaped with either masonry pavers or stamped concrete. The setback between the Low Wall and surface parking shall be planted with low shrubs, groundcover, and climbing plants. |

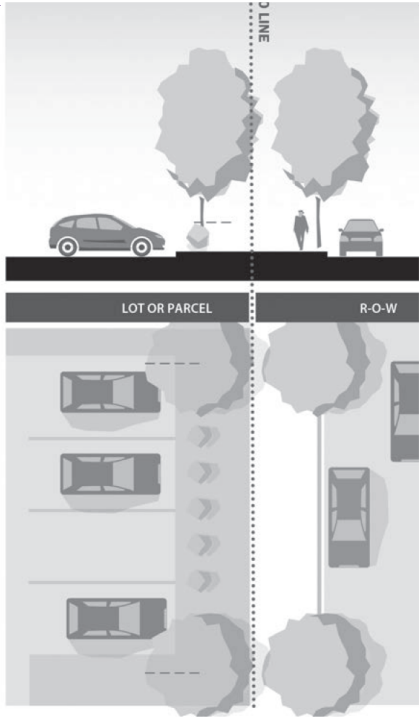


LANDSCAPE FRONTAGE 2 - URBAN FENCE OR WALL

| | |
|---|---|
| A Vertical Landscaping or Structure Setback from Build-to Line, maximum feet | 5 |
| B Height of Structure or Planting, feet | The fence or the wall shall be at least 2 feet high and no more than 3 feet high. |
| C Materials | Walls shall be wood masonry, and/or concrete; fences shall be made of wrought iron, steel, or a similar material (but not chain-link) and must be dark in color. Fences may be no more than 50% sight obscuring. |
| D Openings | Openings in the Urban Fence or Wall are allowed for pedestrian pathways, sidewalks, plazas, and driveways. |
| E Surface Parking Setback | The surface parking area shall be set back, at a minimum, an additional 5 feet to provide room for required landscaping and stormwater infiltration and/or retention. |
| F Ground Cover and Planting | In addition to the required fence or wall, trees and shrubs shall be provided. One large tree is required every 30 linear feet minimum. The shrubs shall be at least as high as the wall or fence, and shall be no more than 6 feet high. |

Landscape Frontages

FRONTAGE TYPES



LANDSCAPED FRONTAGE 3 - LOW HEDGE

| | |
|---|---|
| A Vertical Landscaping or Structure Setback from Build-to Line, minimum feet | 5 |
| B Height of Structure or Planting, feet | The shrubs shall be a minimum of 3 feet high. If a low wall is provided in place of shrubs it shall be a minimum of 3 feet high. |
| C Materials | The surface parking area shall be screened with a continuous row of hedges or shrubs immediately adjacent to the parking area, except where there is a driveway. Shrubs must be mostly opaque year round. A low wall may be substituted for the shrubs but the trees and groundcover plants are still required. |
| D Openings | Openings in the Landscape Frontage are allowed for pedestrian pathways, sidewalks, plazas, and driveways. |
| E Surface Parking Setback | 10 feet minimum |
| F Ground Cover and Planting | In addition to the required shrubs, one large tree is required every 30 linear feet. The shrubs/hedge shall be interrupted with a gap of up to 2 feet wide in order to accommodate trees. |

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**Newport City Center Design Review District – Amendments to the
Newport Municipal Code**

Contents

- Summary of Amendments Table
- Track change amendments

Summary of Amendments

Title XIV – Zoning (Chapters 14.03 through 14.30)

| Chapter | Subsection | Description of Amendments or Discussion |
|--|------------|---|
| 14.03.050 – Residential Uses | | Footnote to land use table listing for R-2, to permit Cottage Clusters, Multifamily uses, and Rooming and Boarding Houses in the CCDRD. |
| 14.03.070 – Commercial and Industrial Uses | | Footnotes to the land use table listings for C-1 and C-3: <ul style="list-style-type: none">· To permit residential uses in the CCDRD on C-1- and C-3-zoned land, and· To permit residential uses on the ground floor of buildings in the CCDRD. |
| 14.03.100 – Public Uses | | Footnote to land use table listing for P-1, to permit residential uses in the CCDRD on P-1-zoned land. |

| Chapter | Subsection | Description of Amendments or Discussion |
|--|------------------------------|--|
| 14.06.060 – Recreational Vehicle Parks | | Added language to make limitations on Recreational Vehicle Parks the same for CCDRD as they currently are for Historic Nye Beach Design Review District. |
| 14.09.050 – Mobile Food Units | | Added language to make limitations on Mobile Food Units the same for City Center parking district as they currently are for Historic Nye Beach parking district: only permitted if it is a Mobile Food Unit Pod. |
| 14.13.010 – Density Limitation | NMC 14.13.020 - Table "A" | <p>Table "A" manages the lot size, density, form, shape, and size of buildings by land use district. Exceptions for special areas such as Nye Beach Design Review District, are referenced in the footnotes.</p> <p>Similar to the way that special zoning and design standards that apply to Nye Beach Design Review District is established in footnote #4, a new footnote is added to reference the special zoning standards that apply to the City Center Design Review District (CCDRD):</p> <p><i>Special Zoning Standards apply to C-1, C-3, P-1, R-2, and R-4 zoned property within the City Center Design Review District as outlined in NMC 14.30.200.</i></p> <p>Where the standards in NMC 14.30.200 differ from those in Table A, the standards in 14.30.200 will govern development form in the CCDRD.</p> |

List of amendments:

- 14.03.050 – Residential Uses. Footnote to land use table listing for R-2, to permit Cottage Clusters, Multifamily uses, and Rooming and Boarding Houses in the CCDRD.
- 14.03.070 – Commercial and Industrial Uses. Footnotes to the land use table listings for C-1 and C-3, to permit residential uses in the CCDRD on C-1- and C-3-zoned land, and to permit residential uses on the ground floor of buildings in the CCDRD.
- 14.03.100 – Public Uses. Footnote to land use table listing for P-1, to permit residential uses in the CCDRD on P-1-zoned land.
- 14.15.020 – Residential Uses in Nonresidential Zoning Districts. Multiple text changes to ensure that residential uses are permitted outright use, subject to the CCDRD FBC provisions.
- 14.30.020 –(B) – Design Review Districts: Overlay Zones Established. New text establishing the CCDRD and describing the boundaries.
- 14.30.030 – Adoption of Design Review: Guidelines and Standards. References the “City Center Design Review District Form Based Code ” and states that the Regulating Plan, Public Realm Standards, and Development and Design Standards, contained therein shall be the standards applicable to the City Center Design Review District.

List of amendments, continued:

- 14.30.060 – Approval Authority. Adds the City Center Design Review Form Based Code to those documents subject to .060, which establishes that the approval or denial of a Design Review application is a ministerial action performed concurrent with review of a building permit by the Community Development.
- 14.30.100 – Special Zoning Standards in Design Review Districts. Adds the CCDRD to areas subject to special zoning standards and references the CCDRD Form Based Code. Prohibits drive through windows in the area defined as Traditional Downtown Zone within the CCDRD FBC.

DISCUSSION QUESTIONS

“Retail Ready” Frontage Standard. The Regulating Plan (Section 3) currently shows “Retail-Ready” Frontage Type required on Hwy 101 and SW 9th, at all corners within the Traditional Downtown Zone. The “retail-ready” Frontage Types are detailed on pages 40-43. Two alternative options for discussion are:

1. Only on side streets which have been identified as retail-focused streets— SW Hurbert, SW Alder, and SW Lee Streets—for the full length of the side street and wrapping the corners (e.g., 25 linear feet from each corner, or to the nearest property line, whichever is less).
2. On Hwy 101 only, (not on SW 9th) where 101 intersects with SW Hurbert, SW Alder, and SW Lee Streets and wrapping the corners (e.g., 25 linear feet from each corner or to nearest property line, whichever is less).

DISCUSSION QUESTIONS

Encroachments. The CCDRD proposed provisions say: “Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.” See Urban Shopfront Frontage Type, page 40, for an example.

Encroachments of the kind mentioned are desirable for urban vitality and interest, especially in the Traditional Downtown Zone. The city permits encroachments on a very limited basis.

Discuss how the city allows encroachments and how the intent for architectural features (such as bay windows), and weather protection (awnings, marquees), can be allowed within the CCDRD.

DISCUSSION QUESTIONS

Architectural Features.

The CCDRD General Design Standards contain menus for architectural features but they are generic (see “Detailed Design,” page 34, for an example). The “Newport Design Review Glossary and Illustrations,” used for Historic Nye Beach Design Review District contains a more comprehensive listing along with illustrations. Besides being more comprehensive, these have the benefit of being in place for some time and are familiar to the community. While they are historic features many of them are timeless architectural features that can be rendered in a variety of architectural styles, from traditional to modern.

Options for discussion:

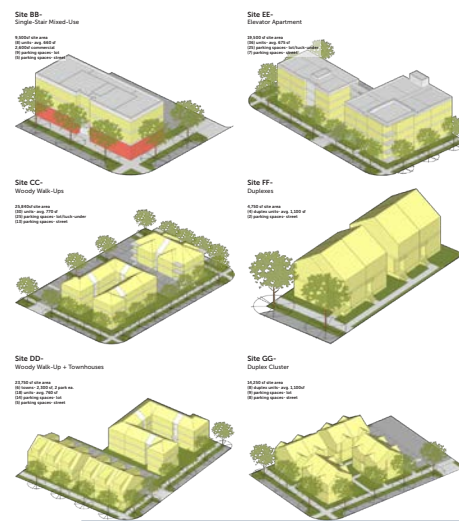
1. Keep the current generic listing (see “Detailed Design,” page 34).
2. Replicate or adapt certain features from the Newport Design Review Glossary and Illustrations, such as balcony, bay window, and/or turret. Combine these with the current generic listing.



Thank you

City Center Design Review District Form Based Code

THE NEWPORT CITY CENTER REVITALIZATION PLAN PROJECT
FINAL DRAFT, JUNE 2025



bruary 2025



u r b s w o r k s

Newport City Center Revitalization Plan Project

City Center Design Review District Form
Based Code

Contents

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| 04 | 01 INTRODUCTION |
| 06 | 02 FORM BASED APPROACH |
| 10 | 03 REGULATING PLAN |
| 16 | 04 PUBLIC REALM |
| 24 | 05 DEVELOPMENT AND DESIGN STANDARDS |
| 38 | 06 REFERENCES |

APPENDIX A | NEWPORT MUNICIPAL CODE (NMC) AMENDMENTS



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This Project is partially funded by a grant from the Transportation and Growth Management (TGM) Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Fixing America's Surface Transportation Act (FAST Act), local government, and the State of Oregon funds.

The contents of this document do not necessarily reflect views or policies of the State of Oregon.

David Evans and Associates Inc.

ECOnorthwest

This document, the City Center Design Review District Form Based Code, represents the Newport Municipal Code (NCM) amendments necessary to implement the City of Newport City Center Revitalization Plan, and fulfills the TGM grant-funded project tasks related to code amendments.

Discussion

This document will be presented to the Newport Planning Commission at their July 2025 work session. On this and the following pages are issues for work session discussion.

“Retail Ready” Frontage Standard

The Regulating Plan (Section 3) currently shows “Retail-Ready” Frontage Type required on Hwy 101 and SW 9th, at all corners within the Traditional Downtown Zone. The “retail-ready Frontage Types is detailed on pages 40-43. Two alternative options for discussion are:

- » Only on side streets which have been identified as retail-focused streets— SW Hurbert, SW Alder, and SW Lee Streets—for the full length of the side street and wrapping the corners (e.g., 25 linear feet from each corner, or to the nearest property line, whichever is less).
- » On Hwy 101 only, (not on SW 9th) where 101 intersects with SW Hurbert, SW Alder, and SW Lee Streets and wrapping the corners (e.g., 25 linear feet from each corner or to nearest property line, whichever is less).

DISCUSSION, CONTINUED

Discretionary process and review standards

» *What is the discretionary process and review standards for deviating from the clear and objective, fixed, form-based dimensional standards in the form based code?*

The CCDRD fosters housing as an important aspect of the policy changes and implementation of the CCARP Revitalization Plan. Amendments to the base zone land use listings mean that housing is allowed (but not required) on any lot within in the CCDRD.

The CCDRD complies with the state’s middle housing requirements and HB 2001 and associated rules. The rules do not allow housing to be subject to discretionary review, and any standards that a city applies to housing beyond those in the Middle Housing ORS or Model Code must be applied to all housing types.

Given that housing is not subject to discretionary approval, the only development in the CCDRD that could be subject to discretionary review would be non-residential projects.

To comply with the middle housing rules, the CCDRD General Standards and Frontage Types (Section 5) allow a high degree of flexibility and choices through menus or options. No specific architectural style is promoted, and flexibility to use a variety of architectural styles is explicitly stated in the general standards.

This means that, while there are clear and objective, fixed, form-based dimensional standards, there is a high degree of flexibility for non-residential projects as well as housing development.

If an alternative, discretionary approval process for non-residential projects is desired, an option can be provided.

Options are:

- » Develop an alternative, discretionary approval process for non-residential projects similar to that taken in Nye Beach design review district, i.e., using design guidelines, intent statements, and example approaches.
- » Do not provide an alternative process for non-residential projects. Such projects would follow the same clear and objective review process and be subject to the same process as housing (ministerial action performed concurrent with review of a building permit by the Community Development).

DISCUSSION, CONTINUED

Encroachments

The CCDRD proposed provisions say: “Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.” See Urban Shopfront Frontage Type, page 40, for an example.

Encroachments of the kind mentioned are desirable for urban vitality and interest, especially in the Traditional Downtown Zone. The city permits encroachments on a very limited basis.

Discuss how the city allows encroachments and how the intent for architectural features (such as bay windows), and weather protection (awnings, marquees), can be allowed within the CCDRD.

Architectural Features

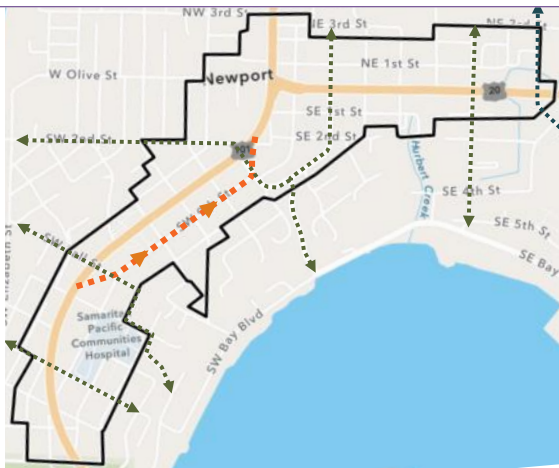
The CCDRD General Design Standards contain menus for architectural features but they are generic (see “Detailed Design,” page 34, for an example). The “Newport Design Review Glossary and Illustrations,” used for Historic Nye Beach Design Review District contains a more comprehensive listing along with illustrations. Besides being more comprehensive, these have the benefit of being in place for some time and are familiar to the community. While they are historic features many of them are timeless architectural features that can be rendered in a variety of architectural styles, from traditional to modern.

Options for discussion:

- » Keep the current generic listing (see “Detailed Design,” page 34).
- » Replicate or adapt certain features from the Newport Design Review Glossary and Illustrations, such as balcony, bay window, and/or turret. Combine these with the current generic listing.

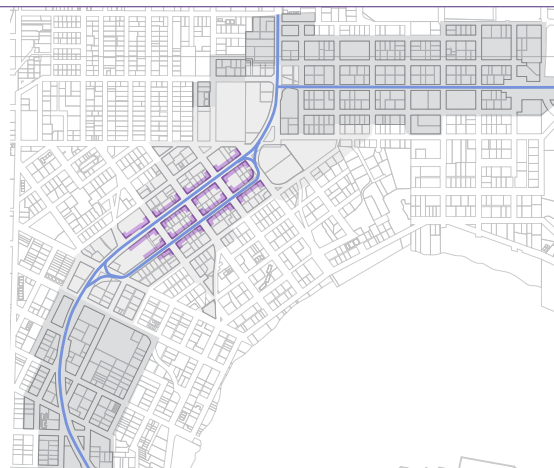
INTRODUCTION





City Center Design Review District (CCDRD)

The City Center Design Review District (CCDRD) is US Highway 101 and adjacent properties between the north end of the Yaquina Bay Bridge and US Highway 20, and US Highway 20 and adjacent properties between US Highway 101 and the eastern city limits.



CCDRD Form Based Code (FBC)

This document translates the vision created during the public involvement process into a series of land use, streetscape, and urban design strategies, using a form based approach.

Form based codes (FBCs) are development regulations used by local government agencies that emphasize the physical character of development and deemphasize the categorization of land uses and activities. They provide greater predictability about the look and feel of development and offer developers a clearer understanding of what the community seeks. In return, FBCs can make it easier for citizens to help create the physical development they want, which will more likely lead to their acceptance of development and street designs in their community.

FORM BASED APPROACH



The new 14.30 City Center Design Review District and amendments to the municipal code and Comprehensive Plan will:

FBC Approach

Promote a “form based” approach that emphasizes building shape, size, and relationship to streets and open spaces and de-emphasizes land use.

Use a “Regulating Plan” to translate the CCARP vision into a plan and map of the project area. The regulating plan will show where different development and design standards apply.

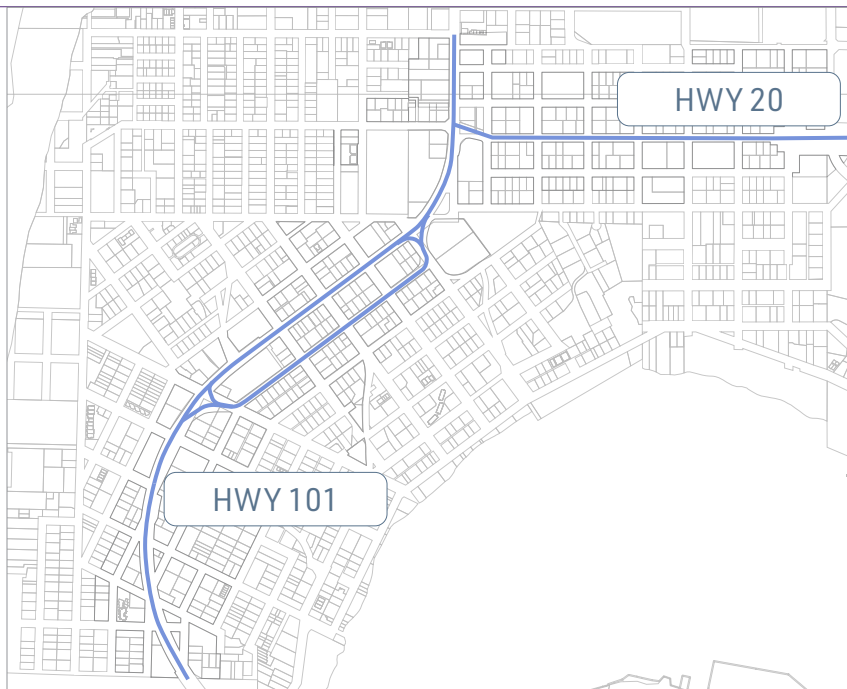
Utilize clear and objective dimensional standards. An example of a dimensional standard is “front yard setback – 20 feet.”

In general, provide as much information in tables as possible to aid clarity of the provisions and use graphics to communicate the intent of the development standards and provisions..

Display development and design standards in tables. The standards in the table will be keyed to the Regulating Plan, Frontage Types, or other diagrams.

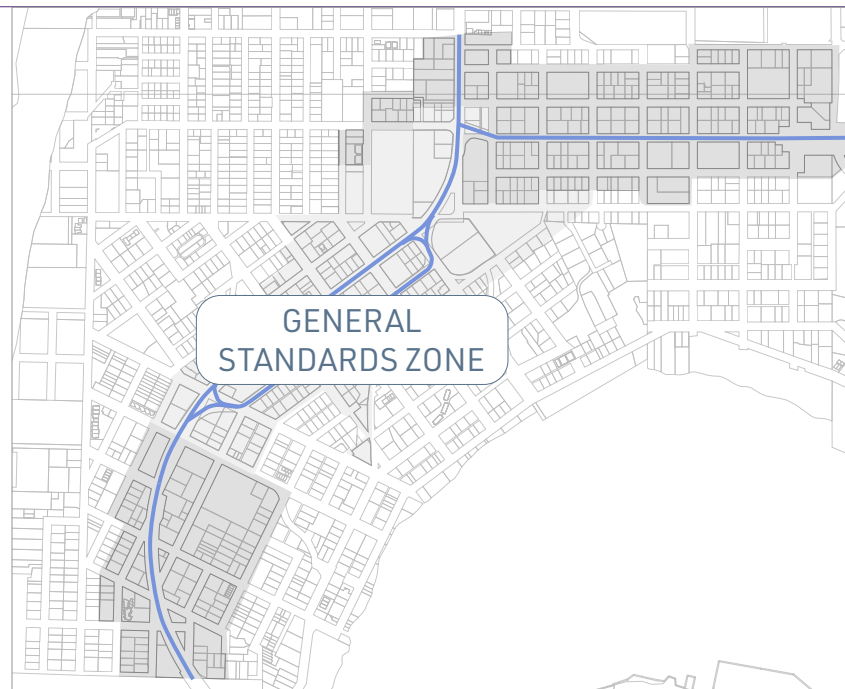
Provide purpose or intent statements and visual examples to help explain the intended outcome.

APPLICABILITY



Public Realm Standards

Street Cross Sections (Section 04)

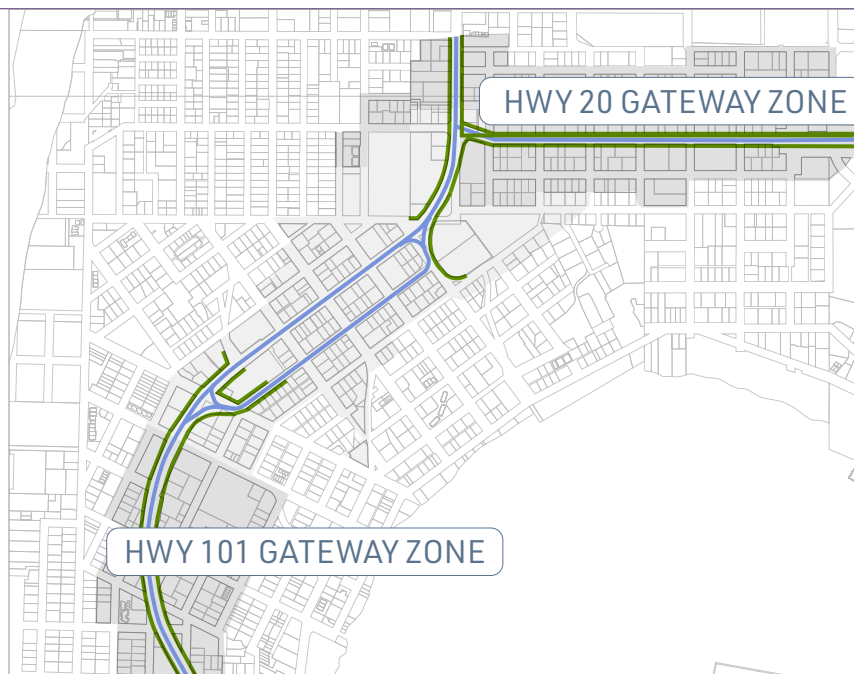


General Standards

Street Cross Sections (Section 04)

General Standards (Section 05)

APPLICABILITY

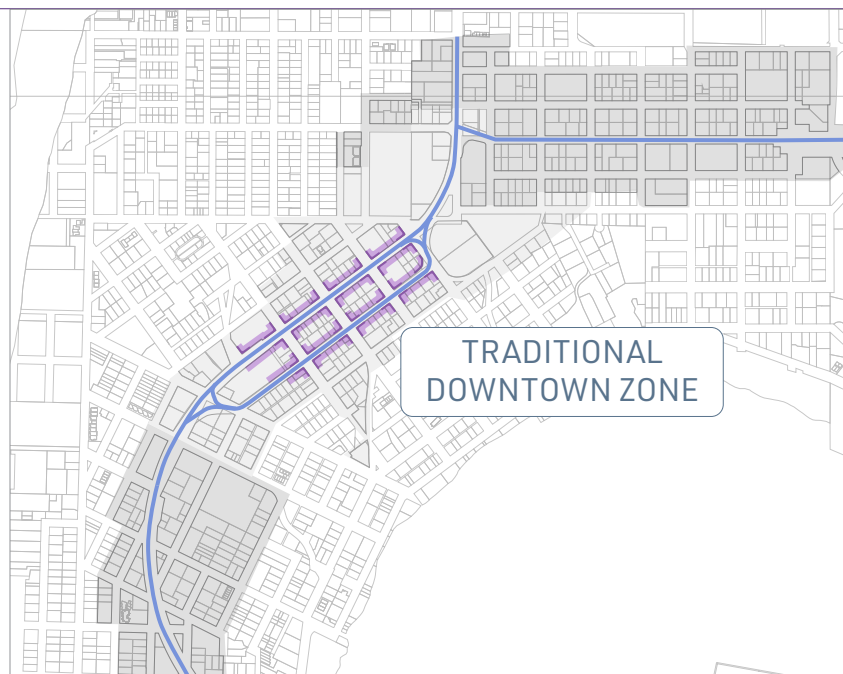


Gateway Frontage Types

Street Cross Sections (Section 04)

General Standards (Section 05)

Gateway Frontage Types (Section 5)



Traditional Downtown Frontage Types

Street Cross Sections (Section 04)

General Standards (Section 05)

Gateway Frontage Types (Section 5)

Traditional Downtown Frontage Types (Section 5)

REGULATING PLAN



Regulating Plan

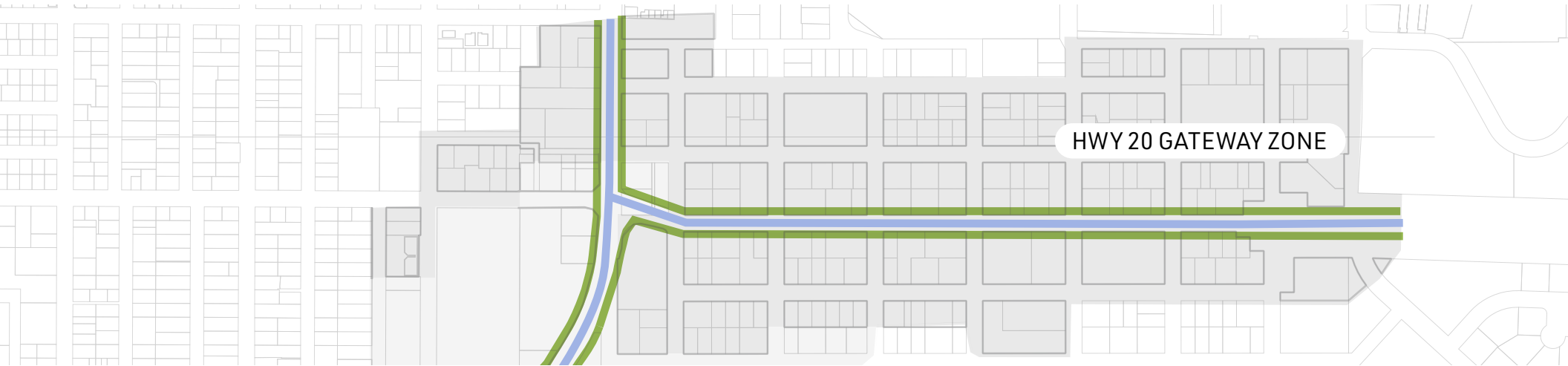
The purpose of the Regulating Plan is to translate the vision created during the public involvement process into an actionable plan and map. Each regulating plan component explains which areas are subject to specific regulations. The regulating plan is a critical tool to inform development of the Newport CCARP Design District and ensure the community vision is realized.

Regulating Plan Components

The regulating plan identifies mandatory components to be implemented through a combination of private development and public capital improvement projects. It provides flexibility for the Newport CCARP Design District to support a wide range of land uses and development types while ensuring strong connectivity, a form and character that aligns with the surrounding context, and effective placemaking and economic development to complement public investments on Hwy 101, SW 9th Street, and Hwy 20.

The Regulating Plan identifies requirements that are geographically located and apply regardless of the proposed development approach. This plan is supplemented by development standards that vary based on proposed development and land uses.

HWY 20 GATEWAY ZONE



HWY 20 GATEWAY ZONE

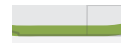
LEGEND



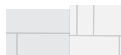
URBAN FRONTAGE REQUIRED



RETAIL-READY FRONTAGE REQUIREMENT



LANDSCAPE FRONTAGE REQUIRED



GENERAL STANDARDS APPLY



PRIMARY STREET

HWY 101 GATEWAY ZONE



LEGEND



URBAN FRONTAGE REQUIRED



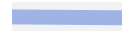
RETAIL-READY FRONTAGE REQUIREMENT



LANDSCAPE FRONTAGE REQUIRED

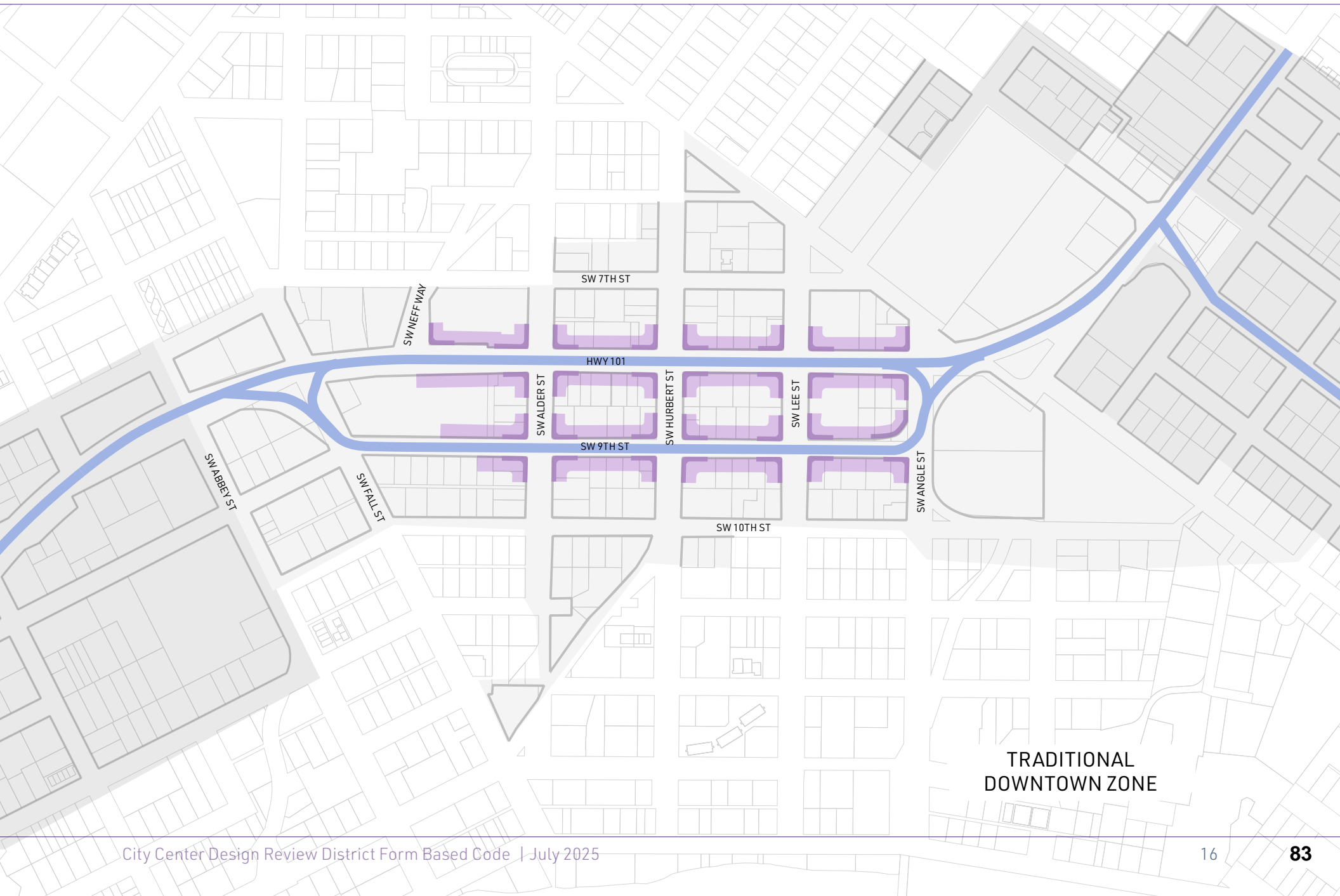


GENERAL STANDARDS APPLY



PRIMARY STREET

TRADITIONAL DOWNTOWN ZONE



TRADITIONAL DOWNTOWN ZONE

LEGEND



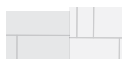
URBAN FRONTAGE REQUIRED



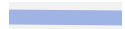
RETAIL-READY FRONTAGE REQUIREMENT



LANDSCAPE FRONTAGE REQUIRED



GENERAL STANDARDS APPLY



PRIMARY STREET



04

PUBLIC REALM

APPLICABLE ODOT URBAN BLUEPRINT STANDARDS

| | CCARP CORE - TRADITIONAL DOWNTOWN | | CCARP GATEWAY - URBAN MIX | |
|--|-----------------------------------|-------------------|---------------------------|---------------|
| PUBLIC REALM STANDARDS | HWY 101 | SW 9TH ST | HWY 20 / SW OLIVE ST | |
| Pedestrian realm (feet) | 14 or greater | 12 or greater | 14 or greater (ideal) | 12 or greater |
| Frontage / Pedestrian / Buffer / Curb-gutter (feet) | <u>2 / 8 / 5 / .5</u> | 0 / 8 / 4 / 0 | 0 / <u>8 / 4</u> / 0 | 0 / 8 / 4 |
| Transition realm (feet) | 16 (one side) / 8 | 8 (one side only) | 0 | none |
| Bike / Buffer / Parking (feet) | <u>5 / 3 / 8</u> / 0 | 5 / 3 / 0 | 0 / 0 / 0 | |
| Travelway realm (feet) | 22 | 22 | 36 | 36 |
| Travel lane / Left turn lane and/or Raised Median (feet) | <u>11 / 11</u> | <u>11 / 11</u> | 12 / 12 / 12 | 12 / 12 / 12 |
| Totals (feet) | 74 | 54 | 60 | 60 |

Key | Critical dimensions

STREET CROSS SECTIONS

Hwy 101 – Southbound couplet

Traditional Downtown Main Street

Supports residential and retail uses at the ground level.

Aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



Cross Section illustrates Urban Frontage – Shopfront (left) and Urban Frontage – Residential Porch-Stoop-Terrace, (right), see page 40.

SW 9th St. – Northbound couplet

Traditional Downtown Secondary Street

Generally supports residential and some retail uses at the ground level.

Generally aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



Cross Section illustrates Urban Frontage – Residential Porch-Stoop-Terrace, vertical separation standard (left); horizontal separation standard (right), see page 40.

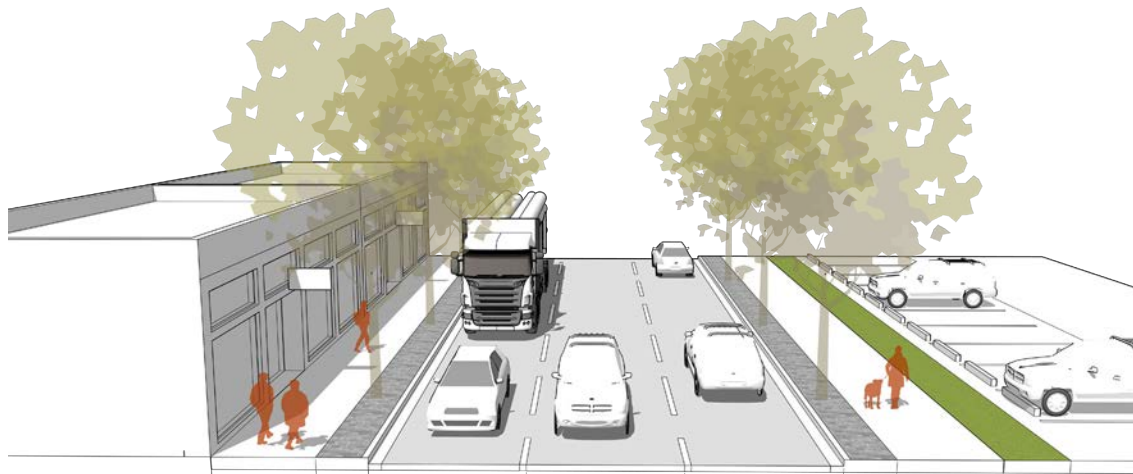
STREET CROSS SECTIONS

Hwy 20 –Olive St.

Improved pedestrian realm supports residential and some retail uses at the ground level.

Street environment and function will be improved with the consolidation of driveways on Olive Street; business access will be relocated to a single, central location on each block face or lot, and on side streets.

Generally aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



STREET CROSS SECTIONS

SW Hurbert St.

Parklet Street



STREET CROSS SECTIONS

SW Alder and SW Lee Streets

Festival Street or Plaza Street



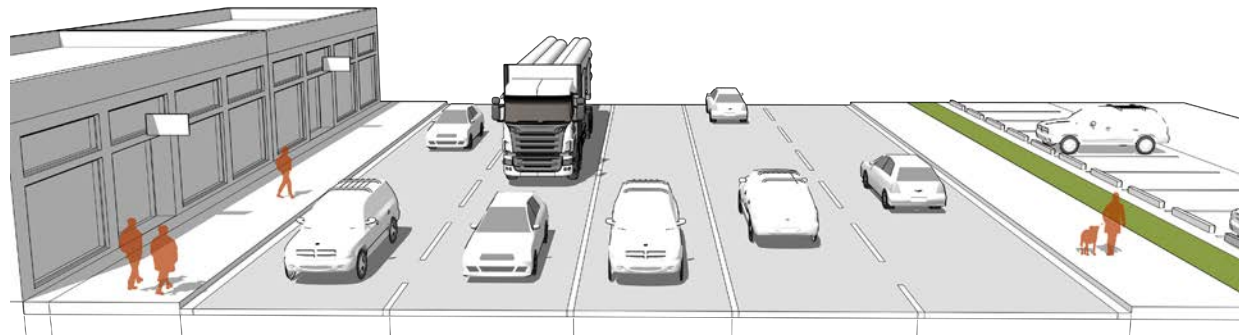
After

STREET CROSS SECTIONS

Hwy 101 –South of the couplet

Does not align with ODOT Blueprint for Urban Design Urban Mix recommendations.

Unimproved pedestrian realm and lack of on-street parking does not support retail, however, public realm and development requirements (e.g., FBC frontage types, General Standards) represent significant enhancements to Hwy 101 S.



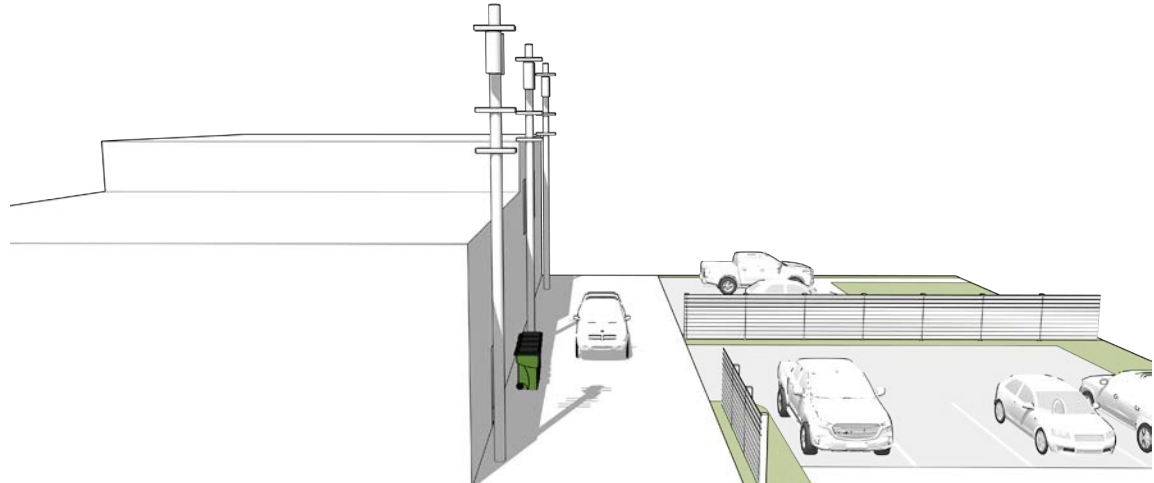
Cross Section showing existing conditions within existing right of way with Landscape Frontage Type applied (right) and General Design Standards applied (left).

Cross section enhancements and FBC frontage zone requirements represent opportunities for Hwy 101 Gateway Zone and hospital-related redevelopment enhancements.

STREET CROSS SECTIONS

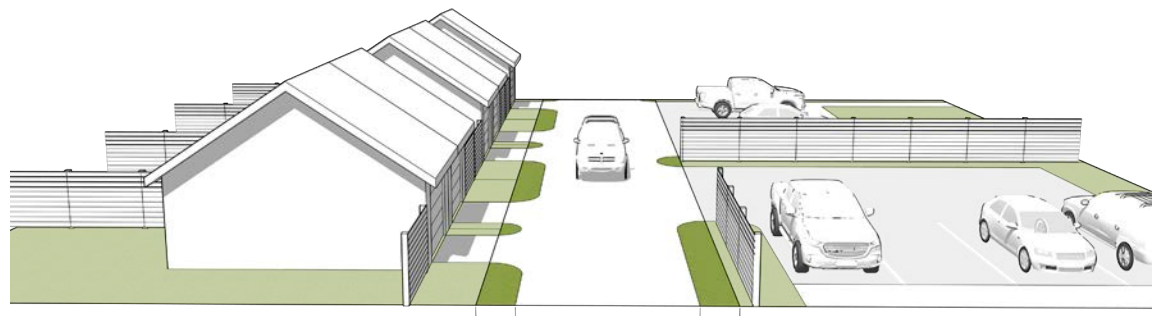
Commercial Alley

Private or public



Residential Alley

Private or public



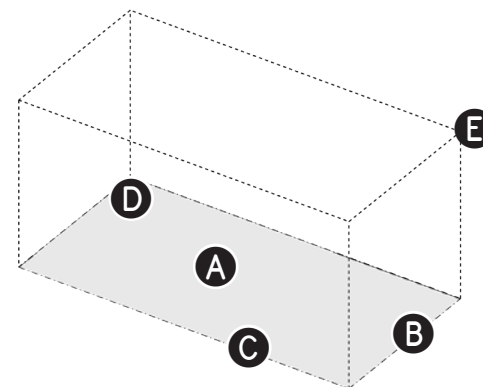
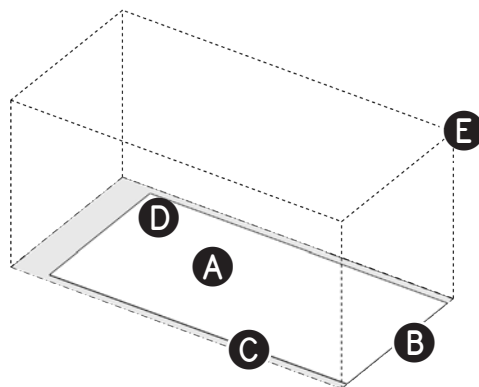
DEVELOPMENT AND DESIGN STANDARDS



SUMMARY OF BASE ZONE DEVELOPMENT STANDARDS

The base zone development standards will apply to new development along with the new form based standards.

Building envelope standards are summarized in the table.



| 14.13.010 DENSITY LIMITATIONS | C-1 AND C-3 | P-1 |
|--|-------------|-----------|
| A Minimum lot size (square feet) | 5,000 | 5,000 |
| B Minimum lot width (feet) | 25 | 25 |
| C Setbacks – Front and 2nd Front / Side / Rear (feet) | 0 / 0 / 0 | 0 / 0 / 0 |
| D Maximum lot coverage (%) | 85-90 | 100 |
| E Maximum building height (feet) | 50 | 50 |
| Density – Land Area Required per Unit (sq. feet)* | 1,250 * | 1,250 * |

Roof slopes shall be between 5:12 and 12:12 except for shed roof additions or shed dormers may be 3:12.

14.30.030 – CCDRD General Standards

A. Purpose

The design standards for the City Center Design Review District require a minimum level of design on every building. These standards are intended to promote district character, attention to detail, human-scale and pedestrian-oriented design, while affording flexibility to use a variety of architectural styles.

City Center development is intended to implement the CCARP community vision and must address the following design objectives:

- Articulation – All street-facing buildings must incorporate design elements that break up façades into smaller planes.
- Eyes on the street – A certain percentage of the area of each street-facing façade must be windows.
- Main entrance – On street-facing façades, at least 1 main entrance must meet standards for location, orientation, and visibility.
- Detailed design – All street-facing buildings must include several features selected from a menu.

B. Applicability

| Design Standard | Where it applies |
|---------------------------------|---|
| Articulation – 14.30.030(BC)(1) | Applicable to dwellings facing the street when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line. |
| Windows – 14.30(C)(2) | Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path. |
| Main entrance – 14.30(C)(3) | Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path. |
| Detailed design – 14.30(C)(4) | Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path. |

| Design Standard | Where it applies |
|--------------------------------------|---|
| Transitional space – 14.30(C)(5) | Applicable to ground floor dwellings with access from the street or shared open space (e.g. a common courtyard), and access entry door is: (a) Within 10 feet of the street-facing property line, or (b) Within the front yard setback, or (c) Within 10 feet of a shared open space common tract or easement. |
| Pedestrian circulation – 14.30(C)(6) | Applicable to the entire site, and Applicable only for new buildings. |
| Off-street parking – 14.30(C)(7) | Applicable to clustered parking where parking spaces are 5 or more. |

The design standards in this subsection apply to the types of development listed below when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line.

1. New dwellings.
2. Expansions of structures in that add area to any street-facing façade. The design standards for such expansions are applicable as follows:
 - a. Expansions that add 75 square feet or less of street-facing façade area are exempt from all design standards.
 - b. Expansions that add more than 75 square feet and less than 200 square feet of street-facing façade area are subject to Subsection Section 14.30(B)(1), Eyes on the Street. The expanded façade area must meet

the standards of Section 14.30(B)(1), Eyes on the Street, without consideration of the original street-facing façade area.

- c. Expansions that add 200 square feet or more of street-facing façade area are subject to the following design standards:
 - 3. The entire street-facing façade shall comply with Section 14.30(B) P (2) Windows.
 - 4. Section 14.30(B)(3) Main Entrance is applicable if an expansion would create a new main entrance. No expansion shall bring the street-facing façade out of conformance, or further out of conformance if already nonconforming, with the design standard.
 - 5. Section 14.30(B)(1) Articulation is applicable for expansions that add 20 lineal feet or more to the length of the street-facing façade.
 - 6. Section 14.30(B)(4) Detailed Design is not applicable for expansions. However, no expansion shall bring the street-facing façade out of conformance, or further out of conformance if already nonconforming, with the Detailed Design standards.
 - 7. Multiple expansions are allowed within a 5-year period if the street-facing façade will comply with the design standards that would have been applicable if the expansions occurred at the same time.
 - 8. Remodels that convert an attached garage to a habitable residential space. When applicable, the design standards apply only to the street-facing façade of the garage being converted. The following design standards are applicable:
 - a. Section 14.30(B)(3) Main Entrance is applicable if the garage conversion would create a new main entrance. No conversion shall bring the street-facing façade out of conformance, or further out of conformance if already nonconforming, with the design standard.

- b. Section 14.30(B)4 Detailed Design is not applicable. However, no conversion shall bring the street-facing façade out of conformance, or further out of conformance if already nonconforming, with the design standard.

C. Standards

All buildings that meet the applicability provisions in Section 14.30(B) Applicability shall meet the following design standards. The graphics provided are intended to illustrate how development could comply with these standards and should not be interpreted as requiring a specific architectural style. An architectural feature may be used to comply with more than one standard.

1. Articulation. All buildings shall incorporate design elements that break up all street-facing façades into smaller planes as follows.
 - a. For buildings with 25 to 50 feet of street frontage, a minimum of 1 of the following elements shall be provided along the street-facing façades.
 - i. A porch at least 5 feet deep.
 - ii. A balcony that is at least 2 feet deep and is accessible from an interior room.
 - iii. A bay window that extends at least 2 feet wide.
 - iv. A section of the façade that is recessed by at least 2 feet deep and 6 feet long.
 - v. A gabled dormer.
 - b. For buildings with over 50 feet of street frontage, at least 1 element in Subsection 4.161(1) i.-v. above shall be provided for every 25 ft of street frontage.
 - c. Elements shall be distributed along the length of the façade so that there are no more than 25 feet between 2 elements.
 - d. For buildings with less than 25 feet of street frontage, the building articulation standard is not applicable.
2. Windows. At least 15% of the area of each street-facing façade must be windows.

- a. Windows used to meet this standard must be transparent and allow views from the building to the street. Glass blocks and privacy windows in bathrooms do not meet this standard.
 - b. Window area is considered the entire area within the outer window frame, including any interior window grid. Glazed portions of entrance doors count as window area.
 - c. Doors used to meet this standard must face the street or be at an angle of no greater than 45 degrees from the street.
 - d. Door area is considered the portion of the door that moves. Door frames do not count toward this standard.
3. Main entrance. At least 1 main entrance must meet both of the following standards.
- a. Be no further than 8 ft behind the longest street-facing wall of the building.
 - b. Face the street, be at an angle of up to 45 degrees from the street, or open onto a porch. If the entrance opens up onto a porch, the porch must meet all of these additional standards.
 - i. Be at least 25 sq ft in area with a minimum 4-ft depth.
 - ii. Have at least 1 porch entry facing the street.
 - iii. Have a roof that is no more than 12 ft above the floor of the porch.
 - iv. Have a roof that covers at least 30% of the porch area.
4. Detailed design. All buildings shall include at least 5 of the following features on any street-facing façade.
- a. Covered porch at least 5 ft deep, as measured horizontally from the face of the main building façade to the edge of the deck, and at least 5 ft wide.
 - b. Recessed entry area at least 2 ft deep, as measured horizontally from the face of the main building façade, and at least 5 ft wide.
 - c. Offset on the building face of at least 16 in from 1 exterior wall surface to the other.

- d. Dormer that is at least 4 ft wide and integrated into the roof form.
 - e. Roof eaves with a minimum projection of 12 in from the intersection of the roof and the exterior walls.
 - f. Roof line offsets of at least 2 ft from the top surface of 1 roof to the top surface of the other.
 - g. Tile or wood shingle roofs.
 - h. Horizontal lap siding between 3 to 7 in wide (the visible portion once installed). The siding material may be wood, fiber-cement, or vinyl.
 - i. Brick, cedar shingles, stucco, or other similar decorative materials covering at least 40% of the street-facing façade.
 - j. Gable roof, hip roof, or gambrel roof design.
 - k. Window trim around all windows at least 3 in wide and 5/8 in deep.
 - l. Window recesses, in all windows, of at least 3 in as measured horizontally from the face of the building façade.
 - m. Balcony that is at least 3 ft deep, 5 ft wide, and accessible from an interior room. For Townhouses this standard is 2 ft deep and 4 ft wide.
 - n. One roof pitch of at least 500 sq ft in area that is sloped to face the southern sky and has its eave line oriented within 30 degrees of the true north/south axis.
 - o. Bay window at least 2 ft deep and 5 ft long. For Townhouses this standard is 2 ft deep by 4 ft wide.
 - p. Attached garage width, as measured between the inside of the garage door frame, of 35% or less of the length of the street-facing façade.
 - q. For Townhouses, balconies and bay windows may encroach into a required setback area.
5. Transitional space. Ground floor dwellings which have their entry access from the street or a shared open space (e.g. a common courtyard) must include an area of transition between the public realm of the right-of-way (or

tract or easement). The standards below apply when the private dwelling entry access door is within 10 feet of the street-facing property line; within the front yard setback, or within 10 feet of a shared open space common tract. The transitional space between the public realm and the entry door may be either vertical or horizontal, as described below.

- a. A vertical transition must be an uncovered flight of stairs that leads to the front door or front porch of the dwelling. The stairs must rise at least 3 ft, and not more than 8 ft, from grade. The flight of stairs may encroach into the required front yard, and the bottom step must be at least 4 ft from the front lot line.
 - b. A horizontal transition shall be a covered porch with a depth of at least 6 ft. The porch may encroach into the required front yard, but it must be at least 4 ft from the front lot line.
6. Pedestrian circulation. The on-site pedestrian circulation system must include the following:
 - a. Continuous connections between the primary buildings, streets abutting the site, ground level entrances, common buildings, common open space, and vehicle and bicycle parking areas.
 - b. At least 1 pedestrian connection to an abutting street frontage for each 200 linear ft of street frontage.
 - c. Pedestrian walkways must be separated from vehicle parking and maneuvering areas by physical barriers such as planter strips, raised curbs, or bollards.
 - d. Walkways must be constructed with a hard surface material, must be permeable for stormwater, and must be no less than 3 ft wide. If adjacent to a parking area where vehicles will overhang the walkway, a 7-ft-wide walkway must be provided. The walkways must be separated from parking areas and internal driveways using curbing, landscaping, or distinctive paving materials.
7. Off-street parking.
 - a. Off-street parking may be arranged in clusters, subject to the following standards:
 - i. Cottage cluster projects with fewer than 16 cottages are permitted parking clusters of not more than five (5) contiguous spaces.


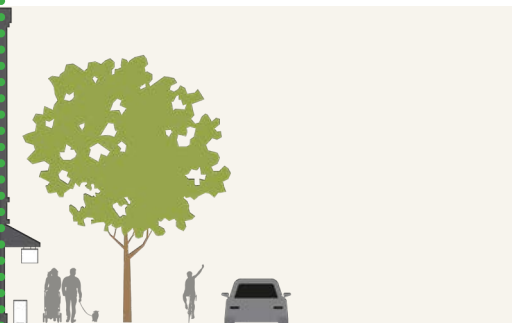

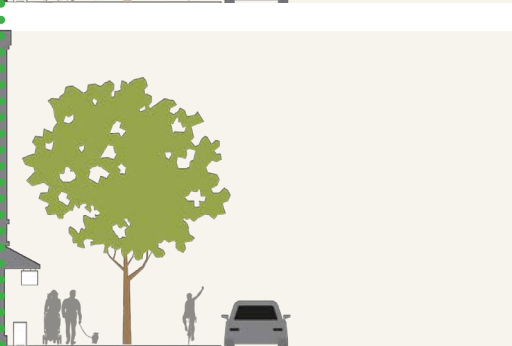
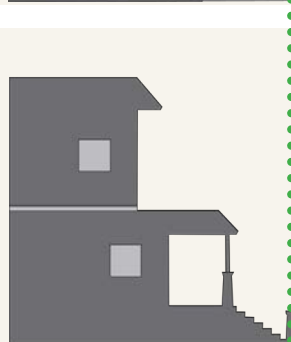
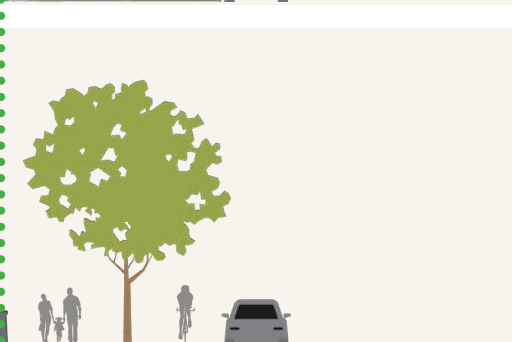
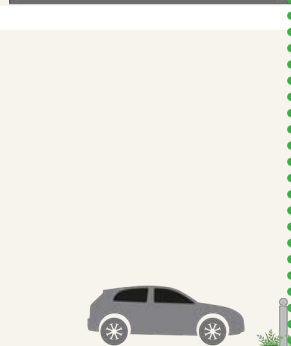
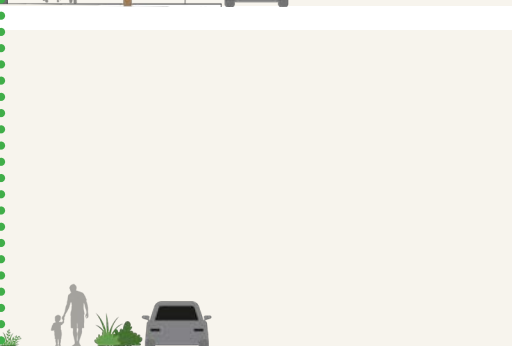
- ii. Cottage cluster projects with 16 cottages or more are permitted parking clusters of not more than eight (8) contiguous spaces.
 - iii. Parking clusters must be separated from other spaces by at least four (4) feet of landscaping.
- b. Clustered parking areas may be covered.
- c. Off-street parking spaces and vehicle maneuvering areas must not be located:
 - i. Within of 20 feet from any street property line, except alley property lines;
 - ii. Between a street property line and the front façade of cottages located closest to the street property line. This standard does not apply to alleys.
- d. Off-street parking spaces must not be located within 10 feet of any other property line, except alley property lines. Driveways and drive aisles are permitted within 10 feet of other property lines.
- e. Landscaping, fencing, or walls at least three feet tall must separate clustered parking areas and parking structures from common courtyards and public streets.
- f. Garages and carports (whether shared or individual) must not abut common courtyards.
- g. Individual attached garages up to 200 square feet must be exempted from the calculation of maximum building footprint for cottages.
- h. Individual detached garages must not exceed 400 square feet in floor area.
- i. Garage doors for attached and detached individual garages must not exceed 20 feet in width.

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FRONTAGE TYPES

BUILD-TO LINE / FRONTAGE LINE

FRONTAGE CATEGORIES

| | | | | | | |
|--------------------------------|--|---|---------------------|-----------------------------|--------------|--|
| SHOPFRONT |  |  | URBAN Frontages | URBAN RESIDENTIAL Frontages | RETAIL-READY | |
| FORECOURT |  |  | | | | |
| PORCH-STOOP-TERRACE |  |  | | | | |
| LANDSCAPE FRONTAGE 1, 2, AND 3 |  |  | LANDSCAPE Frontages | | | |

EXAMPLES



Shopfront frontage is characterized by a façade that is aligned close to the frontage line with the building entrance at sidewalk grade.



Forecourt frontage is characterized by portions of the façade close to the frontage line alternated by portions of the façade which are set back.

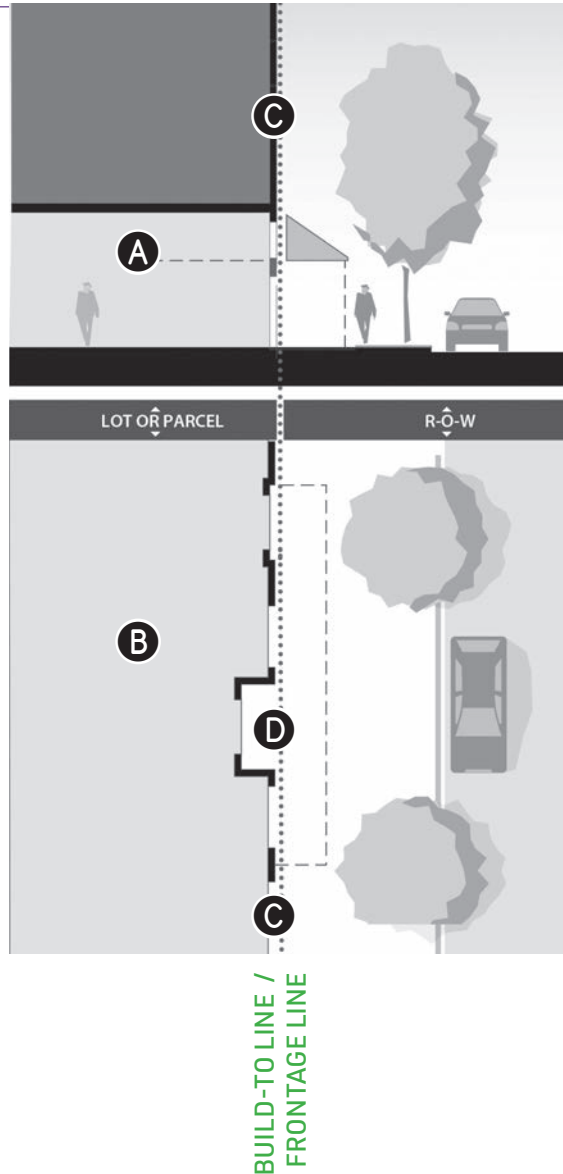


Porch-stoop-terrace frontage is characterized by a façade that is aligned close to the frontage line with the first story set back from the frontage line with a porch, a stoop, or a terrace.



Landscape frontage 2 is characterized by a low wall, fence, and/or vertical landscaping such as a hedge that is aligned with the frontage line.

FRONTAGE TYPES



URBAN FRONTAGE – SHOPFRONT

The Shopfront Frontage is characterized by a façade that is built up to the Build To Line or Frontage Line which is typically the front lot line.

The building entrance shall be at sidewalk grade. The Shopfront Frontage shall have substantial glazing on the ground floor. Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

A “Retail ready” minimum ground floor height (feet) 18

B “Retail ready” minimum ground floor depth (feet) 20

C Building Setback from Build-to Line (feet) 0

Retail ready ground floor shall be constructed to meet commercial building standards.

D Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.

Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.



*Retail-ready
shopfronts*

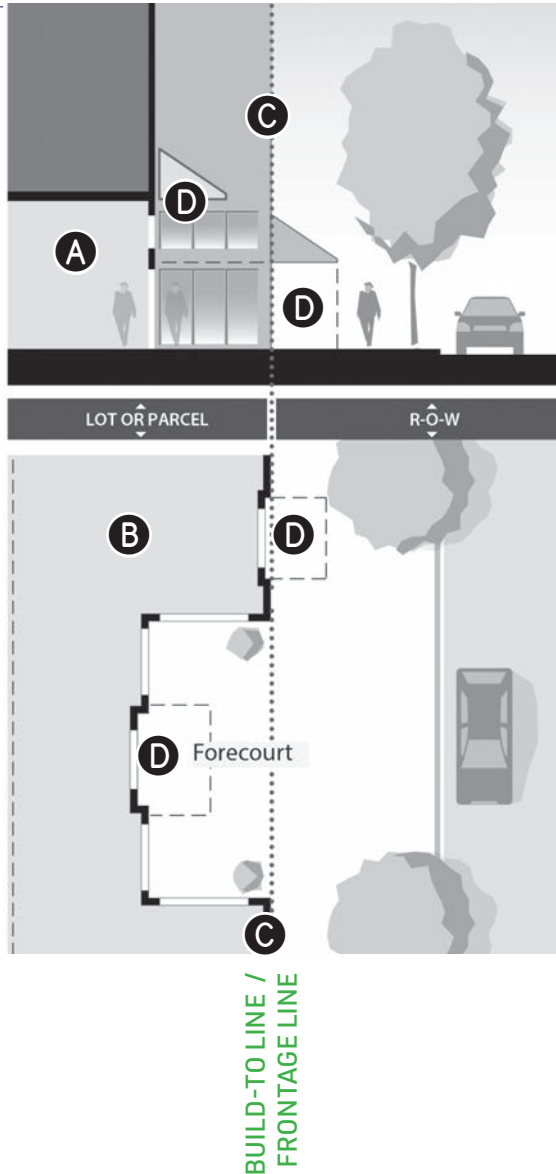


*Bay window and
awning encroachments
into the Right of Way*



Recessed entry

FRONTAGE TYPES



URBAN FRONTAGE – FORECOURT

Forecourt Frontage is created by recessing a portion of the façade behind the Build To Line or Frontage Line. Forecourt Frontage shall be used in conjunction with the Shopfront Frontage. Forecourt Frontage is appropriate for commercial or residential uses, outdoor seating, and hardscaped plaza and/or landscaped gardens.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

A “Retail ready” minimum ground floor height (feet) 18

B “Retail ready” minimum ground floor depth (feet) 20

C Building Setback from Build-to Line for Shopfront Frontage (feet) 0

Retail ready ground floor shall be constructed to meet commercial building standards.

C Each dwelling unit which is accessed from the forecourt or the sidewalk shall comply with either the vertical or horizontal standard.

Vertical separation –
Distance from ground:
Minimum 18 inches;
maximum 3 feet;
Horizontal separation –
Distance from Build To
Line: Minimum 3 feet;
maximum 15 feet

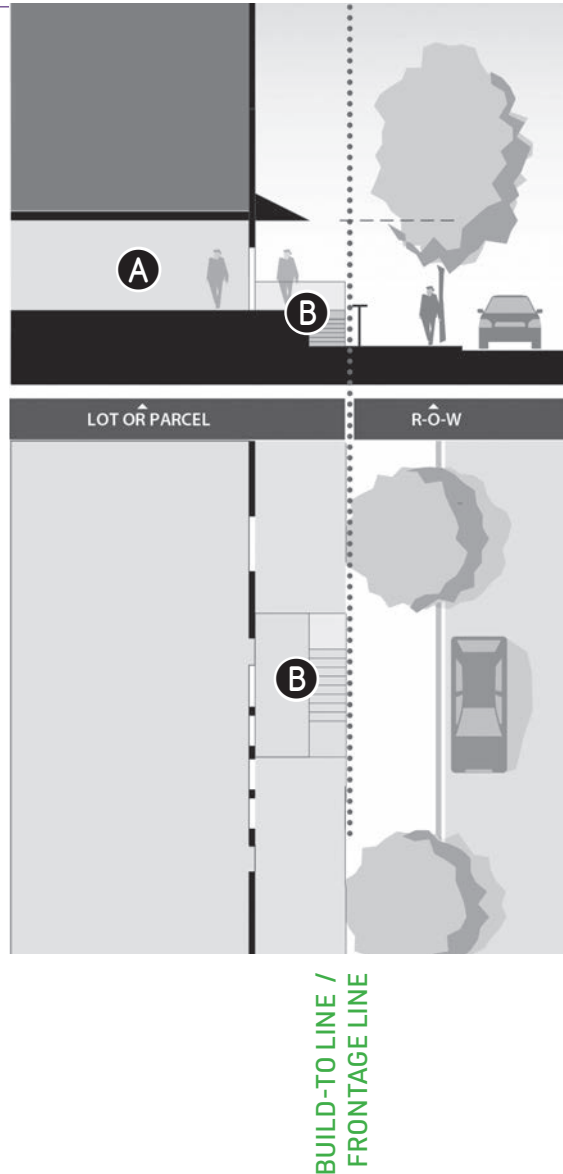
D Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.



Forecourts shared by multiple retail establishments and restaurants



FRONTAGE TYPES



URBAN FRONTAGE – RESIDENTIAL PORCH-STOOP-TERRACE

Urban Residential Building Frontage is characterized by a façade which is set behind the Build To Line and a building entry threshold, such as a porch or terrace, set between the building and the Build To Line or Frontage Line. The threshold may be at the pedestrian sidewalk level, elevated above it, or sunken below it. The residential building entry is accessed from the threshold. Landscaping may be provided in the setback area between the building and the sidewalk. A Porch-Stoop-Terrace Frontage is appropriate for residential uses and service commercial or office uses.

A Minimum ground floor height (feet)

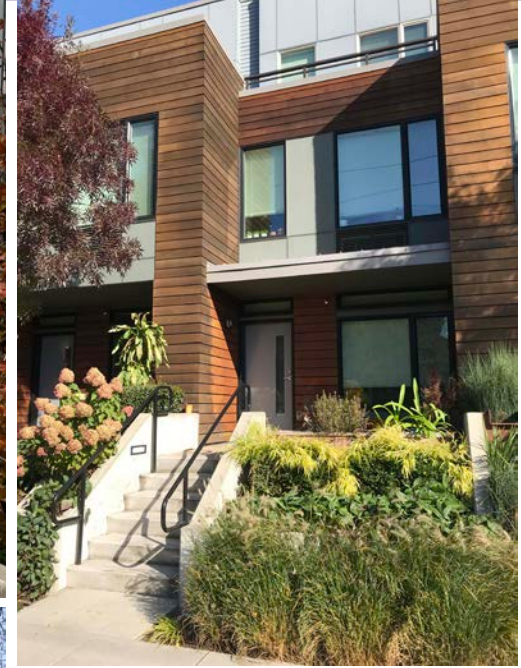
9

B Each dwelling unit which is accessed from the sidewalk shall comply with either the vertical or horizontal standard.

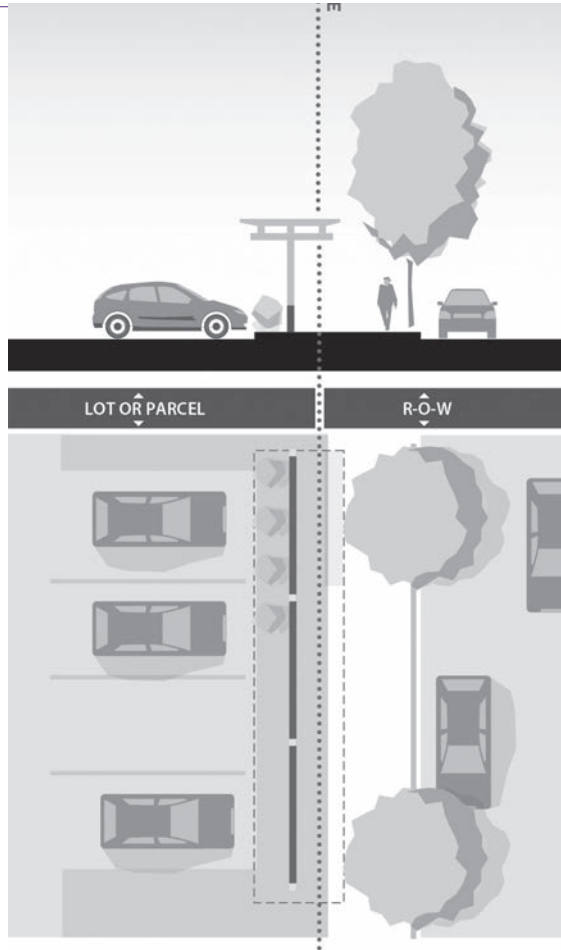
Vertical separation – Distance from ground: Minimum 18 inches; maximum 3 feet;
Horizontal separation – Distance from Build To Line: Minimum 3 feet; maximum 15 feet



*Stoop and terrace
entries*



FRONTAGE TYPES



LANDSCAPE FRONTAGE 1 – LOW WALL AND TRELLIS

A Vertical Landscaping or Structure Setback from Build-to Line, maximum feet

5

B Height of Structure or Planting, feet

The underside of the Trellis portion of a Low Wall and Trellis shall be a minimum of 8 feet above grade and a maximum of 14 feet above grade.

The Low Wall portion of a Low Wall and Trellis shall be a minimum of 1.5 feet and a maximum of 3 feet and have a minimum depth of 1.5 feet.

C Materials

The Trellis shall be heavy timber or steel (or a similar material) and shall consist of open structure with no decking or awning material.

The Trellis shall have masonry, heavy timber, or steel (or similar metal) supporting columns spaced no more than 30 feet on center.

The Low Wall shall be wood, masonry, and/or concrete.

D Openings

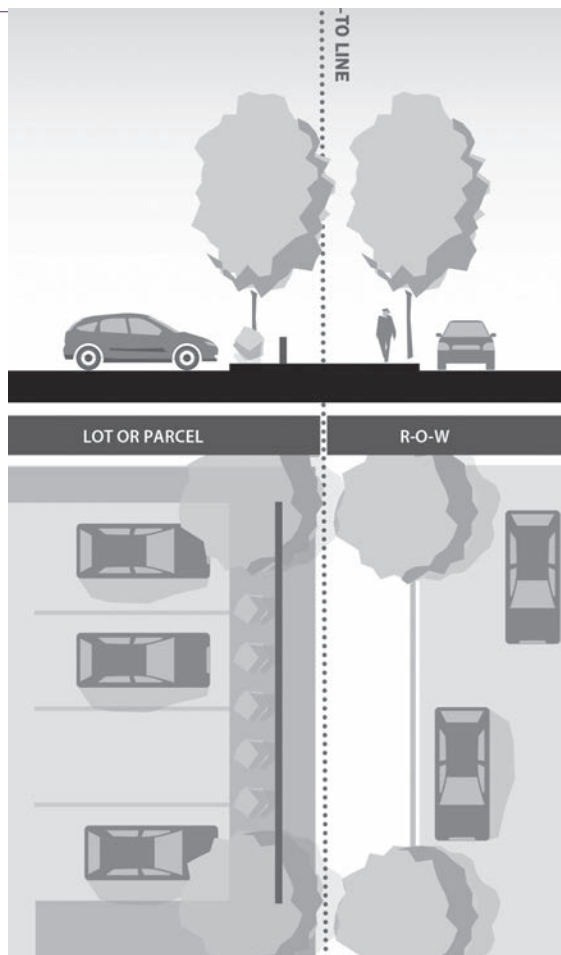
Openings in the Low Wall and Trellis are allowed for pedestrian pathways, sidewalks, plazas, and driveways.

E Surface Parking Setback

Surface Parking shall be set back a minimum of 3 feet from the Low Wall and Trellis.

F Ground Cover and Planting

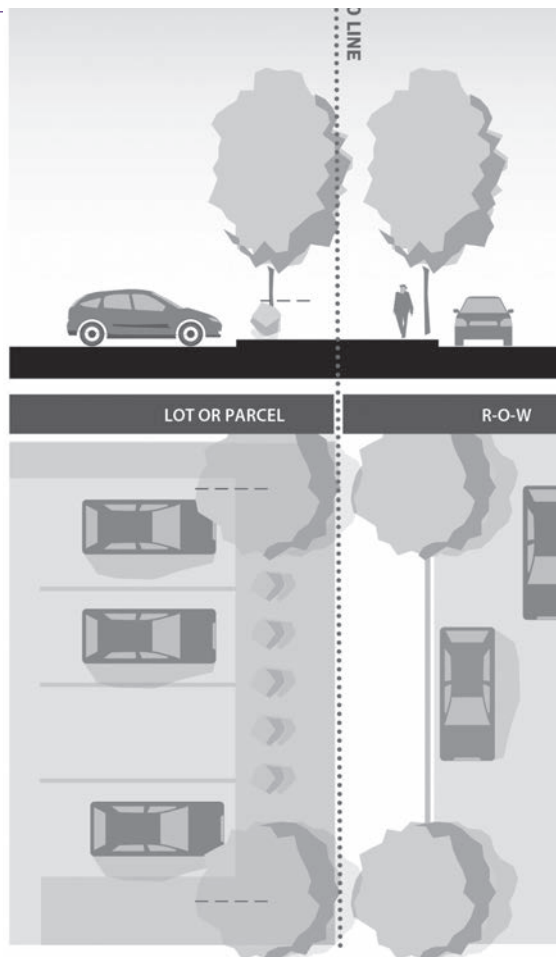
The area between the Build-to Line and the Trellis shall be hardscaped with either masonry pavers or stamped concrete. The setback between the Low Wall and surface parking shall be planted with low shrubs, groundcover, and climbing plants.



LANDSCAPE FRONTAGE 2 - URBAN FENCE OR WALL

- | | |
|---|---|
| A Vertical Landscaping or Structure Setback from Build-to Line, maximum feet | 5 |
| B Height of Structure or Planting, feet | The fence or the wall shall be at least 2 feet high and no more than 3 feet high. |
| C Materials | Walls shall be wood masonry, and/or concrete; fences shall be made of wrought iron, steel, or a similar material (but not chain-link) and must be dark in color. Fences may be no more than 50% sight obscuring. |
| D Openings | Openings in the Urban Fence or Wall are allowed for pedestrian pathways, sidewalks, plazas, and driveways. |
| E Surface Parking Setback | The surface parking area shall be set back, at a minimum, an additional 5 feet to provide room for required landscaping and stormwater infiltration and/or retention. |
| F Ground Cover and Planting | In addition to the required fence or wall, trees and shrubs shall be provided. One large tree is required every 30 linear feet minimum. The shrubs shall be at least as high as the wall or fence, and shall be no more than 6 feet high. |

FRONTAGE TYPES



LANDSCAPED FRONTAGE 3 - LOW HEDGE

| | | |
|----------|--|---|
| A | Vertical Landscaping or Structure Setback from Build-to Line, minimum feet | 5 |
| B | Height of Structure or Planting, feet | The shrubs shall be a minimum of 3 feet high. If a low wall is provided in place of shrubs it shall be a minimum of 3 feet high. |
| C | Materials | The surface parking area shall be screened with a continuous row of hedges or shrubs immediately adjacent to the parking area, except where there is a driveway. Shrubs must be mostly opaque year round. A low wall may be substituted for the shrubs but the trees and groundcover plants are still required. |
| D | Openings | Openings in the Landscape Frontage are allowed for pedestrian pathways, sidewalks, plazas, and driveways. |
| E | Surface Parking Setback | 10 feet minimum |
| F | Ground Cover and Planting | In addition to the required shrubs, one large tree is required every 30 linear feet. The shrubs/hedge shall be interrupted with a gap of up to 2 feet wide in order to accommodate trees. |

FRONTAGE TYPES





07

References

Traditional Downtown/CBD

Table 3-11 provides design guidance for the respective design elements for ODOT roadways through the Traditional Downtown/CBD context. With this design approach, the goal is to design roadways in the Traditional Downtown/CBD context for a target speed of 20-25 mph. Figure 3-8 illustrates various cross section scenarios for how the design elements within this type of context may be arranged.

Table 3-11: Design Element Recommendations for Traditional Downtown/CBD

| | Design Element | Guidance |
|-------------------------------|---|------------|
| Pedestrian Realm | Frontage Zone | 4' to 2' |
| | Pedestrian Zone | 10' to 8' |
| | Buffer Zone | 6' to 0' |
| | Curb/Gutter ¹ | 2' to 0.5' |
| Transition Realm ⁶ | Separated Bicycle Lane (Curb Constrained Facility) ² | 8' to 7' |
| | On-Street Bicycle Lane (not including Buffer) ² | 6' to 5' |
| | Bicycle/Street Buffer ² | 3' to 2' |
| | Right Side Shoulder (if travel lane directly adjacent to curb) ^{3,5} | 2' to 0' |
| Travelway Realm ⁵ | On-Street Parking | 7' to 8' |
| | Travel Lane ^{4,5} | 11' |
| | Right Turn Lane (including Sny Distances) | 11' to 12' |
| | Left Turn Lane ⁴ | 11' |
| | Left Side / Right Side Sny Distance | 1' to 0' |
| | Two-Way-Left-Turn Lane | 11' to 12' |
| | Raised Median – No Turn Lane (including Sny Distances) | 8' to 11' |
| | Left-Turn Lane with Raised Curb Median/separator (includes 16" separator & Sny Distances) | 12' to 14' |

¹ Where curb and gutter is used and on-street parking is provided or travel lane is directly adjacent to curb, gutter pan should be included in shoulder/sny or on-street parking measurement. Gutter pan should be included in travel lane, bicycle lane or turn lane measurements only where a smooth transition from gutter pan to roadway surface is provided.

² Refer to Bicycle Facility Selection process (Section 3.2.2) to determine appropriate bicycle facility type. Consider raised bicycle lanes where appropriate. 5-foot on-street bicycle lane is allowed only with a street buffer. When a raised buffer is used to protect the bicycle lane, the width should be 6' if parking is adjacent or if signs or other features are anticipated.

³ Overall shoulder width depends on other section elements. Elimination of shoulder width/lateral offsets should only be considered in constrained locations and needs to be balanced with all cross-section and drainage needs. If the travel lane is next to a curb with a gutter (e.g., a 2-foot curb zone), the gutter typically serves as the right-side shoulder. A wider shoulder may be needed to accommodate drainage based on hydrological analysis or other specific needs.

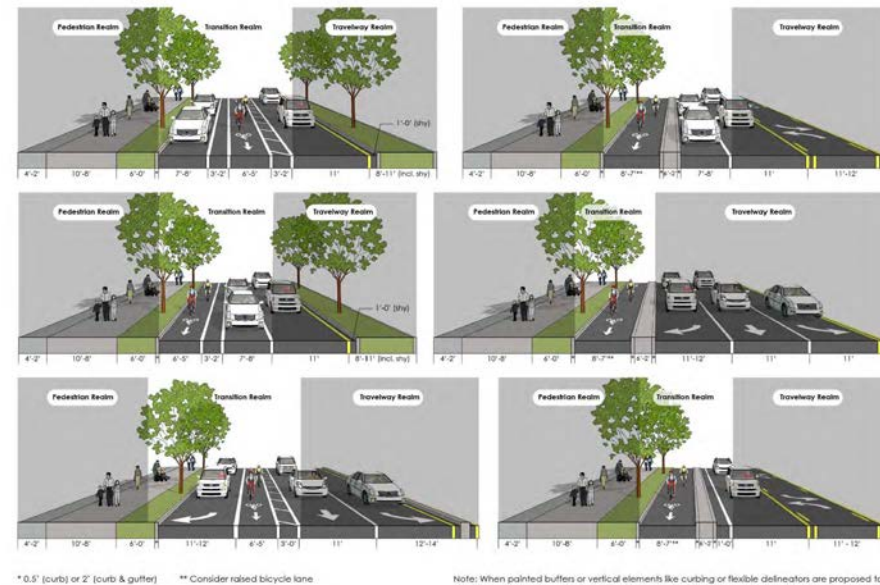
⁴ 11-foot lane width preferred to 12-foot lane; 10-foot lane width requires design approval from the State Roadway Engineer. On freight or transit-oriented streets, a 10-foot travel lane is generally not appropriate without a buffer zone or shoulder.

⁵ On Reduction Review Routes, comply with ODOT Freight Mobility Policies, ORS 366.215 and OAR 731-012. Element dimensions may need to be modified.

⁶ When painted buffers or vertical elements like curbing or flexible delineators are proposed to provide separation in a bicycle facility design, evaluate long-term maintenance needs and provide a solution to identified problems.

ODOT Urban Blueprint

Figure 3-8: Example Cross Section Options for Traditional Downtown/CBD. See Table 3-11 for additional information.



Urban Mix

Table 3-12 provides design guidance for the respective design elements for ODOT roadways through the Urban Mix context. With this design approach, the goal is to design roadways for a target speed of 25-30 mph. Figure 3-9 illustrates various cross section scenarios for how the design elements within this type of context may be arranged.

Table 3-12: Design Element Recommendations for Urban Mix

| Design Element | | Guidance |
|-------------------------------|--|------------|
| Pedestrian Realm | Frontage Zone | 1' |
| | Pedestrian Zone ⁷ | 8' to 5' |
| | Buffer Zone | 6' to 0' |
| | Curb/Gutter ¹ | 2' to 0.5' |
| Transition Realm ⁶ | Separated Bicycle Lane (Curb Constrained Facility) ² | 8' to 7' |
| | On-Street Bicycle Lane (not including Buffer) ² | 6' to 5' |
| | Bicycle/Street Buffer (preferred for On-Street Lane) ² | 4' to 2' |
| | Right Side Shoulder (if travel lane directly adjacent to curb) ^{3,5} | 2' to 0' |
| | On-Street Parking | 8' |
| Travelway Realm ⁵ | Travel Lane ^{4,5} | 11' to 12' |
| | Right Turn Lane (including Shy Distances) | 11' to 12' |
| | Left Turn Lane ⁴ | 11' to 12' |
| | Left Side / Right Side Shy Distance | 1' to 0' |
| | Two-Way Left-Turn Lane | 11' to 12' |
| | Raised Median – No Turn Lane (including Shy Distances) | 8' to 11' |
| | Left-Turn Lane with Raised Curb Median/Separator (including 16" separator & Shy Distances) | 12' to 14' |

¹ Where curb and gutter is used and on-street parking is provided or travel lane is directly adjacent to curb, gutter pan should be included in shoulder/shy or on-street parking measurement. Gutter pan should be included in travel lane, bicycle lane or turn lane measurements only where a smooth transition from gutter pan to roadway surface is provided.

² Refer to Bicycle Facility Selection process (Section 3.2.2) to determine appropriate bicycle facility type. Consider raised bicycle lanes where appropriate. 5-foot on-street bicycle lane is allowed only with a street buffer. When a raised buffer is used to protect the bicycle lane, the width should be 6' if parking is adjacent or if signs or other features are anticipated.

³ Overall shoulder width depends on other section elements. Elimination of shoulder width/lateral offset should only be considered in constrained locations and needs to be balanced with all cross-section and drainage needs. If the travel lane is next to a curb with a gutter (e.g., a 2-foot curb zone), the gutter typically serves as the right-side shoulder. A wider shoulder may be needed to accommodate drainage based on hydrological analysis or other specific needs.

⁴ 11-foot lane width preferred; 10-foot lane width requires design approval from the State Roadway Engineer. On right- or transition-oriented streets, a 10-foot travel lane is generally not appropriate without a buffer zone or shoulder.

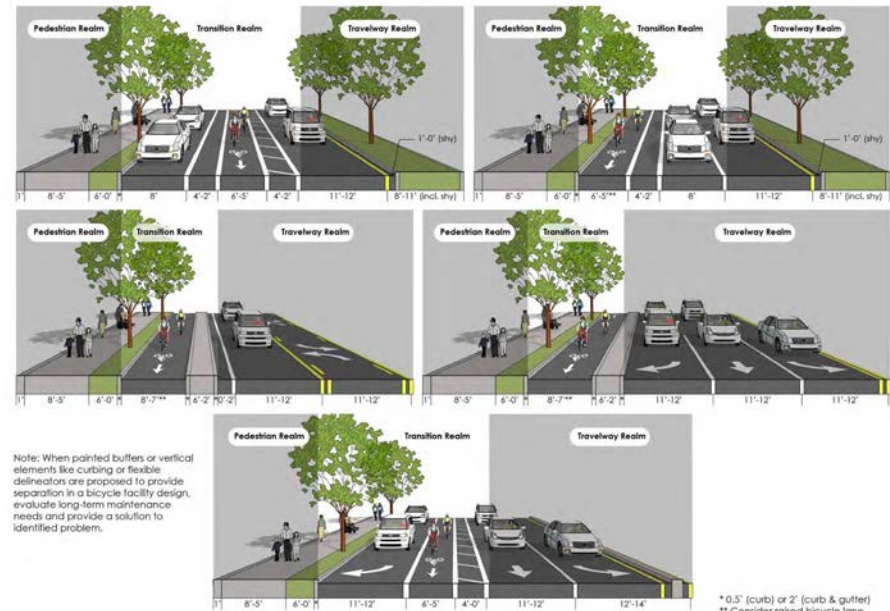
⁵ On Reduction Review Routes, comply with ODOT Freight Mobility Policies, ORS 366.215 and OAR 731-012. Element dimensions may need to be modified.

⁶ When painted buffers or vertical elements like curbing or flexible delineators are proposed to provide separation in a bicycle facility design, evaluate long-term maintenance needs and provide a solution to identified problems.

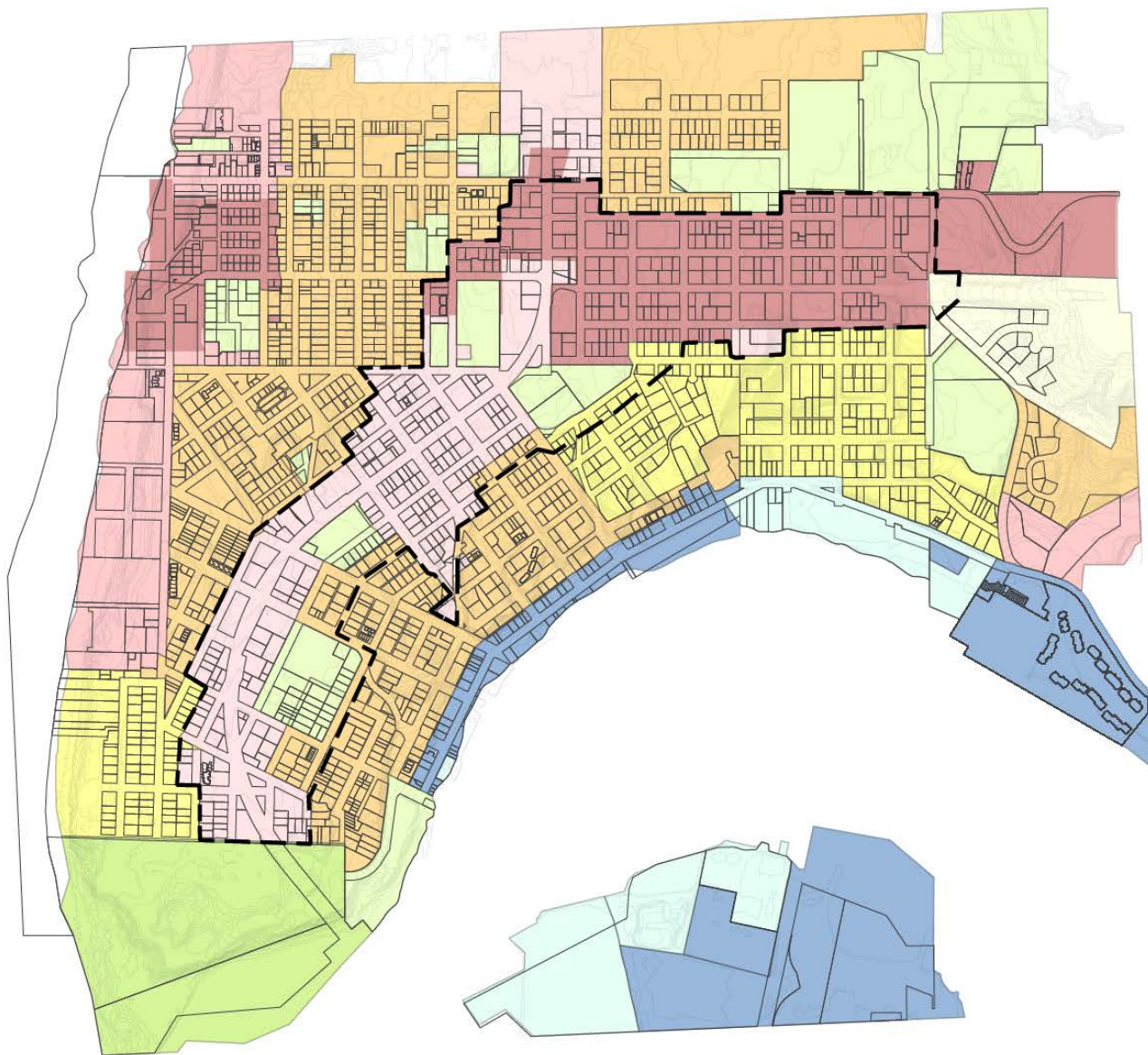
⁷ 5-foot pedestrian zone requires a paved frontage zone and/or a paved buffer zone. Minimum "sidewalk" width is 6-feet.

ODOT Urban Blueprint

Figure 3-9: Example Cross Section Options for Urban Mix. See Table 3-12 for additional information.



Base Zones



Newport Zoning Map

Zoning

| | | |
|-----|-----|-----|
| C-1 | R-1 | P-1 |
| C-2 | R-2 | P-2 |
| C-3 | R-3 | P-3 |
| I-1 | R-4 | W-1 |
| I-2 | | W-2 |
| I-3 | | |

| | |
|--|--|
| | Tax Lots 2023 |
| | City Center Revitalization Plan Boundary |



0 500 1,000 1,500 2,000 ft

Land Uses

CCDRD - LAND USE LISTING

| Applicable regulations | C-1 | C-3 | P-1 | R-2 | R-4 |
|---|-----|-----|-----|-----|-----|
| Uses | | | | | |
| Office/Professional Offices | P | P | | | C |
| Retail sales and service | | | | | |
| Sales-oriented, general retail | P | P | | | |
| Sales-oriented, general retail | C | P | | | |
| Personal services / Beauty and Barber Shops | P | P | | | C |
| Entertainment | P | P | | | |
| Repair-oriented | P | P | | | |
| Major Event Entertainment | C | P | | | |
| Vehicle Repair | C | P | | | |
| Self-Service Storage | X | P | | | |
| Parking Facility | P | P | | | |
| Contractors and Industrial Service | X | P | | | |
| Manufacturing and Production | | | | | |

| Applicable regulations | C-1 | C-3 | P-1 | R-2 | R-4 |
|--|-----|-----|-----|-----|-----|
| Custom Creative Work | P | P | | | |
| Light Manufacturing | X | C | | | |
| Warehouse, Freight Movement, & Distribution | X | P | | | |
| Wholesale Sales | X | P | | | |
| Waste and Recycling Related | C | C | | | |
| Basic Utilities | P | P | | | |
| Utility Corridors | C | C | | | |
| Community Service | P | P | | | |
| Family Child Care Home | P | P | | | P |
| Child Care Center | P | P | | | P |
| Educational Institutions | | | | | |
| Elementary & Secondary Schools / Public Schools | C | C | P | | |
| College and Universities / Public Colleges or Universities | P | P | P | | C |
| Trade/Vocational Schools/Other | P | P | | | |
| Hospitals | C | C | | | P |

| Applicable regulations | C-1 | C-3 | P-1 | R-2 | R-4 |
|--|-----|-----|-----|-----|-----|
| Courts, Jails and Detention Facilities | X | P | P | | |
| Communication Facilities | P | P | | | |
| Residences on Floors Other than Street Grade | P | P | | | |
| Affordable Housing | P | P | | | |
| Transportation Facilities | P | P | P | | P |
| Residential | | | P | | |
| Single-Family | | | | P | P |
| Two-Family | | | | P | P |
| Townhouse | | | | P | P |
| Single Room Occupancy | | | | P | P |
| Cottage Cluster | | | | P | P |
| Multi-family | | | | P | P |
| Manufactured Homes | | | | P | P |
| Manufactured Dwelling Park | | | | P | P |
| Accessory Dwelling Units | P | P | | P | P |
| Accessory Uses | | | | P | P |

| Applicable regulations | C-1 | C-3 | P-1 | R-2 | R-4 |
|--|-----|-----|-----|-----|-----|
| Home Occupations | | | | P | P |
| Community Services / Community Buildings | | | P | | |
| Parks | | | | P | P |
| Publicly Owned Recreation Facilities | | | | C | C |
| Libraries | | | | C | C |
| Utility Substations | | | | C | C |
| Public or Private Schools | | | | C | P |
| Religious Institutions / Places of Worship | | | | C | C |
| Emergency Shelter | | | | P | P |
| Residential Care Homes | | | | | P |
| Nursing Homes | | | | X | P |
| Motels and Hotels | | | | X | C |
| Rooming and Boarding Houses | | | | P | P |
| Membership Organizations | | | | X | P |
| Museums | | | | X | P |
| Condominiums | | | | P | P |

| Applicable regulations | C-1 | C-3 | P-1 | R-2 | R-4 |
|--|-----|-----|-----|-----|-----|
| Hostels | | | | X | C |
| Recreational Vehicle Parks | | | | X | C |
| Necessary Public Utilities and Public Service Uses or Structures | | | | C | C |
| Residential Facility | | | | P | P |
| Movies Theaters | | | | X | C |
| Assisted Living Facilities | | | | C | P |
| Bicycle Shop | | | | X | C |
| Short-Term Rentals | | | | P | P |
| Public Open Space | | | P | | |
| Any Building Erected by a Governmental Entity | | | P | | |
| Fairgrounds | | | P | | |
| Public Cemeteries | | | P | | |
| Water & Wastewater Treatment Plants | | | P | | |
| Performing Arts Centers | | | P | | |
| Visual Arts Centers | | | P | | |

| Applicable regulations | C-1 | C-3 | P-1 | R-2 | R-4 |
|---|-----|-----|-----|-----|-----|
| Senior Centers | | | P | | |
| Airport and Accessory Structures | | | P | | |
| Public Golf Courses | | | P | | |
| City Halls | | | P | | |
| County Courthouses | | | P | | |
| City of County Maintenance Facilities | | | P | | |
| Publicly Owned Recreational Vehicle Parks | | | C | | |
| Public Museums | | | P | | |
| Public Restrooms | | | P | | |
| Recreation Equipment | | | P | | |
| Post Office | | | P | | |
| Parking Lots | | | P | | |
| Public Hospitals | | | P | | |
| Water Storage Facilities | | | P | | |
| Public Libraries | | | | | |
| Fire Stations | | | | | |

| Applicable regulations | C-1 | C-3 | P-1 | R-2 | R-4 |
|---|-----|-----|-----|-----|-----|
| Police Stations | | | | | |
| Accessory Structures for Any of the Above [Public Uses] | | | | | |

CHAPTER 14.17 CLEAR VISION AREAS

14.17.010 Purpose

The purpose of this section is to promote safety at intersections and drive access points by reducing obstructions to clear vision at intersections.

14.17.020 Clear Vision Area Defined

A vision clearance area includes the following:

- A. At the intersection of two streets, a triangle formed by the intersection of the curb lines, with each leg of the vision clearance triangle being a minimum of 35 feet in length. Where curbs are absent, the edge of the asphalt or future curb locations shall be used as a guide. The City Engineer may modify this requirement, in writing, upon finding that more or less distance is required (i.e., due to traffic speeds, roadway alignment, etc.).
- B. A portion of a lot subject to a front yard setback as defined in [Section 14.11](#). A clear vision area does not include that portion of a second front yard outside of the area described in subsection (A).

14.17.030 Clear Vision Area Requirements

A clear vision area shall contain no planting, fence, wall, structure, or temporary or permanent obstruction, except for an occasional utility pole or tree, exceeding three feet in height, measured from the top of the curb, or where no curb exists, from the street centerline grade. Trees located within a clear vision area shall have their branches and foliage removed to the height of eight feet above the grade.

14.17.040 Maintenance of Clear Vision Areas

It shall be the duty of the person who owns, possesses, or controls real property or right-of-way adjacent thereto, to maintain a clear vision area in the manner provided in this section.

14.17.050 Exemptions for Buildings

A building erected in compliance with zoning ordinance setbacks is exempt from this section.

14.17.060 Liability

The person owning, in possession of, occupying or having control of any property within the city shall be liable to any person who is injured or otherwise suffers damage by reason of the failure to remove or trim obstructions and vegetation as required by this section. Furthermore, the person shall be liable to the city for any judgment or expense incurred or paid by the city, by reason of the person's failure to satisfy the obligations imposed by this section.

14.17.070 Variances

The requirements of this section shall be subject to the processes and criteria contained in [Section 14.33](#).

(Chapter 14.17 repealed and re-enacted by Ordinance No. 2031, adopted March 5, 2013; and made effective by Ordinance No. 2054 adopted on June 3, 2013; effective June 13, 2013.)

CHAPTER 14.18 SCREENING AND BUFFERING BETWEEN RESIDENTIAL AND NONRESIDENTIAL ZONES

14.18.010 Height Buffer

Non-residential sites of districts abutting or having any portion located adjacent to any residential zone shall have a height limitation beginning at a height of ten feet at the property line abutting the residential zone and increasing at a slope of 1:2 for R-1 property, 1:1 for R-2 property, 2:1 for R-3 property, and 3:1 for R-4 property until intersecting the height limit otherwise established in that district.

14.18.020 Adjacent Yard Buffer

On any portion of a site in a non-residential zone that abuts a residential zone, a minimum interior yard of 10 feet planted and maintained as a landscaped screen shall be required.

14.18.030 Separated Yard Buffer

On any portion of a non-residential site that is opposite from a residential district and separated therefrom by a street, alley, creek, drainage facility, or other open area, a minimum yard of ten feet shall be required. The minimum yard shall be planted and maintained as a landscape screen (excluding areas required for access to the site).

CHAPTER 14.19 LANDSCAPING REQUIREMENT

14.19.010 Purpose

The purpose of this section is to provide for the installation, long-term maintenance and protection of trees, vegetation and other landscape elements within the City of Newport recognizing however, that development often times requires the removal of trees and other plant material. When removal is done, the purpose of this section is to require replacement that is attractive, well placed and enhances the overall appearance of the property and the City as a whole. It is further the purpose of this section to:

- A. Aid in air purification and storm water runoff retardation;
- B. Aid in the reduction of noise and glare;
- C. Provide visual buffers;
- D. Enhance the beauty of the city;
- E. Improve property values;
- F. Reduce erosion; and
- G. To protect and enhance the natural beauty, environment and greenspace within the City of Newport to advance economic development, attract residents and promote tourism.

14.19.020 Definitions

For purposes of this section, the following definitions shall apply. Where no definition is given, the common usage of the word shall be used. If there is a conflict between the definitions contained in this section and the more general definitions contained in the definitions section of this Ordinance, this section shall apply.

- A. Addition. An increase in the gross floor area.
- B. Bayfront. The area of the city defined in the Bayfront Plan section of the City's Comprehensive Plan.

- C. Buffer. The use of landscaping, or the use of landscaping along with berms or fences, that obscure the sight from an abutting property and uses, that at least partially and periodically obstructs view and noise. For purposes of this Section, the buffer does not count toward the required landscaping.

(* Entire section amended by Ordinance No. 1827 (9-7-00).)

14.19.020 – Definitions

- D. City Center. The area of the city defined in Section 14.14.100(C) of this Code.

- E. Development. That which is done on a tax lot or parcel of property under one ownership pursuant to any permit issued by the City of Newport Department of Planning and Community Development.

- F. Gross Floor Area. The total area of a building measured by taking the outside dimensions of the building at each floor level intended for occupancy or storage.

- G. Hanging Basket. A basket of flowers or other plant material that is over a public right-of-way or private property and attached to a building, pole, wall, tree or other attachment. In no case shall a hanging basket be less than eight feet above a sidewalk or other pedestrian way or within two feet of a street or driveway.

- H. Landscaped Area. That area within the boundaries of a given lot or other area authorized for landscaping purposes which is devoted to and consists of landscaping.

- I. Landscaping. Material placed in a landscaped area including but not limited to grass, trees, shrubs, flowers, vines and other groundcover, native plant materials, planters, brick, stone, natural forms, water forms, aggregate and other landscape features, provided, however that the use of brick, stone,

aggregate or other inorganic materials shall not predominate over the use of organic plant material. Landscaping does not include sidewalks, fences, walls, benches or other manufactured materials unless same is incidental to the total area of the landscaped area.

- J. Landscaping Plan. A drawing to scale showing the location, types and density of landscaping.
- K. Maintain or Maintenance. Any activity such as pruning, mulching, mowing, fertilizing, removal and replacement of dead vegetation and other similar acts that promote the life, growth, health or beauty of the landscape vegetation.
- L. Nye Beach. The area of the City defined in the Historic Nye Beach Overlay District section of this Ordinance.
- M. Planter. A decorative container for flowers, bushes, trees and other plant materials including but not limited to window boxes, planter boxes, flower pots and other containers.
- N. Sight obscuring. Landscaping, berms, fences, walls or a combination of all those elements that completely blocks the ability to see through it.
- O. Window or Planter Box. A decorative box, pot, or other container that contains flowers and other plant material that is placed immediately below a window, along a walkway or other location. In no case shall a window or planter box extend more than two feet or 20% of the distance from the building to the street curb into the public right-of-way, whichever is less.

14.19.030 Applicability

The provisions of this ordinance shall apply to all new commercial, industrial, public/institutional, and multi-family development, including additions to existing development or remodels.

(Section 14.19.030 was amended by Ordinance No. 2182, adopted on May 17, 2021: effective June 16, 2021.)

14.19.040 General Requirements

The objective of this section is to encourage the planting and retention of existing trees and other vegetation to improve the appearance of off-street parking areas, yard areas and other vehicular use areas; to protect and preserve the appearance, character, and value of surrounding properties, and thereby promote the general welfare, safety and aesthetic quality of the City of Newport; to establish buffer strips between properties of different land uses in order to reduce the effects of sight and sound and other incompatibilities between abutting land uses; to insure that noise, glare and other distractions within one area does not adversely affect activity within the other area. Prior to the issuance of a building permit, landscaping plans showing compliance with this section are required.

- A. No landscape plan submitted pursuant to this section shall be approved unless it conforms to the requirements of this ordinance.
- B. Landscape plans shall be submitted for all development. Said plans shall include dimensions and distances and clearly delineate the existing and proposed building, parking space, vehicular access and the location, size and description of all landscape areas and materials.

(Section 14.19.040(B) was amended by Ordinance No. 2182, adopted on May 17, 2021: effective June 16, 2021.)

- C. Landscaping shall not obstruct the view at the intersection of two or more streets or alleys; or at the intersection of a street and a driveway.
- D. A guarantee of performance bond or escrow agreement shall be required in an amount to be determined by the Planning Director and approved by the City Attorney as to form to insure satisfactory completion of the landscaping plan as approved if the required landscaping is not installed prior to certificate of occupancy as required by the Building Code.

14.19.050 Landscaping Required for New Development, Exceptions

All new development, except for one and two family residences, shall be required to install landscaping per this section. For purposes of this section, new development shall mean construction upon a vacant lot or a lot that becomes vacant by virtue of the demolition of an existing building. Landscaping shall be provided as follows:

- A. Area. Landscaping shall be ten percent of the total square footage of a lot or parcel.
- B. Location. Landscaping shall be located along a street frontage or frontages.
 - 1. For commercial and industrial zoned lots south of the Yaquina Bay Bridge that abut US 101, landscaping shall include a minimum 15-foot wide landscape buffer.

(Section 14.19.050(B)(1) added by the adoption of Ordinance No. 2196 on November 7, 2022; effective December 6, 2022.)

- C. Exceptions. The right-of-way between a curb and a property line, not counting any sidewalk, driveway or other hard surfaces, may be used and counted toward the required landscaping as long as it has been determined by the Planning Director that the right-of-way is not needed for future street expansion. A developer may also plant a street tree within the sidewalk and it shall count toward meeting landscaping requirements subject to approval by the Planning Director and the City Engineer. A window or planter box may also be used to meet landscaping requirements at a ratio of 1 to 1. If the developer chooses to exercise this option, he or she shall enter into an agreement that the landscaping in the right-of-way is to be maintained as landscaping.
- D. Landscaping and Screening for Parking Lots. The purpose of this subsection is to break up large expanses of parking lots with landscaping. Therefore, all parking areas or each parking bay where a development contains multiple parking areas shall comply with the following provisions:
 - 1. A minimum of 10 percent of the total surface area of all parking areas, as measured around the perimeter of all parking spaces and maneuvering

areas, shall be landscaped. This 10 percent landscaping requirement includes landscaping around the perimeter of parking areas as well as landscaped islands within parking areas. Such landscaping shall consist of canopy trees distributed throughout the parking area. A combination of deciduous and evergreen trees, shrubs, and ground cover plants is required. At a minimum, one tree per 12 parking spaces on average shall be planted over and around the parking area.

2. All parking areas with more than 20 spaces shall provide landscape islands with trees that break up the parking area into rows of not more than 12 contiguous parking spaces. Landscape islands and planters shall have dimensions of not less than 48 square feet of area and no dimension of less than 6 feet, to ensure adequate soil, water, and space for healthy plant growth;
3. All required parking lot landscape areas not otherwise planted with trees must contain a combination of shrubs and groundcover plants so that, within 2 years of planting, not less than 50 percent of that area is covered with living plants; and
4. Wheel stops, curbs, bollards or other physical barriers are required along the edges of all vehicle-maneuvering areas to protect landscaping from being damaged by vehicles. Trees shall be planted not less than 2 feet from any such barrier.
5. Trees planted in tree wells within sidewalks or other paved areas shall be installed with root barriers, consistent with applicable nursery standards.
6. The edges of parking lots shall be screened to minimize vehicle headlights shining into adjacent rights-of-way and residential yards. Parking lots abutting sidewalk or walkway shall be screened using a low-growing hedge or low garden wall to a height of between 3 feet and 4 feet.

7. The provisions of this subsection do not apply to areas for the storage and/or display of vehicles.

(Section 14.19.050(D.) amended by the adoption of Ordinance No. 2199 on August 15, 2022; effective September 14, 2022.)

14.19.060 Landscaping Requirements for Additions and Remodels

For purposes of this section, addition means any development that increases the floor area of a building. Remodel is any work requiring a building permit. For additions and remodels, landscaping shall be provided as follows:

- A. Area. If the subject development after completion complies with the requirements for new development, no additional landscaping is required. If the subject development does not comply with the requirement for new development, landscaping shall be installed so as follows:
 1. For projects with a value of \$80,000 or less, no additional landscaping is required.
 2. For projects with a value of \$80,001 to \$160,000, the amount of landscaping shall be no less than 25% of that required for new development.
 3. For projects with a value of \$160,001 to \$250,000, the amount of landscaping shall be no less than 50% of that required for new development.
 4. For projects with a value of \$250,001 to \$475,000, the amount of landscaping shall be no less than 75% of that required for new development.
 5. For projects with a value greater than \$475,000, the amount of landscaping shall be 100% of that required for new development.

Values shall be based on year 2022 dollars and adjusted on July 1 of each year for inflation. The adjustment shall be based on the latest available U.S. Department of Labor, Bureau of Labor Statistics, Consumer Price Index for All Urban Consumers (CPI-U).

For purposes of this section, the value shall be based on the amount placed on the application for a building permit. If the Building Official determines that the value is below the actual value as calculated by the formulas developed by the State of Oregon Building Codes Division, the value on the permit shall be as determined by the Building Official. If there is a dispute as to the value, the matter shall be referred to the Planning Commission for resolution. The procedure used shall be the same as for a Type I variance contained in [Section 14.33](#) of this Ordinance.

In the case where a second addition or remodel is commenced within one year of the first addition or remodel, the two projects shall be counted as one with regard to determining the above landscaping requirements.

B. Location. Landscaping shall be located along a street frontage or frontages.

1. For commercial and industrial zoned lots south of the Yaquina Bay Bridge that abut US 101, landscaping shall include a minimum 15-foot wide landscape buffer.

C. Exceptions. The right-of-way between a sidewalk and a property line may be used and counted toward the required landscaping as long as it has been determined by the Planning Director that the right-of-way is not needed for future street expansion. If the developer chooses to exercise this option, he or she shall enter into an agreement that the landscaping in the right-of-way is to be maintained as landscaping. In addition, window boxes may be substituted for surface landscaping. The calculation shall be one square foot of window box accounts for three square feet of surface landscaping as required in Subsection A of this Section. A developer may also plant a street tree within the sidewalk and it shall count toward meeting landscaping requirements subject to approval by the Planning Director and the City Engineer.

Development in the Historic Nye Beach Design Review District shall follow the same landscaping requirements as [Subsection 14.19.080](#) (City Center and Bayfront) of Section 14.19 if landscaping requirements are not specified elsewhere. If landscaping is required under a permit issued under the design review design guidelines or design standards, then the permit requirements shall be the applicable landscaping requirements. If the permit requirements specify landscaping requirements that are to be implemented in conjunction with, or in addition to, the landscape requirements of this section, then the landscaping requirements of the permit shall be implemented in conjunction with, or in addition to, the requirements of landscaping specified in [Subsection 14.19.080](#) (City Center and Bayfront) of Section 14.19.

** Amended by Ordinance No. 1865 (12-1-03).*

14.19.080 City Center and Bayfront

Because the City Center and Bayfront areas were platted and built on very small lots and many of the existing buildings are located on or near the property lines, a strict area landscaping requirement is difficult to obtain and places an undue burden on the property owner. Those areas shall therefore be subject to this section rather than [Sections 14.19.050](#) and [14.19.060](#) of this ordinance.

- A. New Development. The requirement for new development, defined as building on a vacant lot, shall be 10% of the lot area. In lieu of the 10%, hanging baskets or window/planter boxes may be substituted for surface landscaping, or any combination thereof. The calculation for square footage may be up to one square foot of hanging basket, planter box or window box for every three feet of otherwise required landscaping.
- B. Additions. Landscaping shall be required at a rate of 10% of the area of the addition. In lieu of the 10%, hanging baskets or window/planter boxes may be substituted for surface landscaping, or any combination thereof. The calculation for square footage may be up to one square foot of hanging basket, planter box or window box for every three feet of otherwise required landscaping.

- C. Remodels. Landscaping shall be required per [Section 14.19.060](#) except that in lieu of providing surface landscaping, window/planter boxes or hanging baskets may be substituted at a rate of one square foot of window/planter box or hanging basket for every ten square feet otherwise required.

14.19.090 Maintenance of Required Landscaping

Landscaping required by this section, whether existing prior to January 1, 1999 or not, shall be reasonably maintained based on the time of year and kept free of weeds and garbage. Failure to maintain required landscaping may be found to be a violation and subject to penalties contained in [Section 14.54](#) of this Code.

14.19.100 Variances

Variances to the requirements of this section shall be subject to the processes and criteria contained in [Section 14.33](#), Adjustments and Variances, and [Section 14.52](#), Procedural Requirements.* As a condition of approval, the Planning Commission may require a bond to assure satisfactory completion of the required landscaping. The Planning Commission may also approve, in lieu of providing a strict landscaping area, window or planter boxes in numbers and size to comply with the intent of this section or a reduction of up to 25% of the required landscaping when the Commission finds that the architectural character of the building is of such quality to justify the reduction. The Commission may also waive up to 25% of the area requirement if the developer puts in an automatic sprinkling system to water the landscaping. The required parking may be reduced up to 10% of the number ordinarily required by this Code if the parking spaces lost is put into landscaping. The site plan prepared by a registered surveyor as required by [Sections 14.33.040](#) is not required for a variance under this Section. If there is a neighborhood design review process, that process supersedes the requirements in this section and, if the design review committee finds that the landscaping is consistent with their review, supersedes the need for a variance otherwise required by this Section.

*(*Amended by Ordinance No. 1989 (1-1-10).)*

CHAPTER 14.30 DESIGN REVIEW STANDARDS

14.30.010 Purpose

Design review districts may be adopted by the City of Newport in accordance with applicable procedures to ensure the continued livability of the community by implementing standards of design for both areas of new development and areas of redevelopment. Design review is an important exercise of the power of the City to regulate for the general welfare by focusing on how the built environment shapes the character of the community.

The Newport Comprehensive Plan identifies six potential urban design districts within the Newport Peninsula including the City Center District (and Highway 101 corridor), Waterfront District, Nye Beach District, Upland Residential District, East Olive District, and the Oceanfront Lodging/Residential District. Additionally, neighborhood plans may be adopted for other areas of Newport that include as an objective the implementation of design review to maintain and/or provide a flexible approach to development by offering two methods of design review from which an applicant can choose. One method of design review is under clear and objective design standards and procedures to allow development that is consistent with the standards to occur with certainty in a timely and cost effective manner. A second alternative method of design review is review under design guidelines, which are a more flexible process for proposals that are creative/innovative and meet the identified guidelines of the applicable design review district.

It is further the purpose of these standards to:

- A. Preserve the beautiful natural setting and the orientation of development and public improvements in order to strengthen their relationship to that setting.

- B. Enhance new and redeveloping architectural and landscape resources to preserve and strengthen the historic, scenic and/or identified neighborhood character and function of each setting.
- C. Improve the vehicular and pedestrian networks in order to improve safety, efficiency, continuity, and relationships connecting Newport neighborhoods.
- D. Strengthen Newport's economic vitality by improving its desirability through improved appearance, function, and efficiency.
- E. Improve the built environment in order to strengthen the visual appearance and attractiveness of developed areas.
- F. Implement the goals and objectives of the adopted neighborhood plans.

14.30.020 Design Review Districts: Overlay Zones Established

The following:

- A. Historic Nye Beach Design Review District. The Historic Nye Beach Design Review District Overlay Zone shall be indicated on the Zoning Map of the City of Newport with the letters HNBO and is the area described as follows:

Beginning at the northeasterly corner of SW Hubert Street and SW 2nd Street; thence westerly along the north line of SW 2nd Street to the west line of SW Dolphin Street, said point also being the southeast corner of Lot 1, Block B, Barlow Blocks Addition to the City of Newport; thence north along the west line of SW Dolphin Street to 10 feet beyond the north line of Lot 7, said Barlow Blocks Addition; thence westerly, 10 feet north of and parallel with said north line of Lot 7 to the Pacific Ocean; thence northerly along the Pacific Ocean to the south line of NW 12th Street; thence east along the south line of NW 12th Street to the east line of an alley between NW Spring Street and NW Hubert Street; thence south along the east line of said alley way to

the north line of NW 10th Street; thence southwesterly to the southwest corner of the intersection of NW 10th Street and NW Brook Street; thence south along the west line of NW Brook Street to the south line of NW 8th Street; thence east along the south line of NW 8th Street to the west line of NW Hurbert Street; thence south along the west line of NW Hurbert Street to the north line of NW 6th Street; thence east to the northeast intersection of NW 6th Street and NW Hurbert Street; thence south along the east line of NW Hurbert Street and SW Hurbert Street to the north line of SW 2nd Street and the point of beginning.

B. City Center Design Review District. The City Center Design Review District Overlay Zone shall be indicated on the Zoning Map of the City of Newport with the letters CCDRD and is the area described as follows:

The City Center Design Review District is US Highway 101 and adjacent properties between the north end of the Yaquina Bay Bridge and US Highway 20, and US Highway 20 and adjacent properties between US Highway 101 and the eastern city limits.

14.30.030 Adoption of Design Review: Guidelines and Standards

The document entitled “Newport Design Review: Guidelines and Standards” dated July 29, 2015, is hereby adopted by reference and made a part hereof. The guidelines and standards contained therein shall be the guidelines and standards applicable to the Historic Nye Beach Design Review District.

The document entitled “City Center Design Review District Form Based Code ” dated [September 2025], is hereby adopted by reference and made a part hereof. The Regulating Plan, Public Realm Standards, and Development and Design Standards, contained therein shall be the standards applicable to the City Center Design Review District.

14.30.040 Design Review Required

The following development activities in an established design review district are required to obtain a design review permit under the design standards in an identified design review district or, in the alternative, to apply for a design review permit and to obtain approval under the design guidelines for that design review district:

- A. New construction, substantial improvement, or relocation of one or more dwelling units.
- B. New construction, substantial improvement, or relocation of a commercial or public/institutional building.
- C. New construction, substantial improvement, or relocation of a residential accessory structure that contains more than 200 square feet of gross floor area and is not more than 10 feet in height.
- D. New construction, substantial improvement, or relocation of a commercial accessory structure that contains more than 120 square feet of gross floor area.
- E. An addition that increases the footprint of an existing building by more than 1,000 square feet.

14.30.050 Exemptions

The following activities are exempt from the provisions of this chapter:

- A. Development activity that is subject to the provisions of Newport Municipal Code Chapter 14.23, Historic Buildings and Sites.
- B. Any project for improvement of a structure to comply with existing state or local health, sanitary, or safety code specifications that are solely necessary to assure safe living conditions.
- C. Development that does not involve the construction, substantial improvement, or relocation of a dwelling unit, commercial or public/institutional building, or accessory structure.

- D. Conversion of space within an existing structure into an accessory dwelling unit.

(Section 14.30.050 revised by Ordinance No. 2152, adopted on November 4, 2019; effective December 4, 2019.)

14.30.060 Approval Authority

The following are the initial review authorities for a Design Review application:

- A. Community Development Director. For projects subject only to the design standards specified in ~~the following documents entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015.~~ The approval or denial of a Design Review application by the Community Development Director is a ministerial action performed concurrent with City review of a building permit.
- a. Entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015. The approval or denial of a Design Review application by the Community Development Director is a ministerial action performed concurrent with City review of a building permit.
 - b. Entitled “City Center Design Review District Form Based Code.”
- B. Planning Commission. For projects that require design review under the design guidelines contained in the document entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015, including the following:
- 1. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure that is over 65 feet in length or 35 feet in height; or
 - 2. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure

that does not meet the design standards contained in the document entitled “Newport Design Review: Guidelines and Standards” dated July 29, 2015; or

3. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure that involves a conditional use, a variance, or any other type of land use permit for which a Type III Land Use Action decision process is required, pursuant to [Chapter 14.52](#), Procedural Requirements.

14.30.070 Application Submittal Requirements

- A. For requests that are subject to Community Development Director review for compliance with design standards, an application for Design Review shall consist of the following:
 1. A completed and signed City of Newport Building Permit Application Form.
 2. Building plans that conform to the submittal requirements for a building permit that include a site plan, floor plan, exterior architectural elevations, cross-section drawings, and construction specifications illustrating how the design standards have been met.
 3. A written checklist identifying the design elements used to comply with the design standards.
- B. For requests that are subject to Planning Commission review for compliance with design guidelines, an application for Design Review shall consist of the following:
 1. Submittal requirements for land use actions listed in [Section 14.52.050](#).
 2. Exterior elevations of all buildings on the site as they will appear after development. Such plans shall indicate the material, texture, shape, and other design features of the building(s), including all mechanical devices.

3. A parking and circulation plan illustrating all parking areas, drive isles, stalls, and points of ingress/egress to the site.
4. A landscape plan showing the location, type and variety, size and any other pertinent features of the proposed landscaping and plantings for projects that involve multi-family, commercial, and public/institutional development.

(Section 14.30.070(B)(4) was amended by Ordinance No. 2182, adopted on May 17, 2021: effective June 16, 2021.)

5. A lighting plan identifying the location and type of all permanent area lights, including parking area lighting, along with details of the lighting fixtures that are to be installed.
6. A written set of proposed findings that explain how the project complies with the applicable design guidelines.
7. Any other information the applicant believes is relevant to establishing that the project complies with applicable design guidelines.

C. All plans shall be drawn such that the dimensions can be verified with an engineers or architects scale.

14.30.080 Permitted Uses

In addition to uses permitted outright or conditionally in the underlying zoning district, the following uses are permitted within areas subject to design review.

A. Historic Nye Beach Design Review District.

1. Tourist Commercial (C-2) zoned property.
 - a. Up to five (5) multi-family dwelling units per lot or parcel are permitted outright provided they are located on a floor other than a floor at street grade.
 - b. A single-family residence is permitted outright if located on a floor other than a floor at street grade.

- c. A single-family residence is permitted outright, including the street grade floor, within a dwelling constructed prior to January 1, 2004. Residential use at the street grade is limited to the footprint of the structure as it existed on this date.
- d. Single family, duplex, townhouses, cottage clusters, and multi-family dwelling units, including at the street grade, are permitted outright on property located south of NW 2nd Court and north of NW 6th Street, except for properties situated along the west side of NW Cliff Street.

(Section 14.30.080(A)(1)(d) was amended by Ordinance No. 2182, adopted on May 17, 2021; effective June 16, 2021.)

(Section 14.30.080(A)(1)(d) was amended by Ordinance No. 2165, adopted on June 15, 2020; effective July 15, 2020.)

- a.
- 2. High Density Multi-Family Residential (R-4) zoned property.
 - a. Uses permitted outright in the C-2 zone district that are not specified as a use permitted outright or conditionally in the R-4 zone district, are allowed subject to the issuance of a conditional use permit in accordance with the provisions of Chapter 14.34, Conditional Uses and subject to the limitation that the use not exceed a total of 1,000 square feet of gross floor area. This provision does not preclude an application for a use as a home occupation under [Chapter 14.27](#), Home Occupations.

14.30.090 Prohibited Uses

The following uses are prohibited within areas subject to Design Review.

A. Historic Nye Beach Design Review District

- 1. Any new or expanded outright permitted commercial use in the C-2 zone district that exceeds 2,000 square feet of gross floor area. New or expanded uses in excess of 2,000 square feet of gross floor area may be permitted in accordance with the provisions of [Chapter 14.34](#),

Conditional Uses.

2. Recreational vehicle parks within the Tourist Commercial (C-2) and Public Structures (P-1) zoning districts.

(Section 14.30.090 revised by Ordinance No. 2120, adopted on September 18, 2017; effective October 18, 2017.)

14.30.100 Special Zoning Standards in Design Review Districts

All zoning standards and requirements applicable under Ordinance No. 1308 (as amended) in the subject zoning district shall apply, except that the following additional zoning standards are applicable for the design review district as applicable in the underlying zoning designation and shall be modified for each district as specified.

A. Historic Nye Beach Design Review District:

1. No drive through windows are allowed.
2. Commercial buildings with frontage on NW and SW Coast Street, W Olive Street, NW and SW Cliff Street, NW Beach Drive, and NW Third Street shall be set back from the property line fronting the street no more than 5 feet unless the development provides for a pedestrian oriented amenity (such as a courtyard, patio, or café with outdoor seating), compliance with the setback is precluded by topography or by easement, or a larger setback is authorized by the Planning Commission through the design review process.
3. Required yards and setbacks established in [Chapter 14.11](#) (Required Yards and Setbacks) and [Chapter 14.18](#) (Screening and Buffering between Residential and Non-Residential Zones) shall be reduced by 50%, except for [Section 14.11.030](#), Garage Setback, which is to remain at 20-feet. A setback for a garage that is less than 20-feet may be permitted if it is found by the Planning Commission to be consistent with the Design Review Guidelines pursuant to NMC [14.30.060\(B\)](#).

4. The following adjustments to [Chapter 14.12](#) (Minimum Size) and [Chapter 14.13](#) (Density Limitations, Table “A”) are allowed within the District.
 - a. The minimum lot area within both the R-4 and C-2 zones shall be 3,000 square feet.
 - b. The minimum lot width for the R-4 zone shall be 30 feet.
5. Residential use permitted on C-2 zoned property located south of NW 2nd Court and north of NW 6th Street, except for properties situated along the west side of NW Cliff Street, shall comply with the following additional requirements:
 - a. The maximum residential density is 1,250 square feet per unit.
 - b. The maximum building height is 35 feet.
 - c. The maximum lot coverage in structures is 64%. If the proposed residential use provides at least 1 off-street parking space for each dwelling unit in a below-grade parking structure (for the purposes of this section below-grade is defined to mean that 50% or more of the perimeter of the building is below-grade) located directly below the residential portion of the structure, the maximum lot coverage allowed is 90%.
 - d. The residential use provides at minimum 1 off-street parking space for each dwelling unit.
 - e. At least one residential building per lot is set back from the property line abutting the street no more than 5 feet.

(Section 14.30.100(A)(5) was amended by Ordinance No. 2165, adopted on June 15, 2020; effective July 15, 2020.)

6. The following adjustments to the off-street parking requirements of [Chapter 14.14](#) (Parking, Loading, and Access Requirements) are provided for uses within the District:

- a. Commercial uses shall have the first 1,000 square feet of gross floor area exempted from the off-street parking calculation.
- b. All uses within the District shall be allowed an on-street parking credit that shall reduce the required number of off-street parking spaces by one off-street parking space for every one on-street parking space abutting the property subject to the following limitations:
 - i. Each on-street parking space must be in compliance with the City of Newport standards for on-street parking spaces.
 - ii. Each on-street parking space to be credited must be completely abutting the subject property. Only whole spaces qualify for the on-street parking credit.
- a. On-street parking spaces credited for a specific use may not be used exclusively by that use, but shall be available for general public use at all times. No signs or actions limiting general public use of on-street parking spaces are allowed except as authorized by the City of Newport.

B. City Center Design Review District

- 1. No drive through windows are allowed within the area identifies as the Traditional Downtown Zone as identified in the City Center Design Review District Form Based Code.
- 2. Adjustments to Chapter 14.12 (Minimum Size) and Chapter 14.13 (Density Limitations, Table "A") are allowed within the City Center Design Review District as detailed in the City Center Design Review District Form Based Code, subsection 5, Development and Design Standards.

(Section 14.30.100 revised by Ordinance No. 2120, adopted on September 18, 2017; effective October 18, 2017.)




14.30.110 Modification of a Design Review Permit

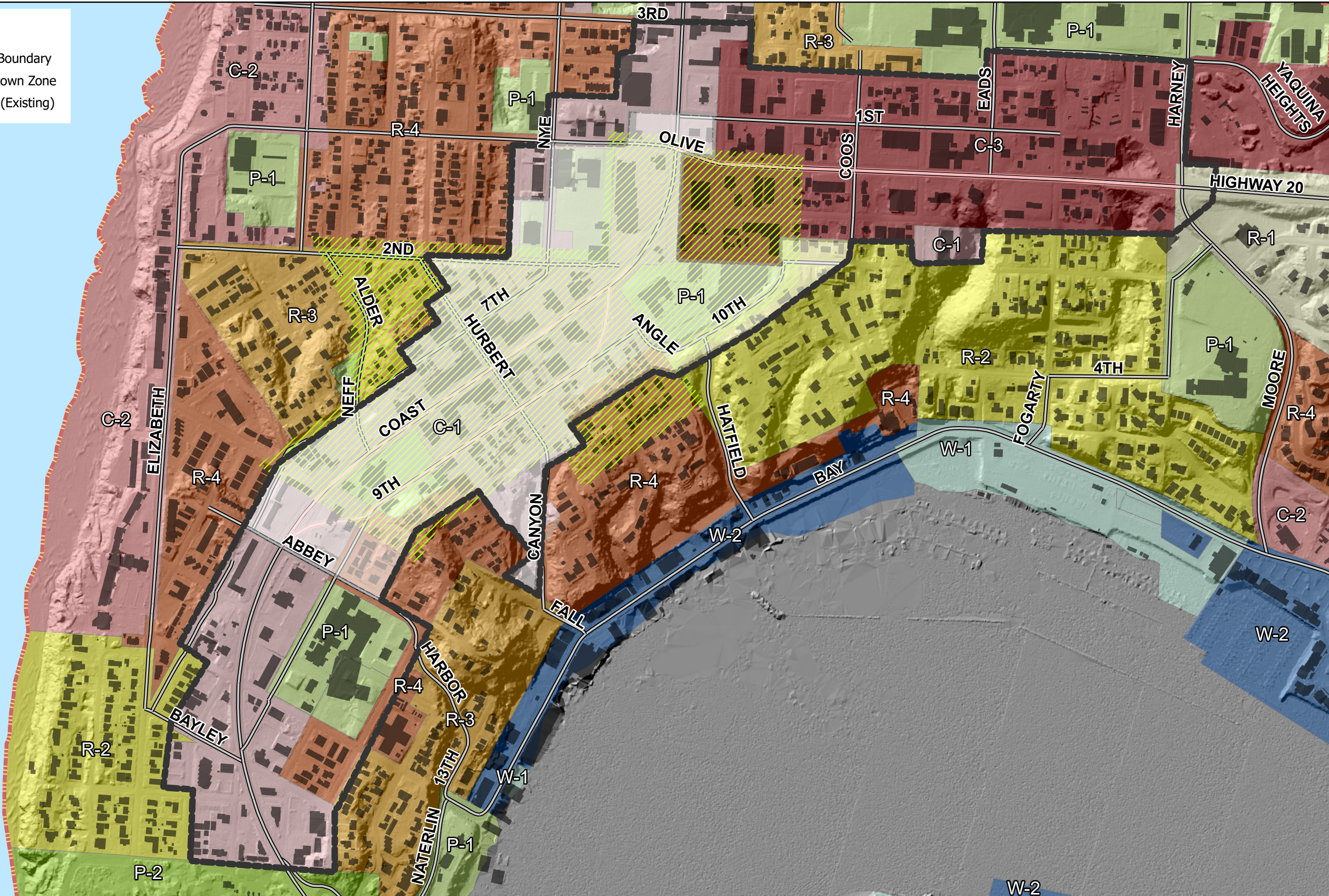
A modification of an approved design may be requested of the approving authority for any reason by an applicant. Applications for a modification shall be submitted and processed in the same manner as the original application.

- A. If the requested modification is from an approval issued under design standards, the modification request shall be approved by the Community Development Director if the modification also meets the design standards.
- B. If the modification does not meet the design standards or if the modification is from an approval issued under the design guidelines, the modification shall be processed under the design review process for compliance with the applicable design guidelines. The Commission's authority is limited to a determination of whether or not the proposed modification is consistent with the applicable design review guidelines.

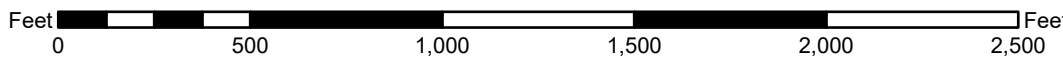
(Chapter 14.30 was revised by Ordinance No. 2084; adopted September 21, 2015; effective October 21, 2015.)

Legend

-  City Center Design District Boundary
-  Proposed Traditional Downtown Zone
-  City Center Parking District (Existing)



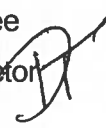
City Center Revitalization Plan Parking District Options



City of Newport

Community Development Department

Memorandum

To: Planning Commission/Commission Advisory Committee
 From: Derrick I. Tokos, AICP, Community Development Director 
 Date: August 6, 2025
 Re: South Beach Island Annexation Boundary Map and Proposed Zoning

On January 18, 2022, the Newport Urban Renewal Agency and Newport City Council approved Resolution No. 3943, adopting Minor Amendment XIV to the South Beach Urban Renewal Plan and Report. This amendment incorporated projects listed in the South Beach / US 101 Refinement Plan as the final round of urban renewal investments before the Plan closes to new projects at the end of 2025 and expires at the end of 2027. One of the projects involves the annexation of unincorporated property along the US 101 corridor that is surrounded by the Newport city limits. It is identified as project "B" in the final draft of the Refinement Plan (attached).

This will be what is commonly referred to as an island annexation, where the City can take in the properties with or without the consent of the affected property owners. As such, we will be following the process outlined in ORS Chapter 222, in lieu of the provisions in Chapter 14.37 of the Newport Municipal Code that apply to owner-initiated annexations (Ref: NMC 14.37.010). The City hired AKS Engineering & Forestry to prepare a boundary map and legal description of the area to be annexed. They also prepared an aerial map with the boundary.

This work session is an opportunity for Commission members to review, ask questions, or request revisions to the boundary map. Additionally, we have prepared maps showing the Comprehensive Plan designations that currently apply to the properties, and zoning that will apply once the properties are annexed. The zoning concept is a little different than what we last discussed in July of last year, with I-3/"Heavy Industrial" being applied to the batch plant site. We need confirmation that the proposed zoning boundaries are appropriate, so that AKS can prepare legal descriptions for each zone.

Our plan is to ask the City Council to initiate the annexation, by motion, at its August 18, 2025 meeting. The motion is also needed to apply urban zoning to the properties (NMC 14.36.020). We will then provide the required 35-day notice to the Department of Land Conservation and Development and are looking at October 13, 2025 as the date for a Planning Commission hearing. There are 46 impacted tax lots, and we have prepared a mailing list and will work with the Assessor to identify the anticipated tax impact to the affected properties. The City will also offer a rebate program for owners interested in connecting to the City sewer system. The Urban Renewal Agency and City will need to have the rebate program parameters in place by the end of the calendar year, and we will put together an advance draft for your review. Notice of the annexation will be mailed to impacted property owners in late September.

Attachments

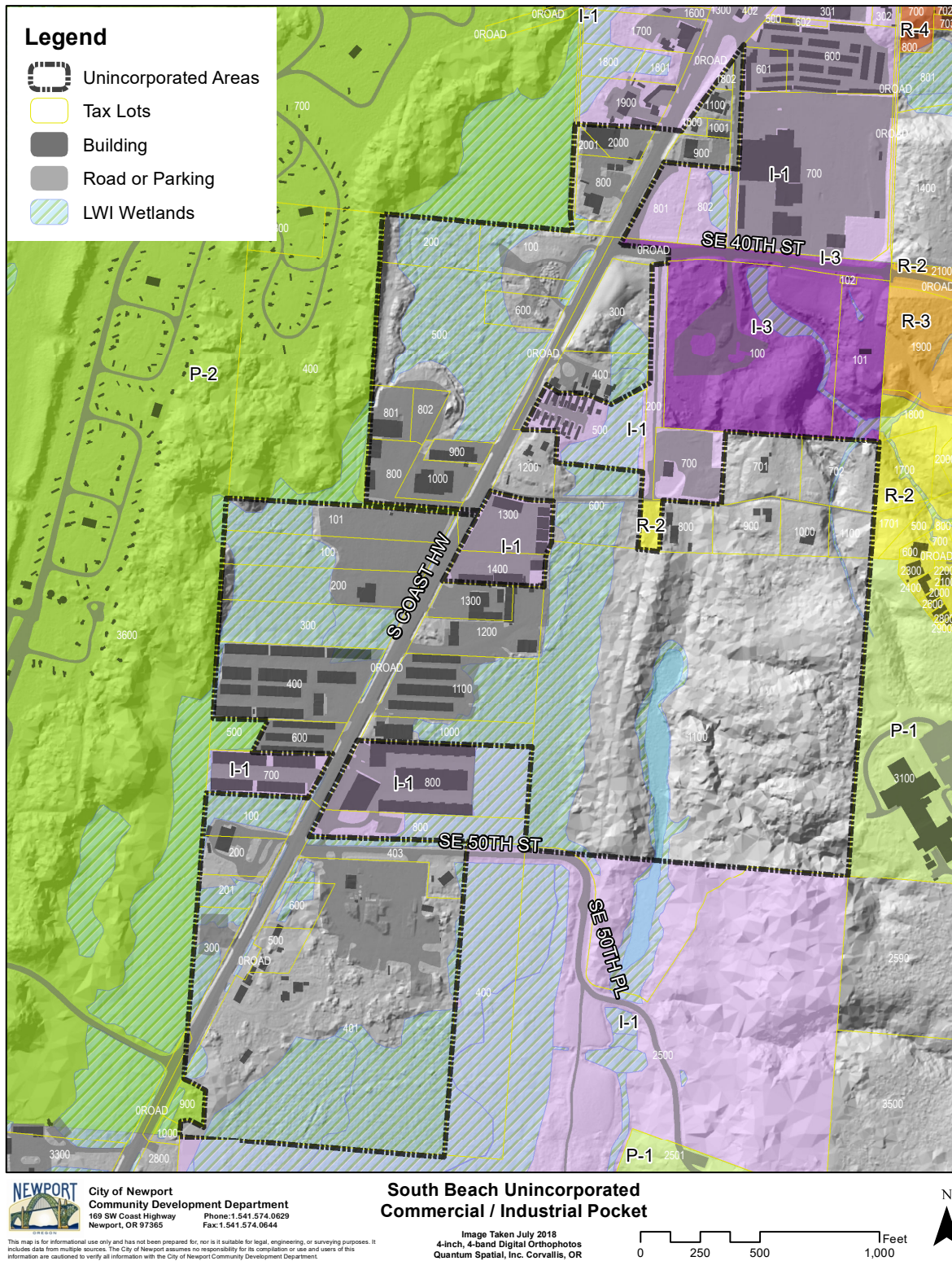
Project Sheets from South Beach Urban Renewal Plan, Draft Annexation Map, Draft Annexation map with Aerial, Draft Annexation Legal descriptions, Comprehensive Plan Designation Map, and Proposed Zoning Map

B Incentivize annexation of unincorporated properties with a focus on US 101 industrial sites, and target predevelopment assistance to vacant or underutilized sites

| Project Description | Lead |
|---|---|
| <p>Approximately 25% of land within the South Beach URA is outside of City of Newport limits, which makes it difficult for the City to provide key services to large parts of South Beach. Through a predevelopment fund, the City would provide an incentive to annex into the city for US 101 property owners that are outside of city limits but inside of the URA boundary. The City would prefer to discuss annexation potential with property owners to ensure it meets their goals. However, properties that are surrounded by the city can be annexed without consent using the island annexation provisions under ORS 222.750 and would be best served by annexation assistance (see Exhibit 13 for unincorporated properties).</p> <p>Key to this strategy would be working with those property owners to annex the unincorporated properties into the City all at once, which would reduce overall costs. The City could cover the annexation expenses, which includes survey costs and old bond debt that the Seal Rock Water District accrued when it provided service to these properties (the properties now receive City water service). The City could also provide incentives for these properties to connect to City sewer service by agreeing to (a) pay wastewater SDCs for existing uses that would otherwise be payable upon connection and (b) provide a rebate of up to \$10,000 to reimburse owners for the construction of new residential service laterals and \$15,000 for commercial/industrial service laterals.</p> <p>Beyond the annexation and SDC costs, the Agency could offer additional assistance to help those property owners overcome market or infrastructure development barriers related to site preparation/grading, removal of invasive species (Scotch Broom), and adding or improving utility and transportation infrastructure.</p> | Community Development, Planning Commission |
| Rationale | |
| <p>Annexing properties outside of city boundaries would:</p> <ul style="list-style-type: none"> ▪ Make it easier for the City to provide infrastructure and services to those properties. ▪ Help to increase the desirability of those properties for industrial development. ▪ Help the City to achieve its economic development goals by enabling development at urban intensities and limiting development under County regulations that may be less compatible with urban renewal development goals. ▪ Normalize the municipal boundaries for emergency service providers. ▪ Create a condition where urban scale development can occur, improving the tax base for all taxing districts. ▪ Prevent property owners from having to retire old Seal Rock Water District debt for services they no longer receive. | |
| Alignment with Vision 2040 | A14. Developable Land [Partner] C8. Local Businesses Support [Support] E6. Disaster Preparedness [Lead] |
| Implementation steps | <ul style="list-style-type: none"> ▪ Refine the list of potential properties for outreach. |

| | |
|--|---|
| | <ul style="list-style-type: none"> ▪ Discuss predevelopment needs and annexation potential with identified property owners. ▪ Engage with Lincoln County to coordinate review of any development within the UGB to ensure that it is consistent with City goals and standards upon future annexation. ▪ Determine which zones are desired upon annexation to provide greater certainty and transparency for landowners and developers. ▪ Explore options for “island annexation” under ORS 222.750. ▪ Conduct outreach with property owners. ▪ Before proceeding with annexation: <ul style="list-style-type: none"> ○ Calculate specific amount of annexation expenses the City will cover. ○ Calculate the specific incentives needed to connect the property to the City’s sewer. ○ Develop language for agreements and a plan to have all properties annexed at one time to reduce costs. ▪ Coordinate with Lincoln County to complete the Urban Growth Management Agreement to ensure an orderly transition from County to City zoning. |
| Public or stakeholder outreach needed | Outreach with property owners to is needed to determine development interest and whether the timing for that development would fall within the Agency’s investment window of 2022-2025. The City should engage with property owners to better understand specific concerns or uncertainties about annexation and encourage annexation. One approach could include developing informational resources for property owners highlighting development potential within the City compared to existing County regulations. In addition to tax and financial implications, these resources could outline any financial incentives for infrastructure development that could be available through the urban renewal area. |
| Partners | Conduct outreach with property owners to gauge interest in joining the City. |
| Estimated Cost | Funding Considerations |
| Up to \$500,000 (Preliminary estimate from the City of Newport; assumes all eligible properties are annexed at the same time) | Completing annexation of all willing properties at one time as opposed to piecemeal can reduce the overall costs associated with the annexation survey and staff time. |

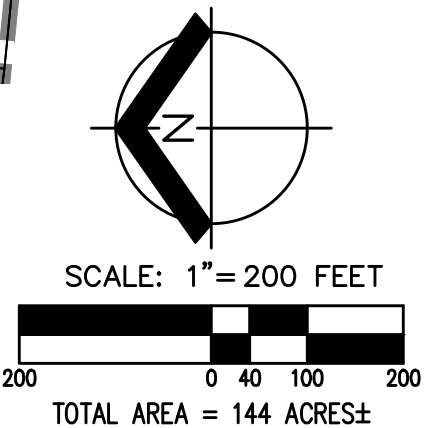
Exhibit 13. South Beach Unincorporated Areas within the Urban Renewal Boundary





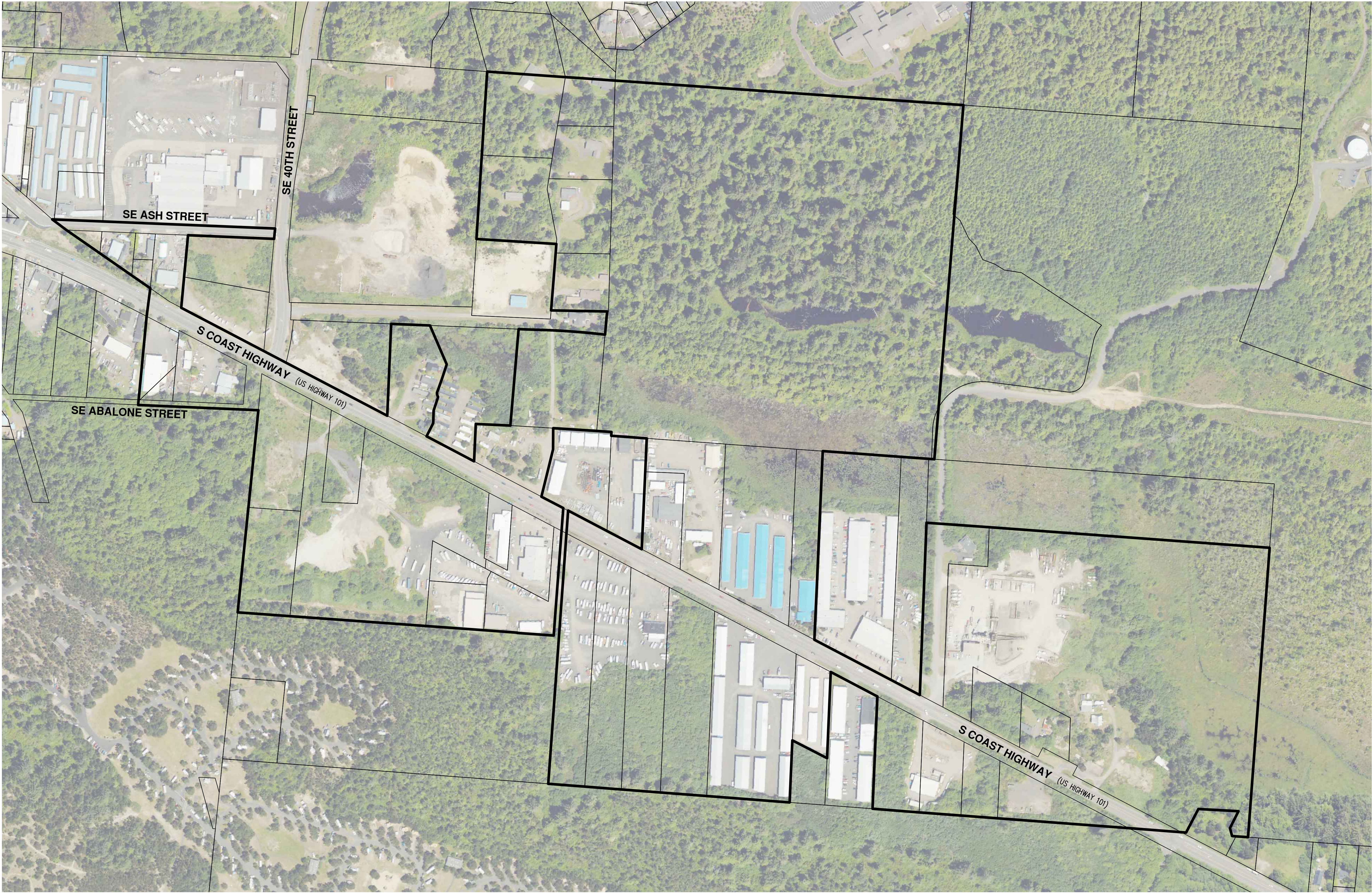
LEGEND

- CITY OF NEWPORT CITY LIMITS LINE
- 2025 ANNEXATION BOUNDARY LINE
- 2025 ANNEXATION AREA

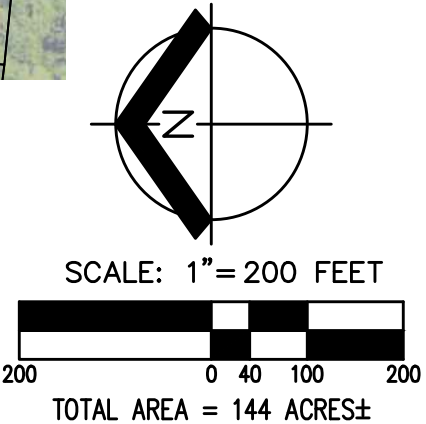


| | |
|---------------------------------------|---------|
| DESIGNED BY: | WCB |
| DRAWN BY: | LJM/NSW |
| MANAGED BY: | LJM/NSW |
| CHECKED BY: | LJM/NSW |
| DATE: 7/15/2025 | |
| REGISTERED PROFESSIONAL LAND SURVEYOR | |
| OREGON | |
| JANUARY 9, 2007 | |
| NICK WHITE | |
| 7065215 | |
| RENEWS: 6/30/26 | |
| REVISIONS | |
| JOB NUMBER | 12703 |
| SHEET | 1 |

CITY OF NEWPORT
2025 ANNEXATION
NEWPORT
LINCOLN COUNTY
OREGON



LEGEND
2025 ANNEXATION BOUNDARY LINE



**MAP OF ANNEXATION
AREA WITH AERIAL PHOTO**

**CITY OF NEWPORT
2025 ANNEXATION
NEWPORT OREGON**
LINCOLN COUNTY

AKS
AKS ENGINEERING & FORESTRY, LLC
12065 SW HERMAN RD, STE 100
TULALIN, OR 97062
503.563.6151
WWW.AKS-ENG.COM
ENGINEERING · SURVEYING · NATURAL RESOURCES
FORESTRY · PLANNING · LANDSCAPE ARCHITECTURE

| | |
|---|-----------|
| DESIGNED BY: | |
| DRAWN BY: | WCB |
| MANAGED BY: | LJM/NSW |
| CHECKED BY: | LJM/NSW |
| DATE: | 7/15/2025 |
| REGISTERED PROFESSIONAL LAND SURVEYOR | |
| <i>Nick White</i> | |
| OREGON JANUARY 9, 2007 NICK WHITE 7065215 RENEWS: 6/30/26 | |
| REVISIONS | |
| | |
| | |
| JOB NUMBER 12703 | |
| SHEET 2 | |

**EXHIBIT A**City of Newport
Annexation Area Description

A tract of land and road rights-of-way, located in the Southeast One-Quarter of Section 17, and in the Northeast and Northwest One-Quarters of Section 20, Township 11 South, Range 11 West, Willamette Meridian, Lincoln County, Oregon, and being more particularly described as follows:

Beginning at the north one-quarter corner of said Section 20, also being on the west right-of-way line of SE Abalone Street and the City of Newport city limits line (Assessor's Map 11.11.17DC);

1. Thence along said west right-of-way line and said city limits line, Northerly 459 feet, more or less, to the westerly extension of the south line of Instrument Number 2023-03623, Lincoln County Book of Records (Assessor's Map 11.11.17DC);
2. Thence leaving said city limits line along said westerly extension, Easterly 20 feet, more or less, to the south line of said deed and said city limits line (Assessor's Map 11.11.17DC);
3. Thence along said south line and said city limits line, Easterly 320 feet, more or less, to the westerly right-of-way line of S Coast Highway (US Highway 101) (Assessor's Map 11.11.17DC);
4. Thence leaving said westerly right-of-way line along said city limits line, Easterly 108 feet, more or less, to the north line of Number 2 of Parcel 2 of Instrument Number 2025-02704, Lincoln County Book of Records, also being on the easterly right-of-way line of S Coast Highway (US Highway 101) (Assessor's Map 11.11.17DC);
5. Thence along said easterly right-of-way line and said city limits line, Northeasterly 457 feet, more or less, to the east right-of-way line of SE Ash Street (Assessor's Map 11.11.17DC);
6. Thence along said east right-of-way line and said city limits line, Southerly 852 feet, more or less, to the north right-of-way line of SE 40th Street (Assessor's Map 11.11.17DC);
7. Thence along said north right-of-way line and said city limits line, Westerly 40 feet, more or less, to the west right-of-way line of SE Ash Street (Assessor's Map 11.11.17DC);
8. Thence along said west right-of-way line and said city limits line, Northerly 329 feet, more or less, to the north line of Partition Plat No. 2001-08, Lincoln County Survey Records (Assessor's Map 11.11.17DC);

9. Thence along said north line and said city limits line, Westerly 278 feet, more or less, to the easterly right-of-way line of said S Coast Highway (Assessor's Map 11.11.17DC);
10. Thence along said easterly right-of-way line and said city limits line, Southwesterly 888 feet, more or less, to the north line of Instrument Number 2024-07227, Lincoln County Book of Records (Assessor's Map 11.11.20AB);
11. Thence along said north line and said city limits line, Easterly 351 feet, more or less, to the east line of said deed (Assessor's Map 11.11.20AB);
12. Thence along said east line and said city limits line, Southerly 148 feet, more or less, to the southerly line of said deed (Assessor's Map 11.11.20AB);
13. Thence along said southerly line and said city limits line, Westerly 452 feet, more or less, to the easterly right-of-way line of said S Coast Highway (Assessor's Map 11.11.20AB);
14. Thence along said easterly right-of-way line and said city limits line, Southwesterly 205 feet, more or less, to the north line of Parcel 1 of Partition Plat No. 1990-11, Lincoln County Survey Records (Assessor's Map 11.11.20BA);
15. Thence along said north line and said city limits line, Easterly 140 feet, more or less, to the east line of said Parcel 1 (Assessor's Map 11.11.20BA);
16. Thence along said east line and said city limits line, Southerly 132 feet, more or less, to the north line of Instrument Number 2022-10255, Lincoln County Book of Records (Assessor's Map 11.11.20AB);
17. Thence along said north line and said city limits line, Easterly 369 feet, more or less, to the west line of Parcel II of Instrument Number 2017-08916, Lincoln County Book of Records (Assessor's Map 11.11.20AB);
18. Thence along said west line and said city limits line, Southerly 331 feet, more or less, to the south line of said Parcel II (Assessor's Map 11.11.20AB);
19. Thence along said south line and said city limits line, Easterly 89 feet, more or less, to the east line of said Parcel II (Assessor's Map 11.11.20AB);
20. Thence along said east line and said city limits line, Northerly 214 feet, more or less, to the south line of Parcel 1 of Partition Plat No. 2006-26, Lincoln County Survey Records (Assessor's Map 11.11.20AB);
21. Thence along said south line and said city limits line, Easterly 254 feet, more or less, to the east line of said Parcel 1 (Assessor's Map 11.11.20AB);
22. Thence along said east line and said city limits line, Northerly 306 feet, more or less, to the north line of said Partition Plat No. 2006-26 (Assessor's Map 11.11.20AB);

23. Thence along said north line and said city limits line, Easterly 641 feet, more or less, to the east line of the West One-Half of the Northeast One-Quarter of said Section 20 (Assessor's Map 11.11.20AB);
24. Thence along said east line and said city limits line, Southerly 1,823 feet, more or less, to the south line of the Northeast One-Quarter of said Section 20 (Assessor's Map 11.11.20);
25. Thence along said south line and said city limits line, Westerly 1,356 feet, more or less, to the west line of the Northeast One-Quarter of said Section 20 (Assessor's Map 11.11.20CA);
26. Thence along said west line and said city limits line, Northerly 427 feet, more or less, to the south line of Book 445, Page 272, Lincoln County Book of Records (Assessor's Map 11.11.20BD);
27. Thence along said south line and said city limits line, Westerly 721 feet, more or less, to the easterly right-of-way line of said S Coast Highway (Assessor's Map 11.11.20BD);
28. Thence along said easterly right-of-way line and said city limits line, Southwesterly 454 feet, more or less, to the north line of Parcel 1 of Partition Plat No. 2002-03, Lincoln County Survey Records (Assessor's Map 11.11.20CA);
29. Thence along said north line and said city limits line, Easterly 658 feet, more or less, to the east line of said Partition Plat No. 2002-03 (Assessor's Map 11.11.20CA);
30. Thence along said east line and said city limits line, Southerly 1,313 feet, more or less, to the south line of the Northeast One-Quarter of the Southwest One-Quarter of said Section 20 (Assessor's Map 11.11.20CA);
31. Thence along said south line and said city limits line, Westerly 1,108 feet, more or less, to the west line of the Northeast One-Quarter of the Southwest One-Quarter of said Section 20 (Assessor's Map 11.11.20CA);
32. Thence along said west line and said city limits line, Northerly 61 feet, more or less, to the southerly line of Instrument Number 2021-11969, Lincoln County Book of Records (Assessor's Map 11.11.20CA);
33. Thence along said southerly line and said city limits line, Easterly 103 feet, more or less, to the east line of said deed (Assessor's Map 11.11.20CA);
34. Thence along said east line and said city limits line, Northerly 153 feet, more or less, to the northerly line of said deed (Assessor's Map 11.11.20CA);

35. Thence along said northerly line and said city limits line, Northwesterly 102 feet, more or less, to the west line of the Northeast One-Quarter of the Southwest One-Quarter of said Section 20 (Assessor's Map 11.11.20CA);
36. Thence along said west line and the west line of Southeast One-Quarter of the Northwest One-Quarter of said Section 20 and said city limits line, Northerly 1,200 feet, more or less, to the south line of Parcel 2 of Book 231, Page 1164, Lincoln County Book of Records (Assessor's Map 11.11.20BD);
37. Thence along said south line and said city limits line, Easterly 429 feet, more or less, to the westerly right-of-way line of said S Coast Highway (Assessor's Map 11.11.20BD);
38. Thence along said westerly right-of-way line and said city limits line, Northeasterly 201 feet, more or less, to the north line of said deed (Assessor's Map 11.11.20BD);
39. Thence along said north line and said city limits line, Westerly 329 feet, more or less, to the easterly line of Book 206, Page 131, Lincoln County Book of Records (Assessor's Map 11.11.20BD);
40. Thence along said easterly line and said city limits line, Northeasterly 149 feet, more or less, to the north line of said deed (Assessor's Map 11.11.20BD);
41. Thence along said north line and said city limits line, Westerly 237 feet, more or less, to west line of Southeast One-Quarter of the Northwest One-Quarter of said Section 20 (Assessor's Map 11.11.20BD);
42. Thence along said west line and said city limits line, Northerly 924 feet, more or less, to the south line of Book 321, Page 1558, Lincoln County Book of Records (Assessor's Map 11.11.20BA);
43. Thence along said south line and the easterly extension thereof and said city limits line, Easterly 1,046 feet, more or less, to the easterly right-of-way line of said S Coast Highway (Assessor's Map 11.11.20BA);
44. Thence along said easterly right-of-way line and said city limits line, Southwesterly 318 feet, more or less, to the south line of Book 397, Page 2235, Lincoln County Book of Records (Assessor's Map 11.11.20BA);
45. Thence along said south line and said city limits line, Easterly 424 feet, more or less, to the east line of said deed (Assessor's Map 11.11.20BA);
46. Thence along said east line and said city limits line, Northerly 136 feet, more or less, to the south line of Parcel 2 of said Partition Plat No. 1990-11 (Assessor's Map 11.11.20BA);

47. Thence along said south line and said city limits line, Easterly 15 feet, more or less, to the east line of said Parcel 2 (Assessor's Map 11.11.20BA);
48. Thence along said east line and said city limits line, Northerly 223 feet, more or less, to the northerly line of said Parcel 2 (Assessor's Map 11.11.20BA);
49. Thence along said northerly line and said city limits line, Westerly 267 feet, more or less, to the easterly right-of-way line of said S Coast Highway (Assessor's Map 11.11.20BA);
50. Thence along said easterly right-of-way line and said city limits line, Southwesterly 94 feet, more or less, to the easterly extension of the south line of Parcel 3 of Partition Plat No. 2016-09, Lincoln County Survey Records (Assessor's Map 11.11.20BA);
51. Thence along said easterly extension and the south line of said Parcel 3 and said city limits line, Westerly 488 feet, more or less, to the west line of said Partiton Plat No. 2016-09, also being on the easterly line of said Book 321, Page 1558 (Assessor's Map 11.11.20BA);
52. Thence along said easterly line and said city limits line, Northerly 1,210 feet, more or less, to the north line of said Section 20 (Assessor's Map 11.11.20BA);
53. Thence along said north line and said city limits line, Easterly 776 feet, more or less, to the Point of Beginning.

The above described tract of land contains 144 acres, more or less.


7/15/2025



Legend

 Unincorporated Areas

 Building

 Road or Parking

Comprehensive Plan Designation


 Commercial

 Industrial

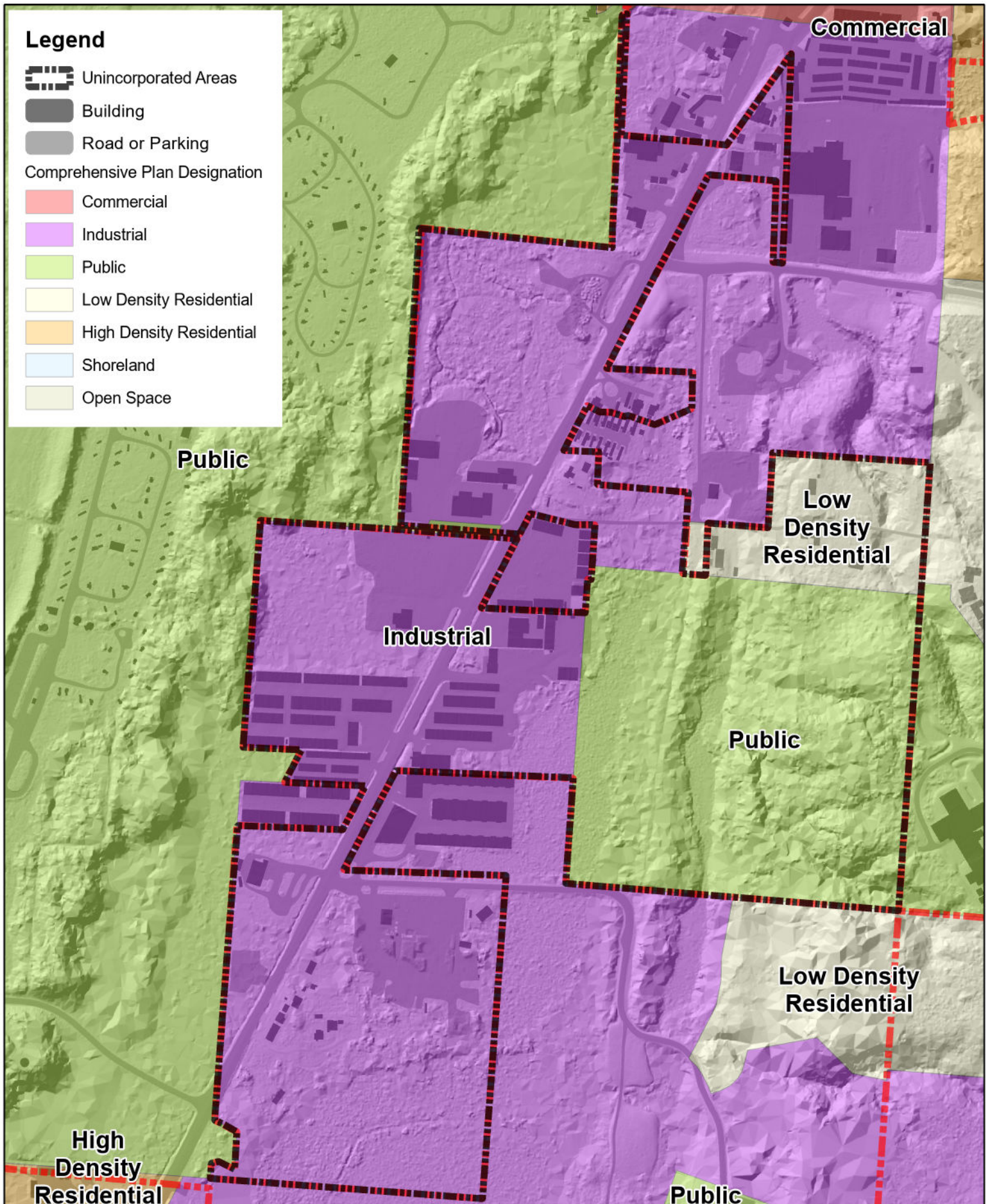
 Public

 Low Density Residential

 High Density Residential

 Shoreland

 Open Space



City of Newport
Community Development Department
169 SW Coast Highway
Newport, OR 97365
Phone: 1.541.574.0629
Fax: 1.541.574.0644

Comprehensive Plan Designations South Beach Unincorporated Pocket


Image Taken July 2018
4-inch, 4-band Digital Orthophotos
Quantum Spatial, Inc. Corvallis, OR

0 250 500 1,000 Feet


N
166

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Legend


 Unincorporated Areas


 Building


 Road or Parking


Zone


 C-1 Retail and Service

 C-2 Tourist


 C-3 Heavy


 I-1 Light

 I-2 Medium

 I-3 Heavy

 P-1 Public Structures

 P-2 Public Parks


 P-3 Public Open Space


 R-1 Low Density Single-Family

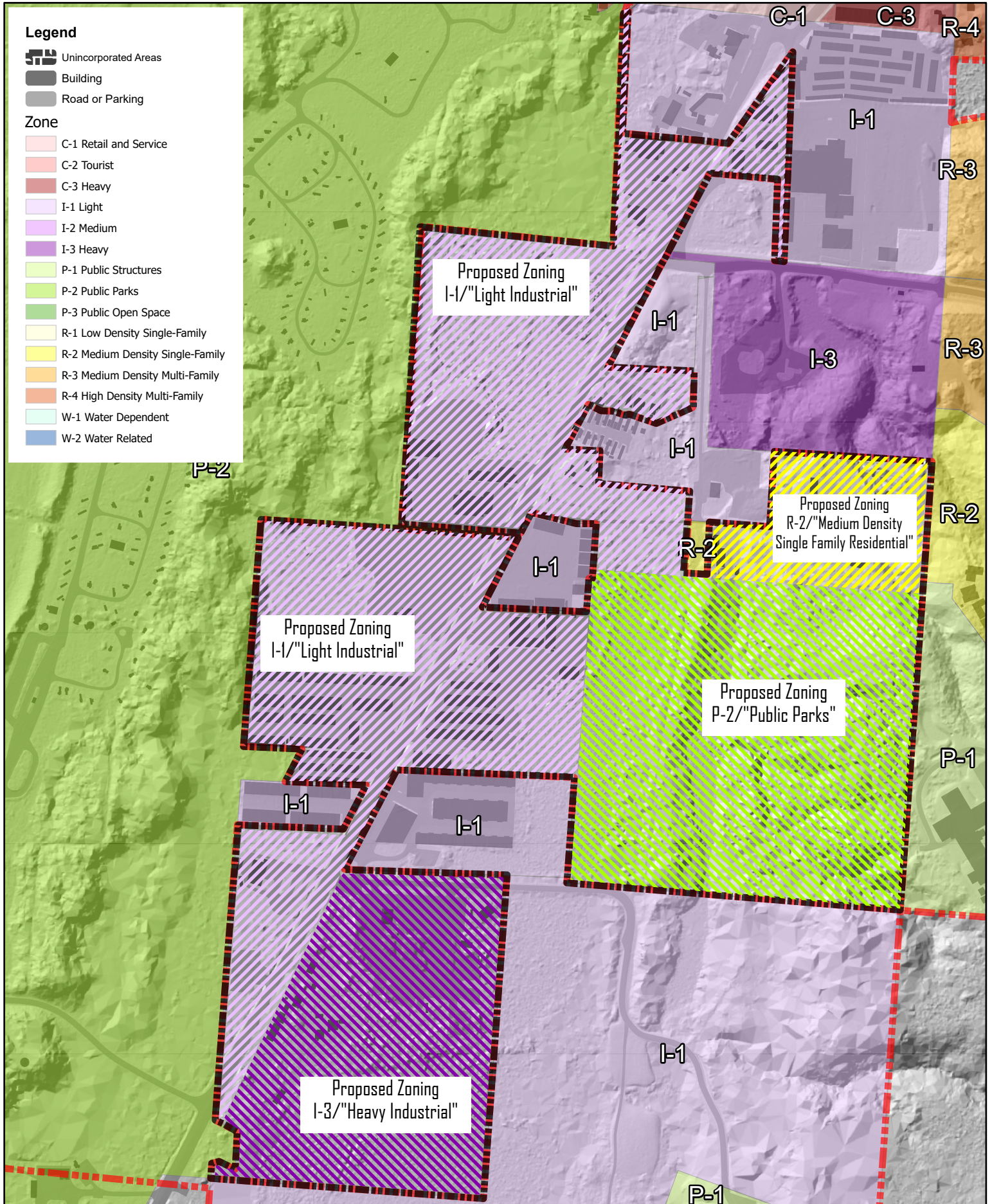
 R-2 Medium Density Single-Family

 R-3 Medium Density Multi-Family

 R-4 High Density Multi-Family

 W-1 Water Dependent

 W-2 Water Related



City of Newport
Community Development Department
169 SW Coast Highway
Newport, OR 97365
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Comprehensive Plan Designations South Beach Unincorporated Pocket

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N
167

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Tentative Planning Commission Work Program

(Scheduling and timing of agenda items is subject to change)



June 23, 2025

Work Session

- Update on the Comprehensive Plan streamlining project
- Review draft City Center Revitalization Comprehensive Plan amendments

June 23, 2025

Regular Session

- Initiate legislative process for the City Center Revitalization Comprehensive Plan amendments

July 14, 2025

Work Session

- Review draft Zoning Ordinance housekeeping amendments
- Review draft Wastewater Treatment Plant Comprehensive Plan policy amendments

July 14, 2025

Regular Session

- Initiate legislative process for draft Zoning Ordinance housekeeping amendments
- Initiate legislative process to amend the wastewater section of the Comprehensive Plan

July 28, 2025

Work Session

- Review City Center Revitalization Plan form-based zoning amendments (Urbworks to attend)

July 28, 2025

Regular Session

- Initiate legislative process for City Center Revitalization Plan form-based zoning amendments
- Public hearing on City Center Revitalization Comprehensive Plan Amendments

August 11, 2025

Work Session

- Second review of the City Center Revitalization Plan form-based zoning amendments
- South Beach Island Annexation boundary and proposed zoning

August 25, 2025

Work Session

- Review City Center Revitalization Plan design guidelines
- Overview of CDD updated website (and associated resources)

August 25, 2025

Regular Session

- Public hearing to amend the wastewater section of the Comprehensive Plan
- Public hearing on draft zoning ordinance housekeeping amendments

September 8, 2025

Cancelled

September 22, 2025

Work Session

- Placeholder Review DOGAMI coastal dune hazard map updates and potential code changes

September 22, 2025

Regular Session

- Placeholder for Toyota of Newport amended Conditional Use Permit

October 13, 2025

Regular Session

- Public hearing on City Center Revitalization Plan form-based zoning amendments
- Public hearing on South Beach Island Annexation