

# STREET AND TRANSPORTATION SYSTEMS

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NEIGHBORHOOD COLLECTOR STREETS
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## STANDARD DETAIL DRAWING INDEX

DETAIL NO.

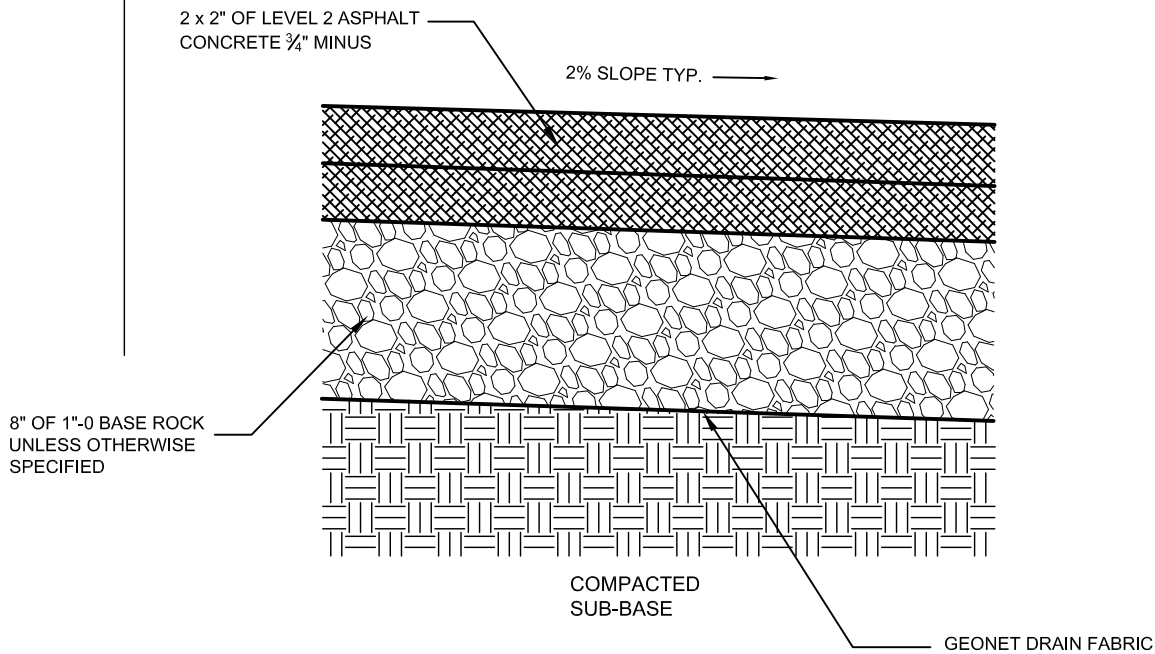
T-010

11/9/23

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CURB

# 4" AC PAVEMENT WITH 8" AGGREGATE BASE



**NOTES:**

1. ASPHALT CONCRETE PAVEMENT SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00744
2. SEE ODOT STANDARD DRAWING RD610 FOR SAW CUT SPECIFICATIONS
3. NEW ASPHALT CONCRETE SHALL NOT HAVE GREATER THAN A ¼" VERTICAL TRANSITION WHEN MEETING EXISTING ASPHALT CONCRETE
4. EARTHWORK AND SUBGRADE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00330
5. AGGREGATE BASE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00641
6. ALL ACP AND JOINTS SHALL BE SANDED AND SEALED WITH TACK.
7. GEONET DRAIN FABRIC SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00350



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TYPICAL STREET  
SECTION: LOCAL, YIELD,  
AND SHARED STREETS

DETAIL NO.

T-050

2/12/2024



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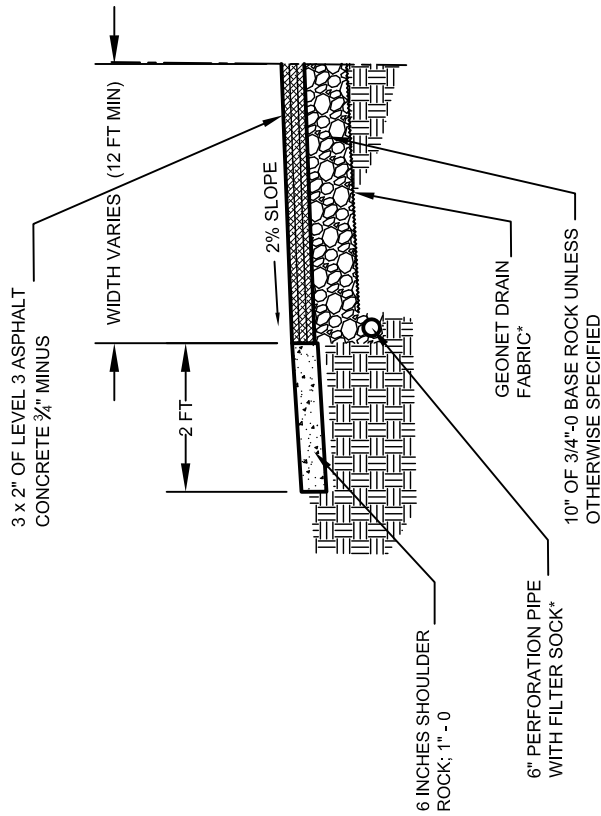
# STANDARD STREET SECTION: MAJOR COLLECTOR AND NEIGHBORHOOD COLLECTOR STREETS

DETAIL NO.

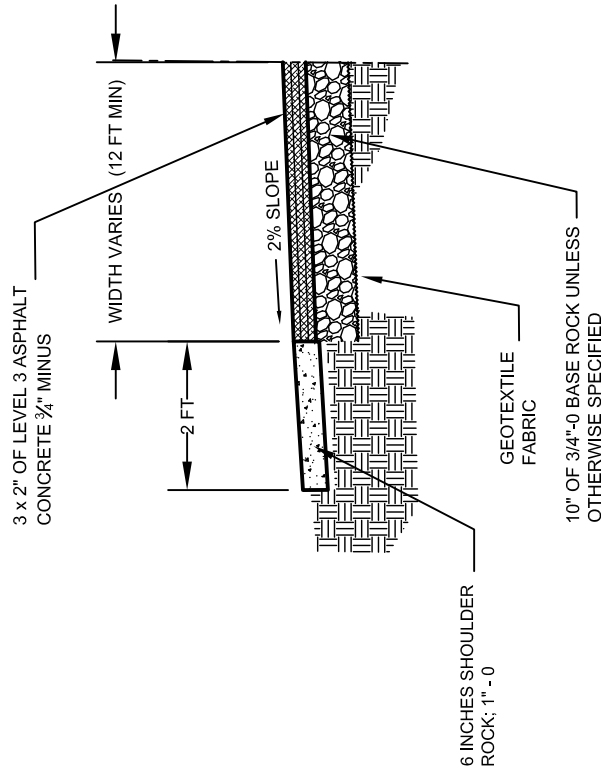
T-050A

2/12/2024

## 6" AC PAVEMENT WITH 10" AGGREGATE BASE



### FULL ROAD SECTION REPLACEMENT\*



### FULL ROAD SECTION REPLACEMENT\*\* , \*\*\* , \*\*\*\* , \*\*\*\*\*

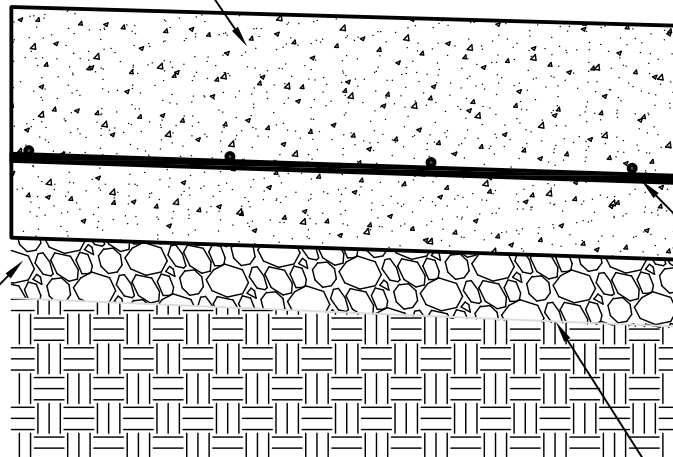
- NOTES:
1. ASPHALT CONCRETE PAVEMENT SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00744
  2. SEE ODOT STANDARD DRAWING RD610 FOR SAW CUT SPECIFICATIONS
  3. NEW ASPHALT CONCRETE SHALL NOT HAVE GREATER THAN A 1/4" VERTICAL TRANSITION WHEN MEETING EXISTING ASPHALT CONCRETE
  4. GEOTEXTILE FABRIC SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 02320
  5. EARTHWORK AND SUBGRADE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00330
  6. AGGREGATE BASE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00641
  7. ALL ACP AND JOINTS SHALL BE SANDED AND SEALED WITH TACK.
  8. GEONET DRAIN FABRIC SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00350

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CURB

8" CONCRETE

2% SLOPE TYP. →



#4 REBAR 12" OC EW

4" OF 1"-0 BASE ROCK  
UNLESS OTHERWISE  
SPECIFIED

COMPACTED  
SUB-BASE

GEONET DRAIN FABRIC

**NOTES:**

1. CONCRETE PAVEMENT SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00756
2. SEE ODOT STANDARD DRAWING RD610 FOR SAW CUT SPECIFICATIONS
3. EARTHWORK AND SUBGRADE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00330
4. AGGREGATE BASE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00641
5. GEONET DRAIN FABRIC SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (OSS), CURRENT EDITION, SECTION 00350



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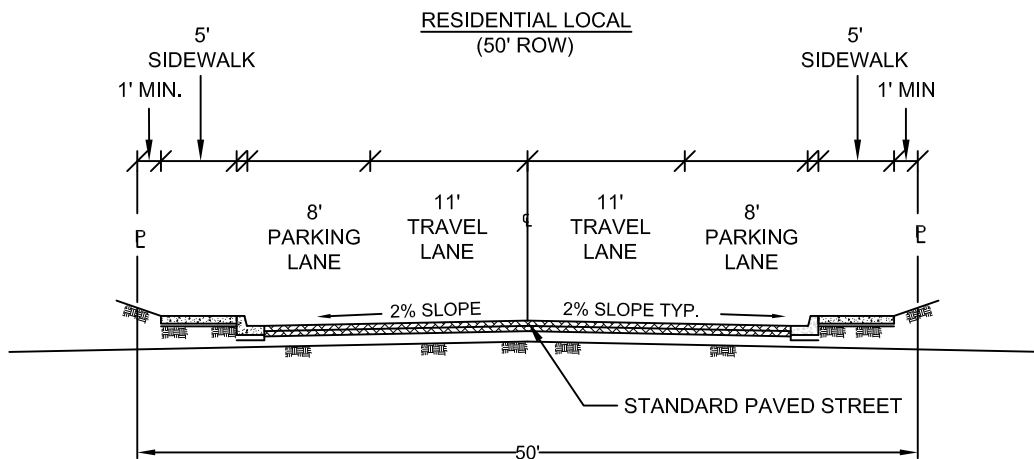
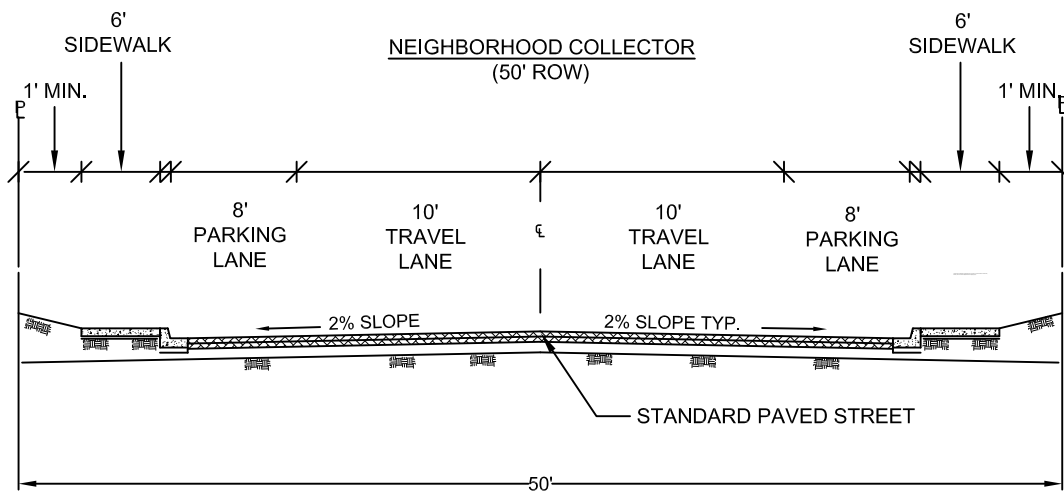
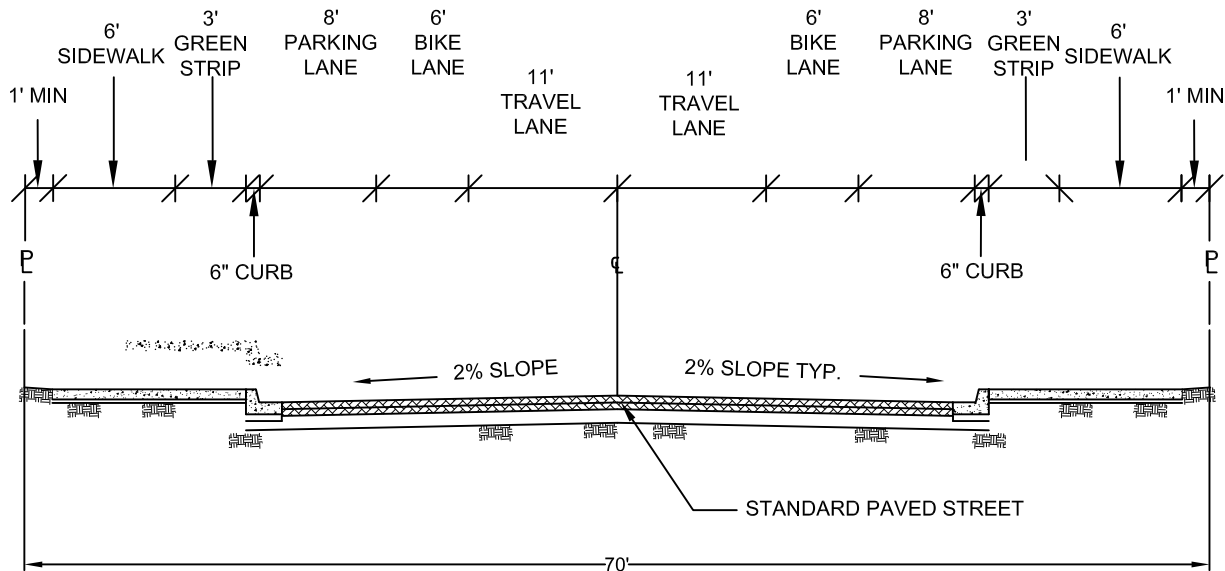
**TYPICAL CONCRETE SECTION**

DETAIL NO.

**T-050B**

2/12/2024

MAJOR COLLECTOR (70' ROW)  
 REFER TO THE 2022 TRANSPORTATION  
 SYSTEM PLAN FOR POTENTIAL ALTERNATIVES



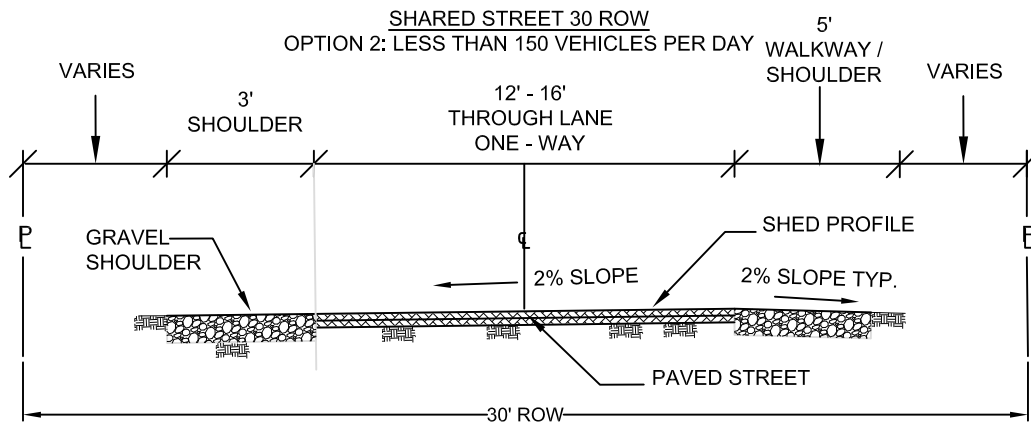
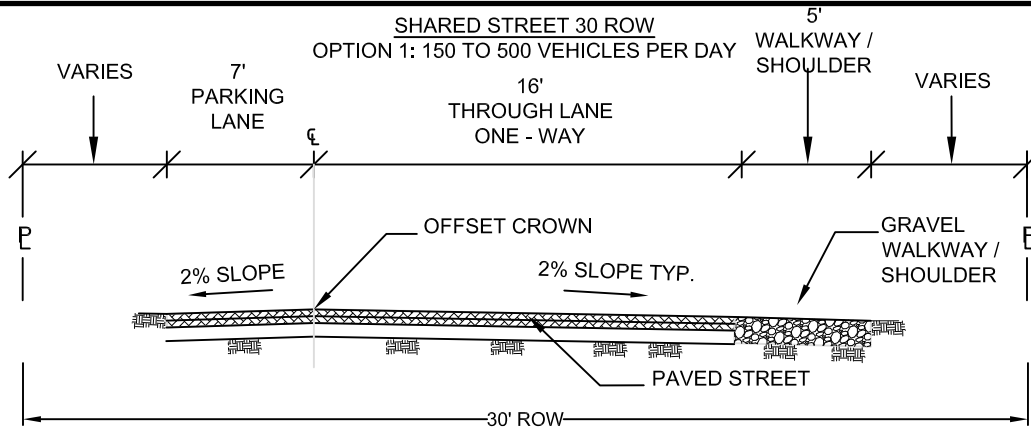
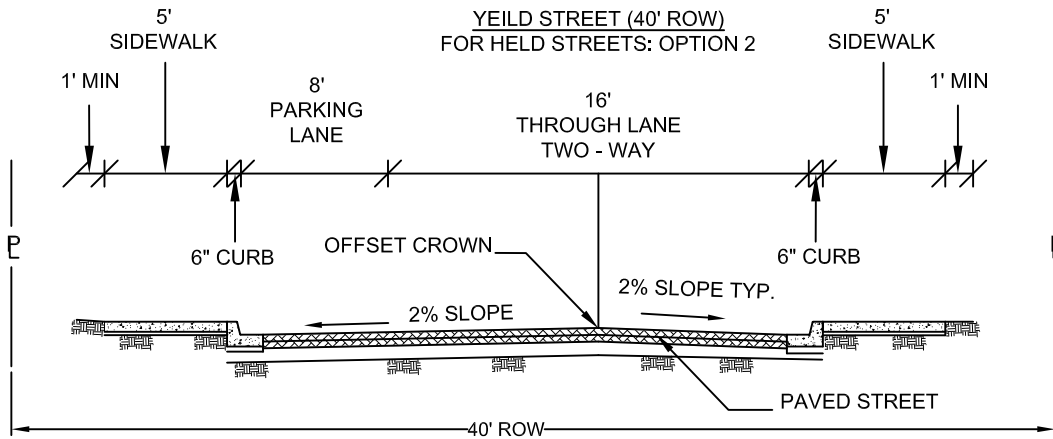
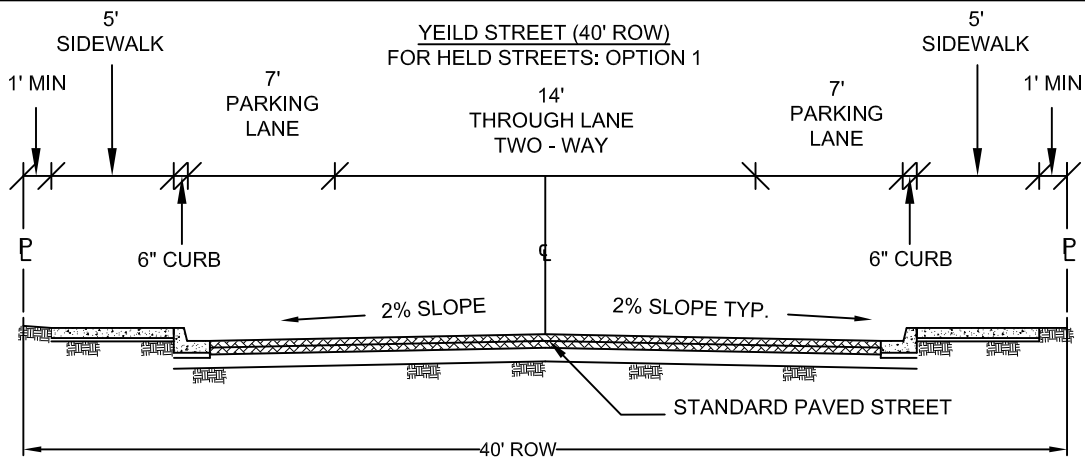
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TYPICAL ROADWAY LAYOUTS:  
 MAJOR COLLECTORS, NEIGHBORHOOD  
 COLLECTORS, LOCAL STREETS

DETAIL NO.

T-051

11/9/2023



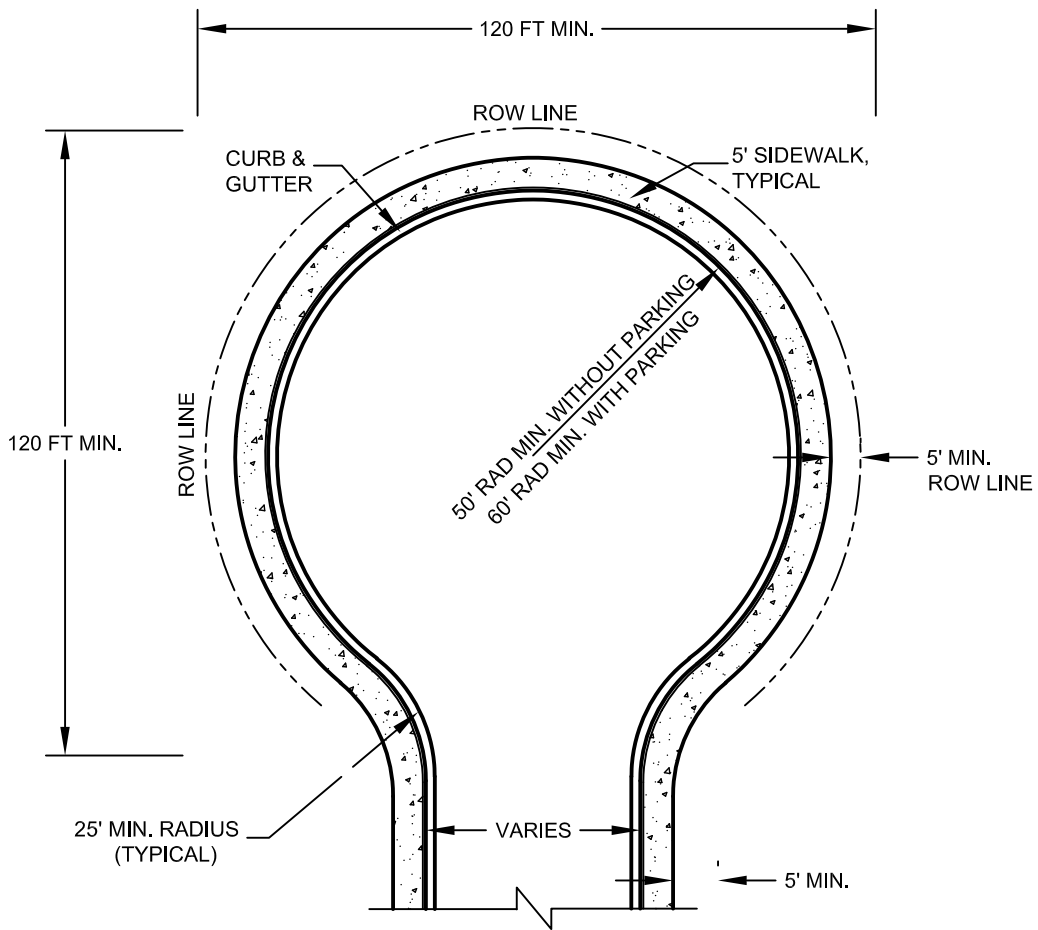
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**TYPICAL ROADWAY LAYOUTS:  
YIELD AND SHARED STREETS**

DETAIL NO.

T-051A

11/9/2023



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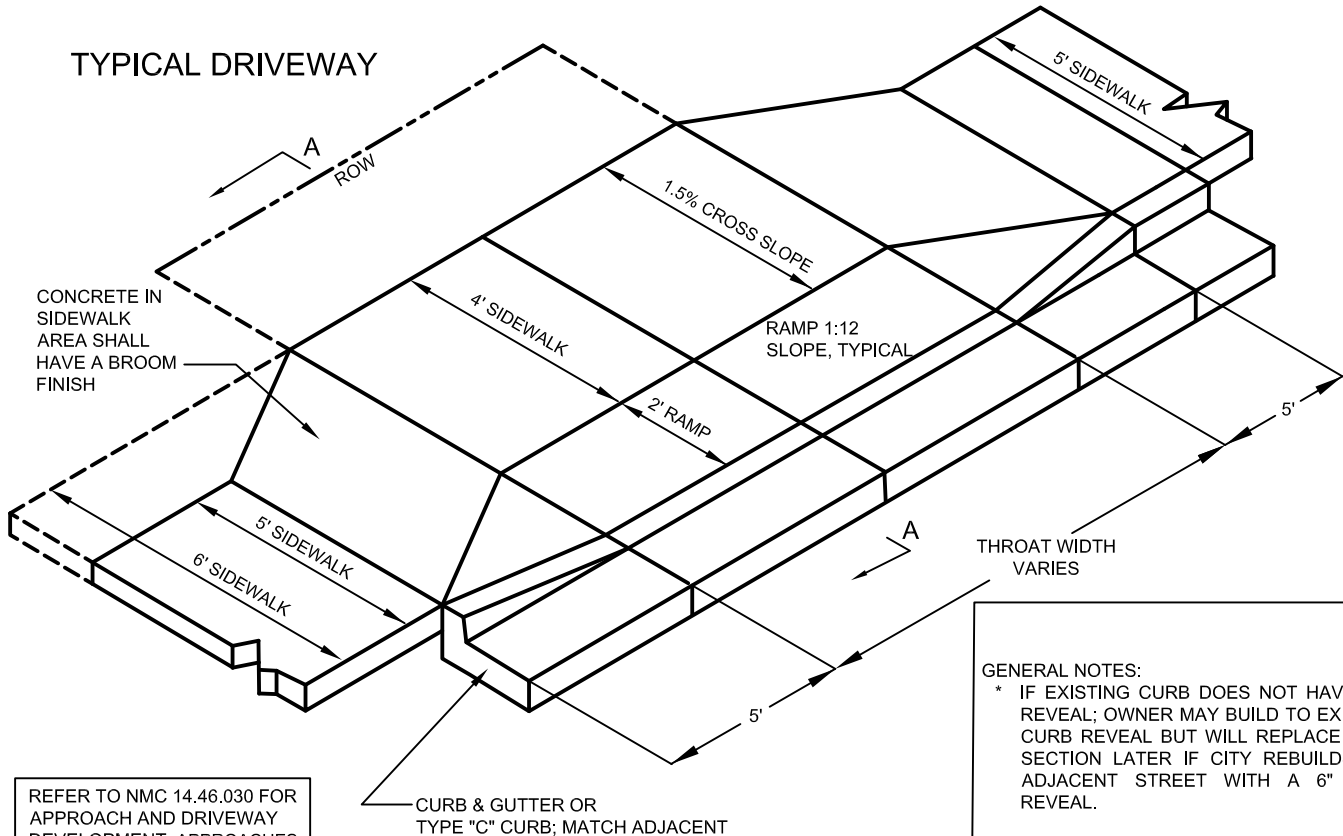
TYPICAL CUL-DE-SAC DETAIL

DETAIL NO.

T-053

5/7/2024

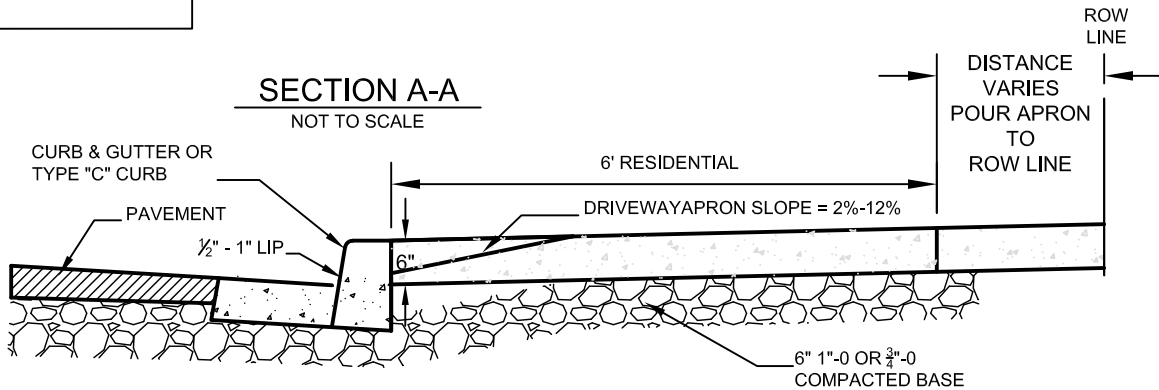
# TYPICAL DRIVEWAY



REFER TO NMC 14.46.030 FOR APPROACH AND DRIVEWAY DEVELOPMENT. APPROACHES AND DRIVEWAYS SHALL BE A MINIMUM OF TWELVE (12) FEET FOR A ONE-WAY DRIVEWAY Y AND NO GREATER THAN EIGHTEEN (18) FEET MAXIMUM WIDTH.

**GENERAL NOTES:**  
 \* IF EXISTING CURB DOES NOT HAVE A 6" REVEAL; OWNER MAY BUILD TO EXISTING CURB REVEAL BUT WILL REPLACE RAMP SECTION LATER IF CITY REBUILDS THE ADJACENT STREET WITH A 6" CURB REVEAL.  
 \*\* REGARDLESS OF CURB REVEAL; DEPTH OF CURB REMAINS THE SAME AS THOUGH THERE WERE A 6" REVEAL. SEE CURB STANDARD DETAILS.

## SECTION A-A NOT TO SCALE



**NOTES**

1. A MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. DRIVEWAY STRUCTURE SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (CURRENT EDITION) SECTION 00759.
3. ADA CROSS-SLOPE COMPLIANT CONCRETE APRON REQUIRED FOR ALL DRIVEWAYS EVEN WHERE NO SIDEWALK IS CONSTRUCTED ADJACENT TO DRIVEWAY.
4. CURB SHALL BE FORMED & POURED SEPARATE FROM DRIVEWAY; MONOLITHIC POURS NOT ALLOWED.
5. A RIGHT-OF-WAY PERMIT IS REQUIRED ON NEW DRIVEWAYS AND DRIVEWAY MODIFICATIONS. SCHEDULE ONSITE MEETING WITH CITY
6. REPRESENTATIVE AFTER FORMS ARE SET BUT BEFORE POURING CONCRETE; ALLOW ADEQUATE TIME FOR FORM ADJUSTMENT BEFORE SCHEDULED POUR.
7. FOR SIDEWALKS REFER TO STANDARD SIDEWALK DETAIL T-210.
8. FOR CURBS REFER TO STANDARD CURB & GUTTER DETAIL T-301.
9. DROP PANEL DRIVEWAYS MAY BE USED WHEN GROUND AT BACK OF WALK SLOPES DOWN TO A HOUSE OR DRIVEWAY LOWER THAN THE SIDEWALK. SEE DETAIL T-151.
10. FOR COMMERCIAL DRIVEWAYS SEE T-152 FOR REINFORCEMENT REQUIREMENTS.
11. CONCRETE TO EXTEND STRAIGHT BACK FROM DRIVEWAY THROUGH ROW TO PROTECT WALKWAY.



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# STANDARD APPROACH FOR 5' SIDEWALK

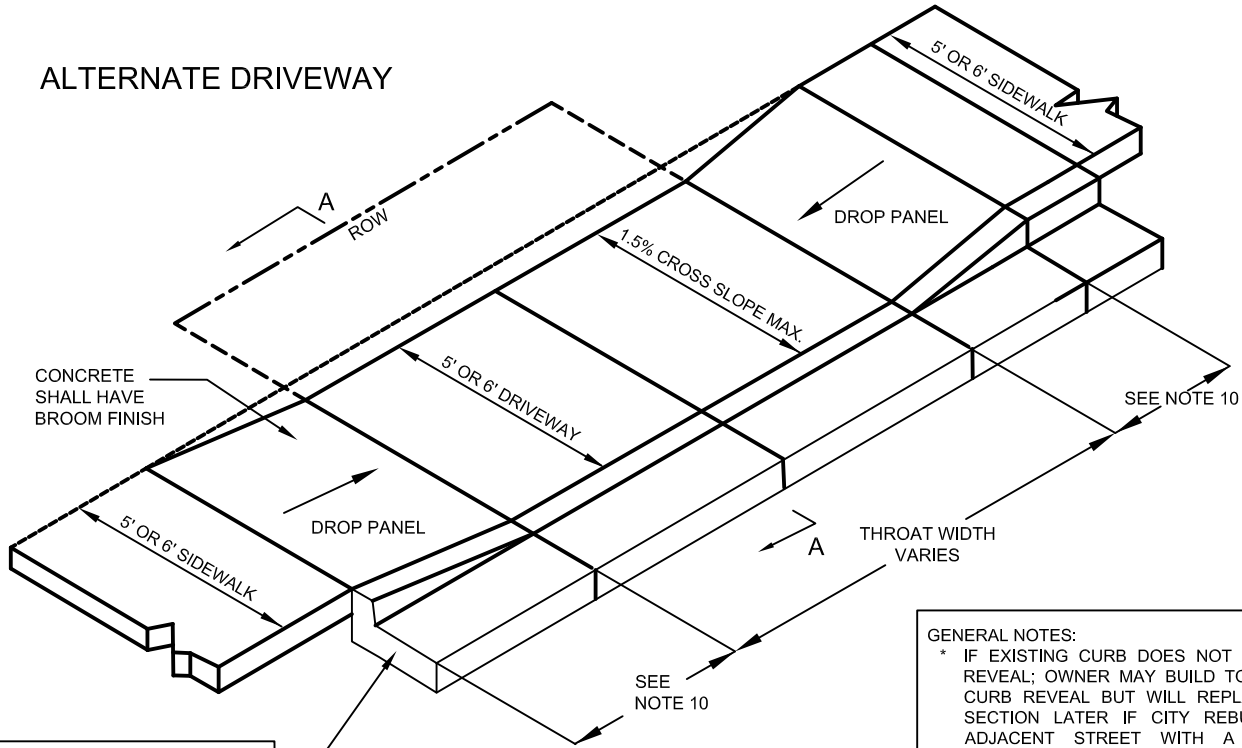
DETAIL NO.

T-150

2/12/2024



# ALTERNATE DRIVEWAY

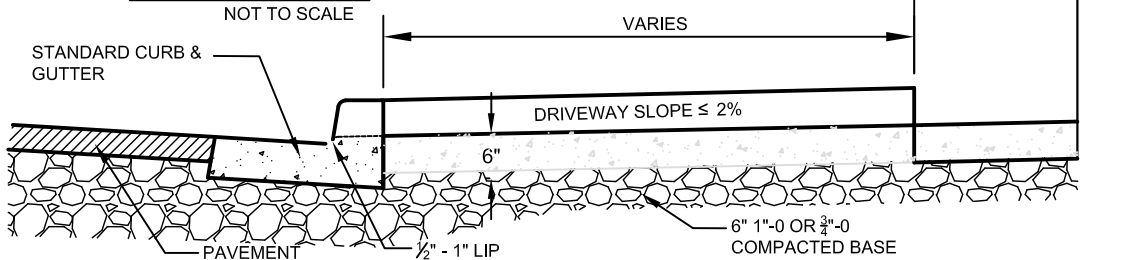


REFER TO NMC 14.46.030 FOR APPROACH AND DRIVEWAY DEVELOPMENT. APPROACHES AND DRIVEWAYS SHALL BE A MINIMUM OF TWELVE (12) FEET FOR A ONE-WAY DRIVEWAY AND NO GREATER THAN EIGHTEEN (18) FEET MAXIMUM WIDTH.

**GENERAL NOTES:**  
 \* IF EXISTING CURB DOES NOT HAVE A 6" REVEAL; OWNER MAY BUILD TO EXISTING CURB REVEAL BUT WILL REPLACE RAMP SECTION LATER IF CITY REBUILDS THE ADJACENT STREET WITH A 6" CURB REVEAL.  
 \*\* REGARDLESS OF CURB REVEAL; DEPTH OF CURB REMAINS THE SAME AS THOUGH THERE WERE A 6" REVEAL. SEE CURB STANDARD DETAILS.

CURB & GUTTER OR TYPE "C" CURB; MATCH ADJACENT

## SECTION A-A



### NOTES

1. A MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. DRIVEWAY STRUCTURE SHALL CONFORM TO OREGON STANDARD SPECIFICATION FOR CONSTRUCTION (CURRENT EDITION) SECTION 00759.
3. ADA COMPLIANT CONCRETE APRON REQUIRED FOR ALL DRIVEWAYS EVEN WHERE NO SIDEWALK IS CONSTRUCTED ADJACENT TO DRIVEWAY.
4. CURB SHALL BE FORMED & POURED SEPARATELY FROM DRIVEWAY; MONOLITHIC POURS NOT ALLOWED.
5. A RIGHT-OF-WAY PERMIT IS REQUIRED ON NEW DRIVEWAYS AND DRIVEWAY MODIFICATIONS. SCHEDULE ONSITE MEETING WITH CITY REPRESENTATIVE AFTER FORMS ARE SET BUT BEFORE POURING
6. FOR SIDEWALKS REFER TO STANDARD SIDEWALK DETAIL T-210.
7. FOR CURBS REFER TO STANDARD CURB & GUTTER DETAIL T-301.
8. DROP PANEL DRIVEWAYS MAY BE USED WHEN GROUND AT BACK OF WALK SLOPES DOWN TO A HOUSE OR DRIVEWAY LOWER THAN THE SIDEWALK.
9. FOR COMMERCIAL DRIVEWAYS SEE T-152 FOR REINFORCEMENT REQUIREMENTS.
10. WING WIDTH VARIES IN CONFORMANCE TO ADA REQUIREMENTS AND EXISTING GEOGRAPHY.
11. CONCRETE TO EXTEND STRAIGHT BACK FROM DRIVEWAY THROUGH ROW TO PROTECT WALKWAY.

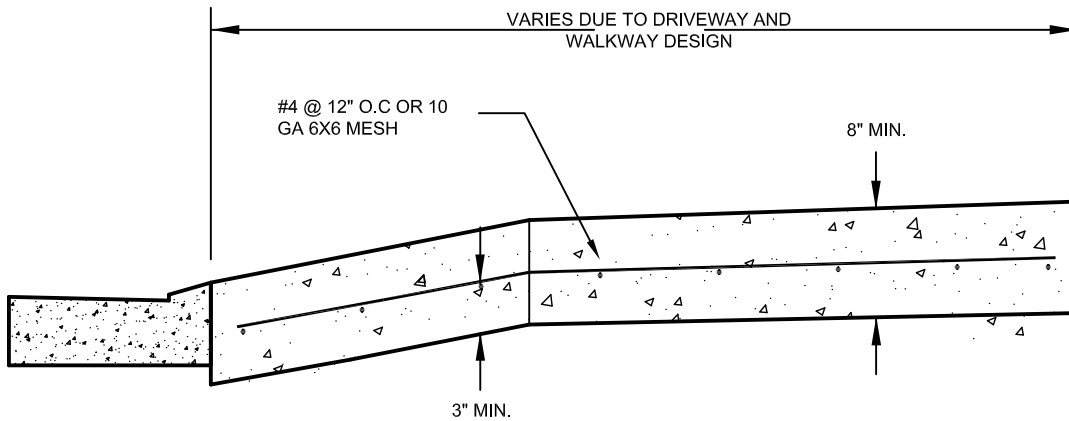


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# DRIVEWAY DROP PANEL APPROACH FOR 5' SIDEWALK

DETAIL NO.  
**T-151**  
 2/12/2024

REFER TO NMC 14.46.030 FOR APPROACH AND DRIVEWAY DEVELOPMENT. APPROACHES AND DRIVEWAYS SHALL BE A MINIMUM OF TWELVE (12) FEET FOR A ONE-WAY DRIVEWAY OR TWENTY (20) FEET FOR A TWO-WAY DRIVEWAY AND NO GREATER THAN 150% OF MINIMUM WIDTH.



**NOTES:**

1. A MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. SCORE MARKS ARE TO BE TOOLED INTO CONCRETE SURFACE TO FORM A SQUARE BUT NOT TO EXCEED 8' IN ANY DIRECTION.
3. REINFORCING BAR IS TO BE HELD UP WITH NON-CORROSIVE MATERIALS (ROCK, CONCRETE) PRIOR TO CONCRETE BEING PLACED.
4. REBAR SHALL BE BENT WITH GRADE BREAK TO MATCH SLOPE CHANGE WHERE WALKWAY AND RAMP MEET.
5. REBAR FRAME CAN BE USED WITH EITHER T-150 OR T-151 APPROACH LAYOUTS.



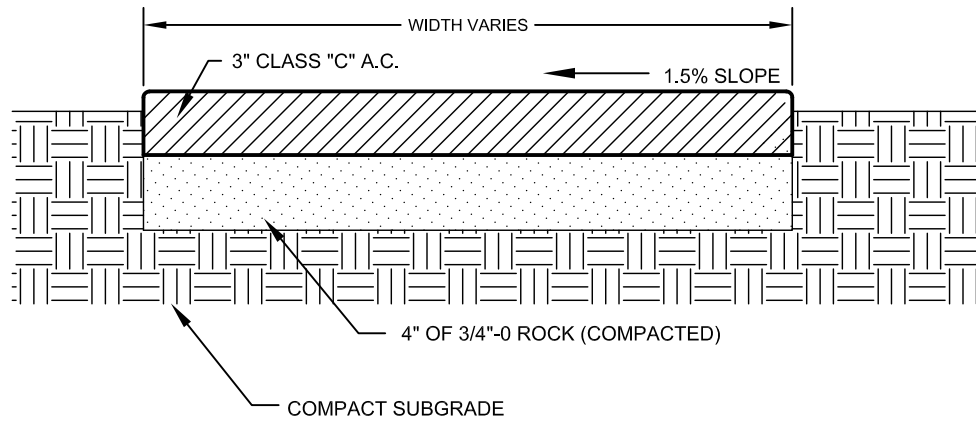
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**COMMERCIAL ONE-WAY  
 AND TWO-WAY DRIVEWAY  
 REINFORCEMENT DETAIL**

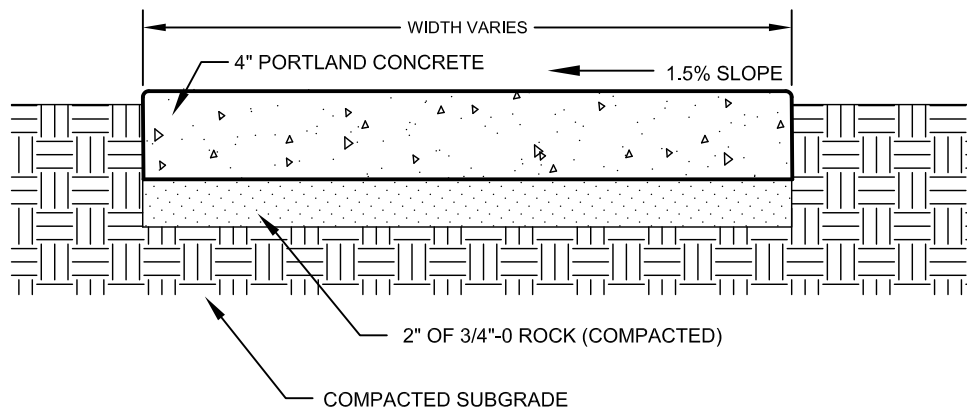
DETAIL NO.

T-152

11/14/2023



-OR-



**NOTES:**

1. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI
2. SIDEWALK PANELS SHALL BE SQUARE, 3/4" DEEP SCRIBES AT JOINTS, EDGED ON 4 SIDES AND HAVE A LIGHT BROOM FINISH.
3. PEDESTRIAN PATH OR BIKEWAY SHALL HAVE A MINIMUM WIDTH OF 5 FEET (ONE WAY) A MINIMUM WIDTH OF 10 FEET (TWO WAY)



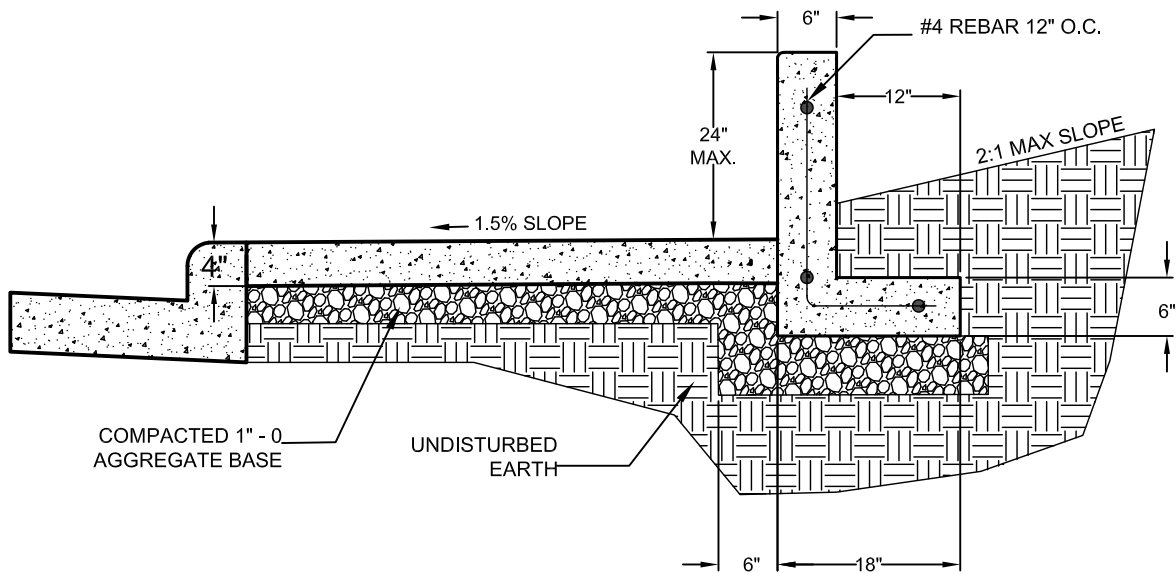
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**MULTI-USE PATH  
 DETAILS**

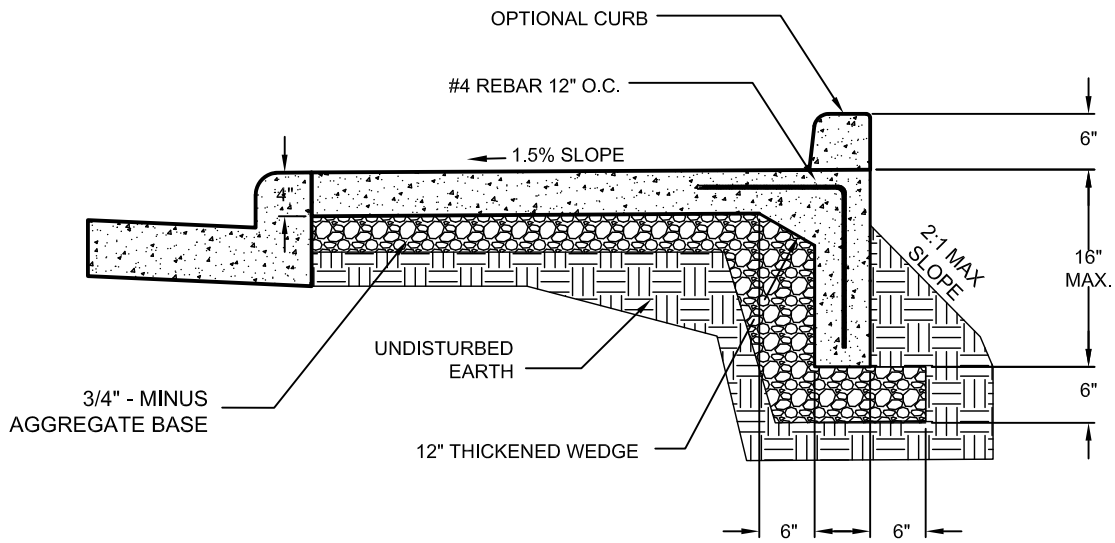
DETAIL NO.

T-201

2/12/2024



PONY WALL



WEDGE WALL

NOTES

1. A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. STRUCTURES SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
3. REINFORCING BAR IS TO BE HELD UP WITH NON-CORROSIVE MATERIALS (ROCK, CONCRETE) PRIOR TO CONCRETE BEING PLACED.
4. REBAR SHALL HAVE A MINIMUM 2" CONCRETE COVER FROM OUTSIDE

CONCRETE EDGE.

5. CONCRETE SHALL RECEIVE A MEDIUM BROOM FINISH. BROOMING SHALL BE PERPENDICULAR TO CURB LINE.
6. CURB JOINT SHALL BE TROWELED JOINT WITH MIN. 1/2" RADIUS ALONG BACK OF CURB.
7. FOR SIDEWALKS REFER TO STANDARD SIDEWALK DETAIL T-210.
8. FOR CURB REFER TO STANDARD CURB & GUTTER DETAIL T-301.
9. PONY WALLS TALLER THAN 24 INCHES WILL NEED TO BE DESIGNED BY A REGISTERED PROFESSIONAL ENGINEER..



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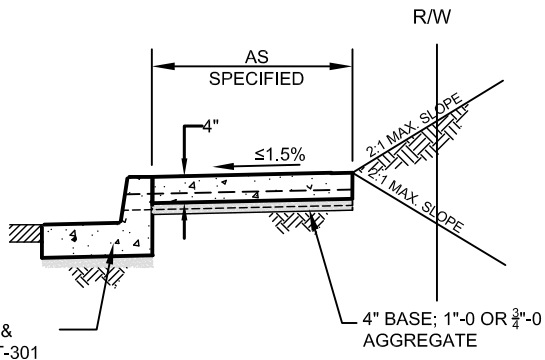
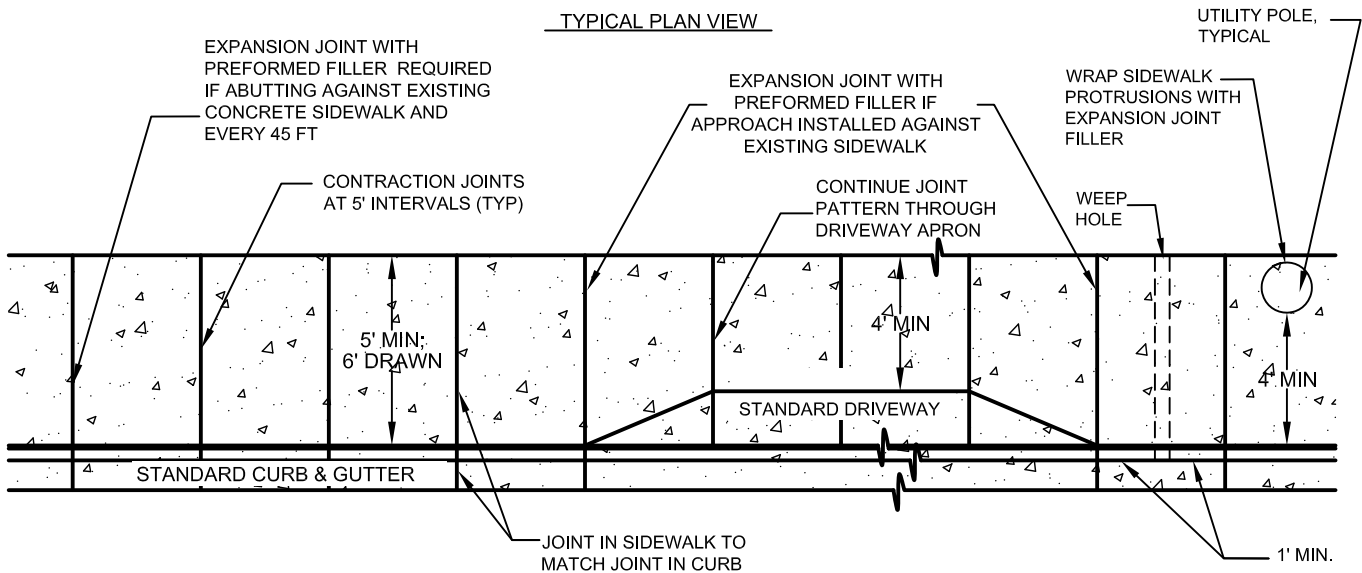
PONY WALLS &  
 WEDGE WALLS

DETAIL NO.

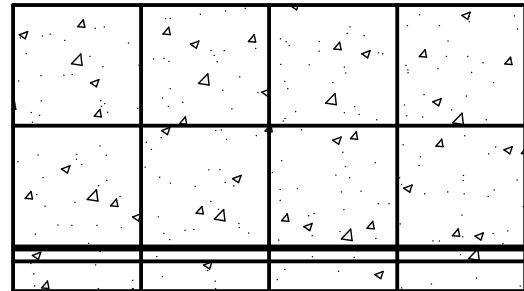
T-202

2/12/2024

TYPICAL PLAN VIEW



STANDARD CURB & GUTTER, DETAIL T-301



SIDEWALKS 8' AND WIDER SHALL HAVE A LONGITUDINAL CONTRACTION JOINT AT THE MIDPOINT

NOTES:

1. A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. STRUCTURES SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
3. STANDARD SIDEWALK CROSS SLOPE SHALL BE 1.5% TOWARD THE STREET. WHEN THE ADJACENT PROPERTY IS LOWER THAN THE TOP OF THE CURB, SIDEWALK MAY SLOPE AWAY FROM STREET.
4. DRAIN BLOCKOUTS IN THE CURB SHALL BE EXTENDED TO THE BACK OF THE SIDEWALK WITH A 3" DIA. PLASTIC PIPE AT A 1.5% SLOPE. A CONTRACTION JOINT SHALL BE PLACED OVER THE PIPE. NO COUPLINGS UNDER SIDEWALK. OUTFALL AT FACE OF CURB EQUALS GUTTER PAN ELEVATION.
5. SEE STANDARD WHEELCHAIR/BICYCLE RAMP DETAILS ODOT STANDARD DRAWING RD744, CITY STANDARD DRAWING T-212.
6. SEE STANDARD DETAILS FOR DRIVEWAYS: T-150, T-151, AND T-152.
7. WRAP PROTRUDING STRUCTURES IN SIDEWALKS WITH PREFORMED EXPANSION JOINT FILLER.



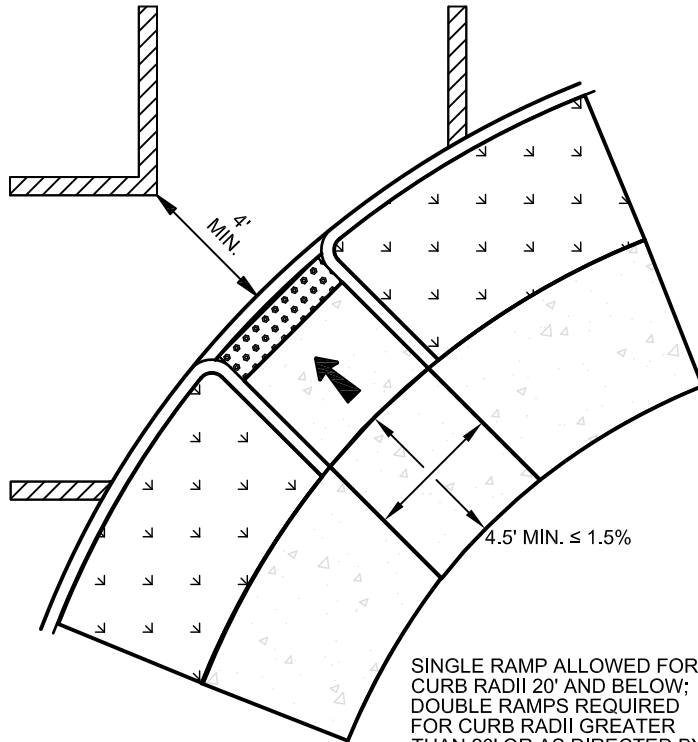
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STANDARD SIDEWALK  
 DETAILS

DETAIL NO.

T-210

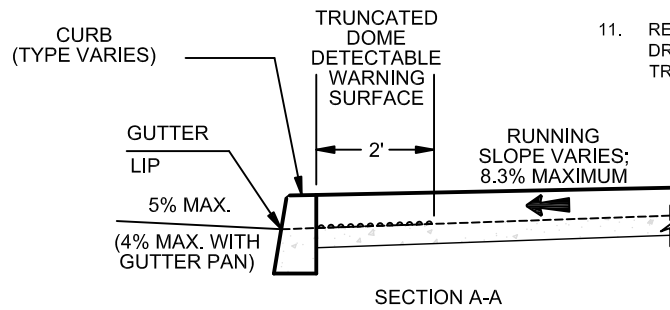
2/12/2024



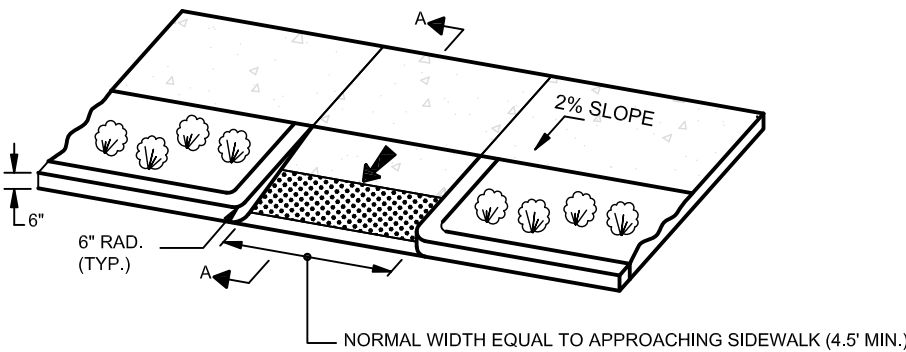
SINGLE RAMP ALLOWED FOR CURB RADII 20' AND BELOW; DOUBLE RAMPS REQUIRED FOR CURB RADII GREATER THAN 20' OR AS DIRECTED BY CITY ENGINEER.

GENERAL NOTES FOR ALL DETAILS:

1. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI
2. SIDEWALK RAMP DETAILS ARE BASED ON UNITED STATES ACCESS BOARD STANDARDS.
3. ALL SIDEWALK AND RAMP GRADES SHALL MEET ADA STANDARDS.
4. TOOLED JOINTS ARE REQUIRED AT ALL SIDEWALK RAMP SLOPE BREAK LINES.
5. SIDEWALK CURB RAMP SLOPES SHOWN ARE RELATIVE TO THE TRUE LEVEL HORIZON (ZERO BUBBLE).
6. PLACE TRUNCATED DOME DETECTABLE WARNING SURFACE IN THROAT OF RAMP CLOSEST TO STREET. SEE STANDARD DRAWING T-213.
7. CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN RAMP PATHWAY.
8. CROSSWALK LINES WHERE REQUIRED MUST BE AS PER STRIPING PLAN APPROVED BY THE CITY ENGINEER.
9. SEE STANDARD SIDEWALK DETAIL T-210 FOR ADDITIONAL INFORMATION.
10. CONFORM WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, SECTION 3B.18 FOR PEDESTRIAN CROSSWALK MARKINGS.
11. REFER TO ODOT STANDARD DRAWING RD 758 & RD 759 FOR TRUNCATED DOME PLACEMENT.



SECTION A-A



PERPENDICULAR SIDEWALK RAMP DETAIL (THROUGH BUFFER STRIP)



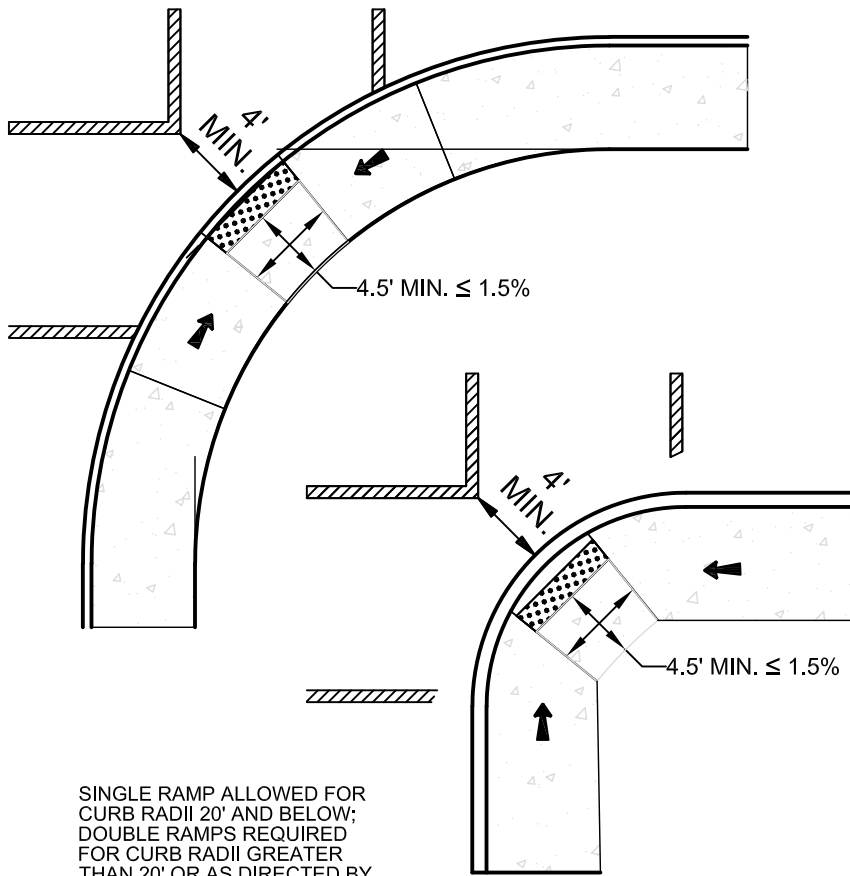
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SIDEWALK AND RAMP DETAIL  
WITH PLANTER STRIP

DETAIL NO.

T-211

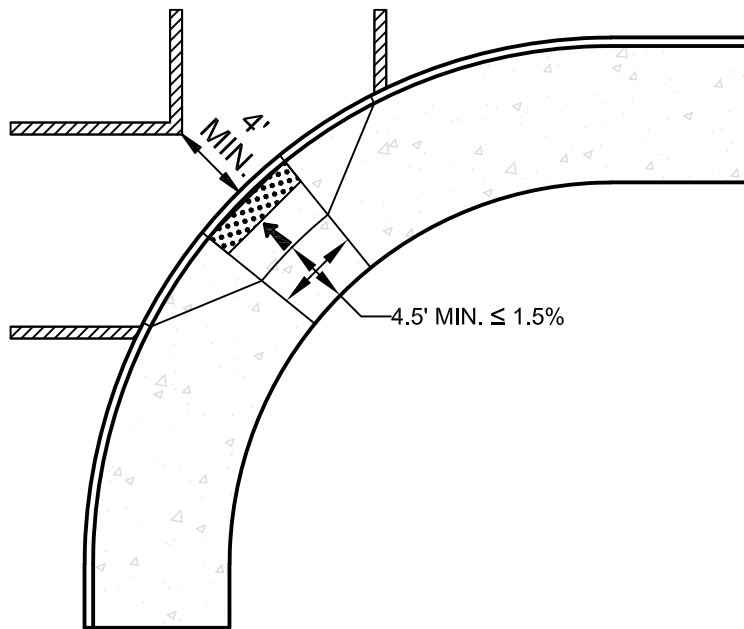
5/12/2020



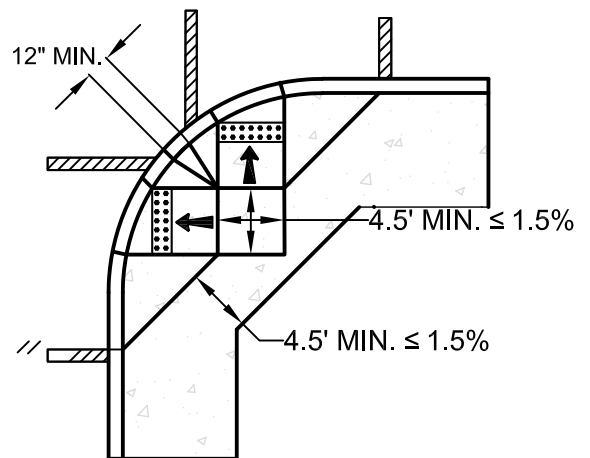
SINGLE RAMP ALLOWED FOR CURB RADII 20' AND BELOW; DOUBLE RAMPS REQUIRED FOR CURB RADII GREATER THAN 20' OR AS DIRECTED BY CITY ENGINEER.

GENERAL NOTES FOR ALL DETAILS:

1. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. SIDEWALK RAMP DETAILS ARE BASED ON UNITED STATES ACCESS BOARD STANDARDS.
3. ALL SIDEWALK AND RAMP GRADES SHALL MEET ADA STANDARDS.
4. TOOLED JOINTS ARE REQUIRED AT ALL SIDEWALK RAMP SLOPE BREAK LINES.
5. SIDEWALK CURB RAMP SLOPES SHOWN ARE RELATIVE TO THE TRUE LEVEL HORIZON (ZERO BUBBLE).
6. PLACE TRUNCATED DOME DETECTABLE WARNING SURFACE IN THROAT OF RAMP CLOSEST TO STREET. SEE STANDARD DRAWING T-213.
7. CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN RAMP PATHWAY.
8. CROSSWALK LINES WHERE REQUIRED MUST BE AS PER STRIPING PLAN APPROVED BY THE CITY ENGINEER.
9. SEE STANDARD SIDEWALK DETAIL T-210 FOR ADDITIONAL INFORMATION.
10. CONFORM WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, SECTION 3B.18 FOR PEDESTRIAN CROSSWALK MARKINGS.
11. REFER TO ODOT STANDARD DRAWING RD 758 & RD 759 FOR TRUNCATED DOME PLACEMENT.



COMBINATION RAMP (FOR WIDE SIDEWALKS; USE IN ALTERATIONS ONLY AND WHEN SITE CONSTRAINTS PROHIBIT INSTALLING TWO RAMPS)



PERPENDICULAR RAMP (FOR NARROW SIDEWALKS)



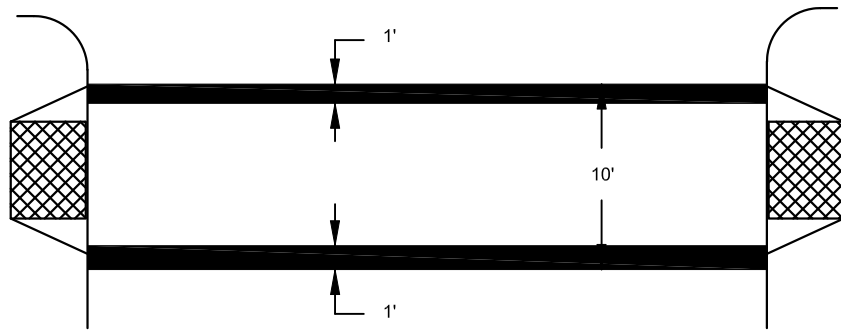
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SIDEWALK AND RAMP DETAIL  
WITHOUT PLANTER STRIP

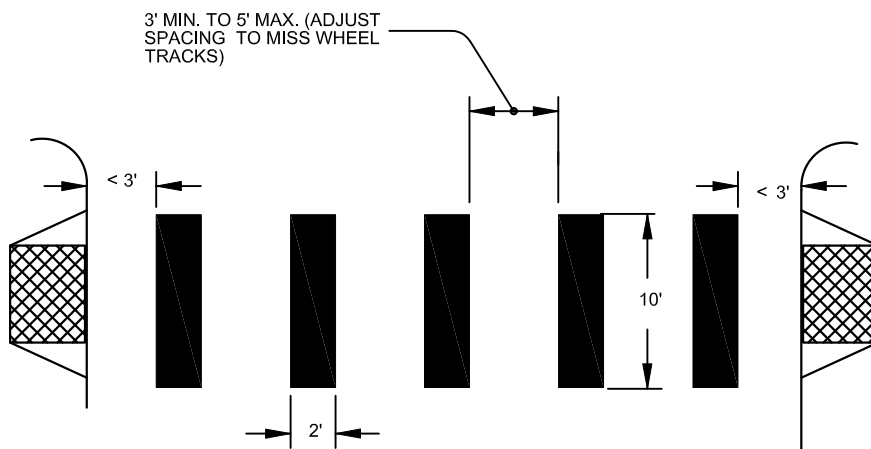
DETAIL NO.

T-212

2/12/2024



STOP CROSSINGS  
STANDARD CROSSWALK  
TWO 1' WHITE BARS  
USED AT AT STREET INTERSECTIONS



NON-STOP CROSSINGS  
STAGGERED CONTINENTAL CROSSWALK  
2' WHITE BARS  
USED AT MID-BLOCK CROSSINGS

NOTES:

1. CROSSWALKS SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL, CURRENT EDITION, SECTION 3B.18.
2. ALL PAVEMENT MARKING SHALL BE WHITE THERMOPLASTIC.
3. THERMOPLASTIC MATERIAL SHALL CONFORM TO THE OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00850.



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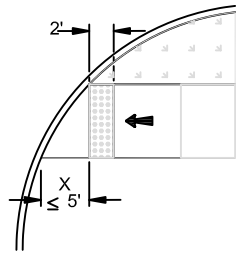
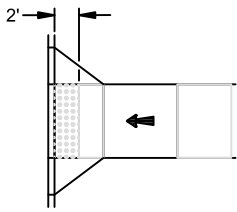
PEDESTRIAN CROSSING  
DETAIL

DETAIL NO.

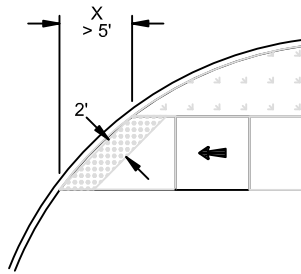
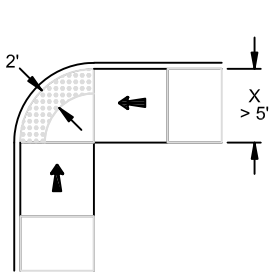
T-213

5/12/2020



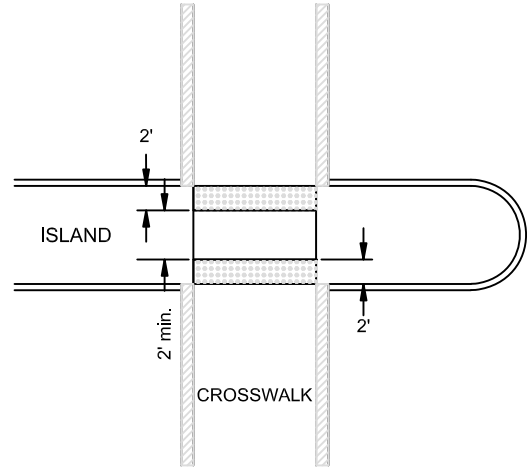


WHEN DISTANCE "X" IS LESS THAN 5', TRUNCATED DOME DETECTABLE WARNING SURFACE SHALL BE PLACED PERPENDICULAR TO THE PATH OF TRAVEL.



WHEN DISTANCE "X" IS GREATER THAN 5', TRUNCATED DOME DETECTABLE WARNING SURFACE SHALL BE PLACED PARALLEL TO THE BOTTOM OF CURB RAMP.

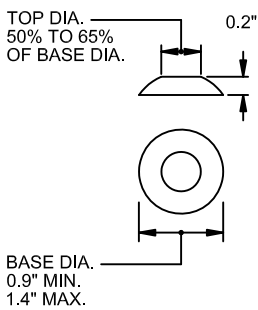
PLACEMENT ON SIDEWALK RAMP



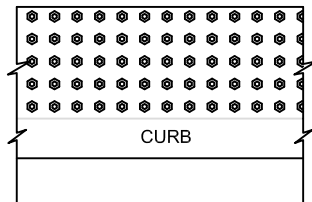
PLACEMENT ON CROSSING ISLAND

NOTES

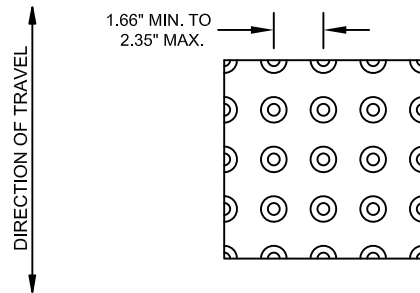
1. REFER TO ODOT STANDARD DRAWING RD759 FOR DETAILS.
2. COLOR OF DOMES TO BE SAFETY YELLOW.



TRUNCATED DOME DETAIL



TRUNCATED DOME PATTERN



RAMP TEXTURE DETAIL

TRUNCATED DOME DETECTABLE WARNING SURFACE



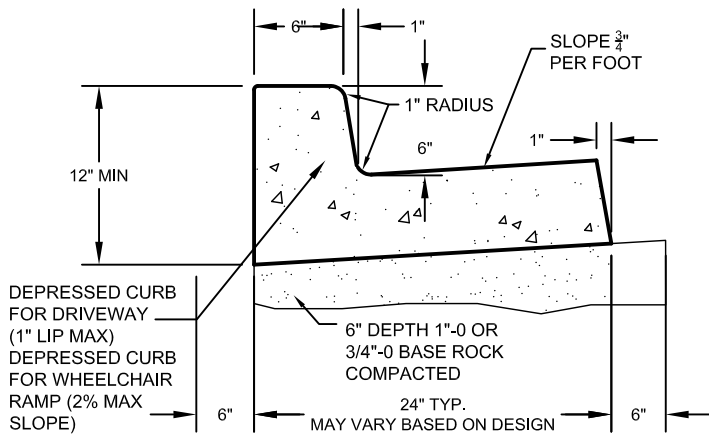
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TRUNCATED DOME  
PLACEMENT

DETAIL NO.

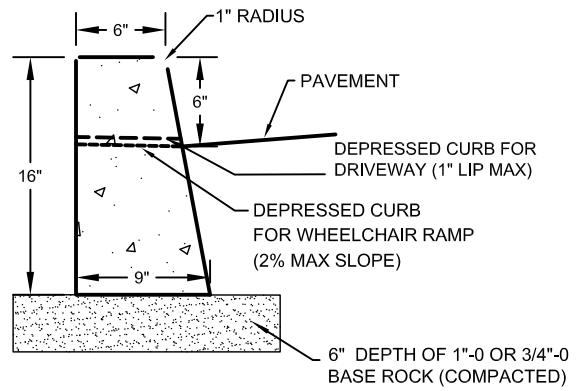
T-214

6/7/23



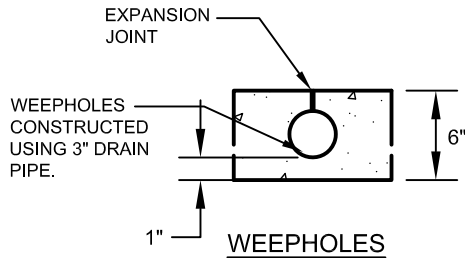
**TYPICAL INTEGRAL CURB & GUTTER**

NOT TO SCALE; FOR CROWN ROADS ONLY; SEE NOTE 8 FOR SHED ROADS.



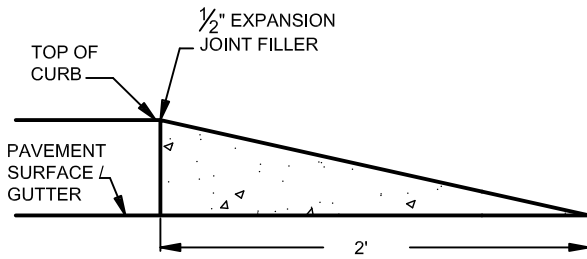
**TYPE C CURB**

NOT TO SCALE



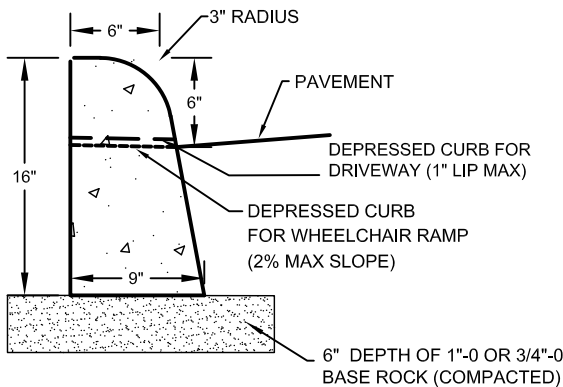
DRAIN PIPES SHALL BE EXTENDED TO BACK OF WALK WHEN SIDEWALKS ARE CONSTRUCTED ADJACENT TO CURB

**WEEPHOLES**



**CURB ENDING DETAIL**

NOT TO SCALE



**SURMOUNTABLE CURB**

**NOTES:**

1. A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR.
2. CURB STRUCTURES SHALL CONFORM TO THE OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
3. CONTRACTION JOINTS
  - A) TO BE PROVIDED
    - AT EACH POINT OF TANGENCY
    - AT EACH SIDE OF INLET STRUCTURES
    - AT BOTH SIDES OF AN APPROACH
  - B) SPACING TO BE NOT MORE THAN 15 FEET
  - C) DEPTH OF JOINT SHALL BE AT LEAST 1/3 THICKNESS OF CONCRETE
3. EXPANSION JOINTS
  - A) TO BE PROVIDED
    - AT EACH COLD JOINT
    - AT EACH JOINT BETWEEN NEW AND EXISTING CONCRETE
  - B) EXPANSION JOINTS IN CURB & GUTTER SHALL BE PLACED AT MAX 45' INTERVALS
  - C) DEPTH OF JOINT SHALL BE EQUAL TO THICKNESS OF CONCRETE
  - D) EXPANSION JOINTS SHALL USE PREFORMED EXPANSION JOINT FILLER
4. WEEPHOLES
  - A) DRAINAGE ACCESS THROUGH EXISTING CURBS SHALL BE DONE BY:
    - CORE DRILLING, OR
    - VERTICAL SAWCUT OF CURB 18" EACH SIDE OF DRAIN AND RE-POURED (WITH DOWELS BOTH SIDES INTO EXISTING CUT) TO FULL DEPTH OF CURB OR CURB & GUTTER
5. STAMP TOP OF CURB WITH "W" AT WATER SERVICE CROSSING AND "S" AT SANITARY LATERAL CROSSING AS SPECIFIED.
6. SEE STANDARD CURB CUT DETAIL FOR DRIVEWAYS: T-150, T-151, AND T-152.
7. TYPE C CURB MAY BE USED FOR REPLACEMENT OF EXISTING TYPE C CURBS ONLY UPON APPROVAL BY CITY ENGINEER.
8. STANDARD CURB AND GUTTER MUST BE ROTATED ON SHED ROADS TO DRAIN TOWARD STREET.



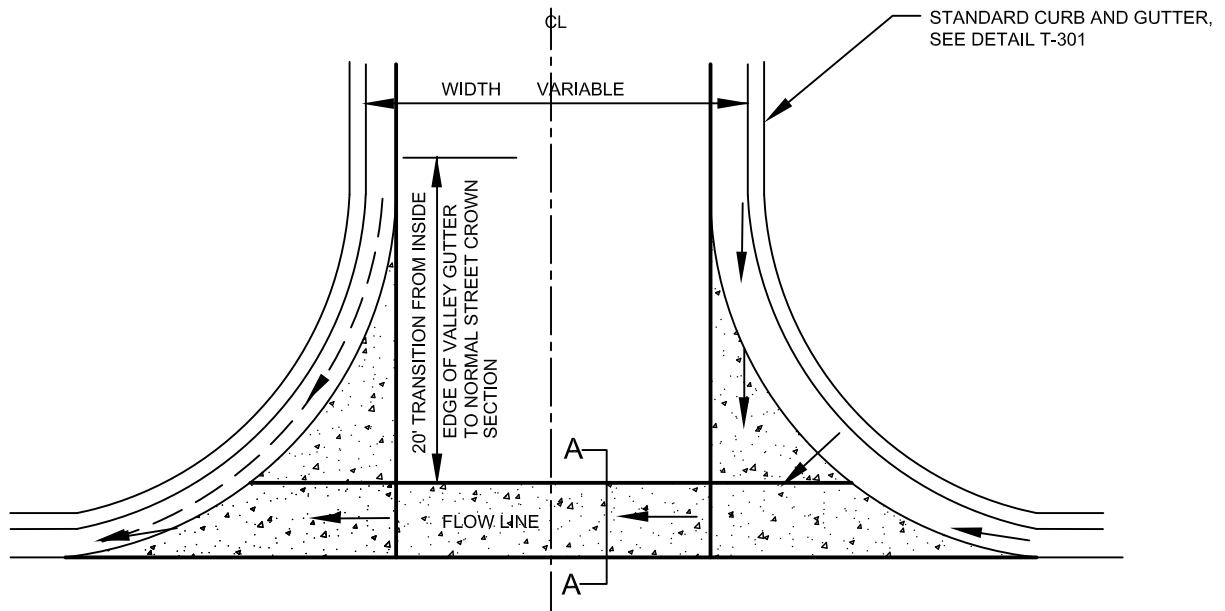
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**CURB AND GUTTER DETAILS**

DETAIL NO.

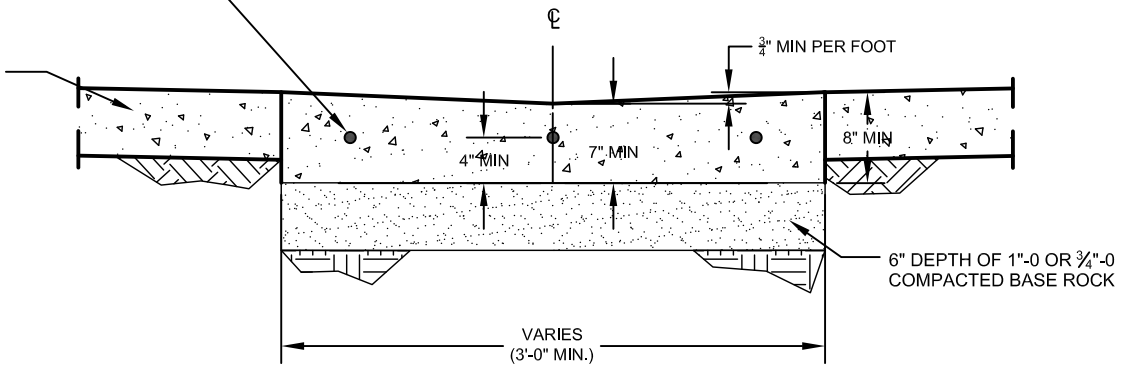
T-301

12/13/2023



3 - #4 REBAR CONTINUOUS,  
REBAR SHOULD BE CENTERED IN  
VALLEY GUTTER

SEE STANDARD  
STREET DRAWING  
T-050



SECTION A - A

**NOTES:**

1. A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR.
2. STRUCTURES SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.



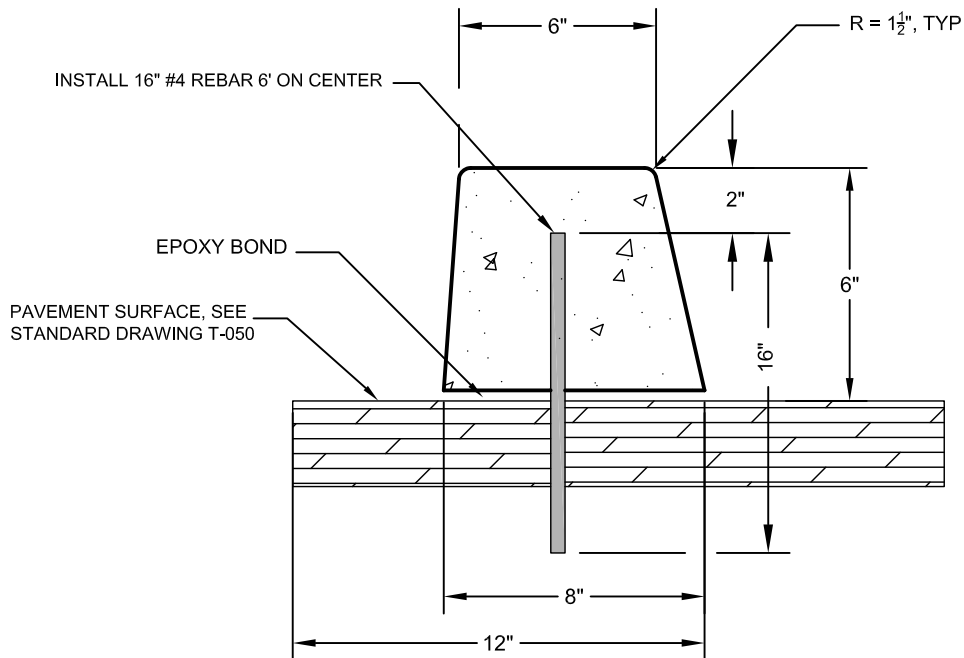
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**VALLEY GUTTER DETAIL**

DETAIL NO.

T-302

5/12/2020



**NOTES**

1. A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR.
2. STRUCTURES SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
3. DOWEL NEW CURB TO EXISTING ASPHALT WITH #4 BAR WITH 6" PENETRATION.
4. REBAR SHALL HAVE A MINIMUM 2" CONCRETE COVER FROM OUTSIDE CONCRETE EDGE.
5. CONCRETE SHALL RECEIVE A MEDIUM BROOM FINISH.
6. CURB JOINT SHALL BE TROWELED JOINT WITH MIN. 1/2" RADIUS ALONG BACK OF CURB.
7. FOR SIDEWALKS REFER TO STANDARD SIDEWALK DETAIL T-210.
8. FOR CURB REFER TO STANDARD CURB & GUTTER DETAIL T-301.



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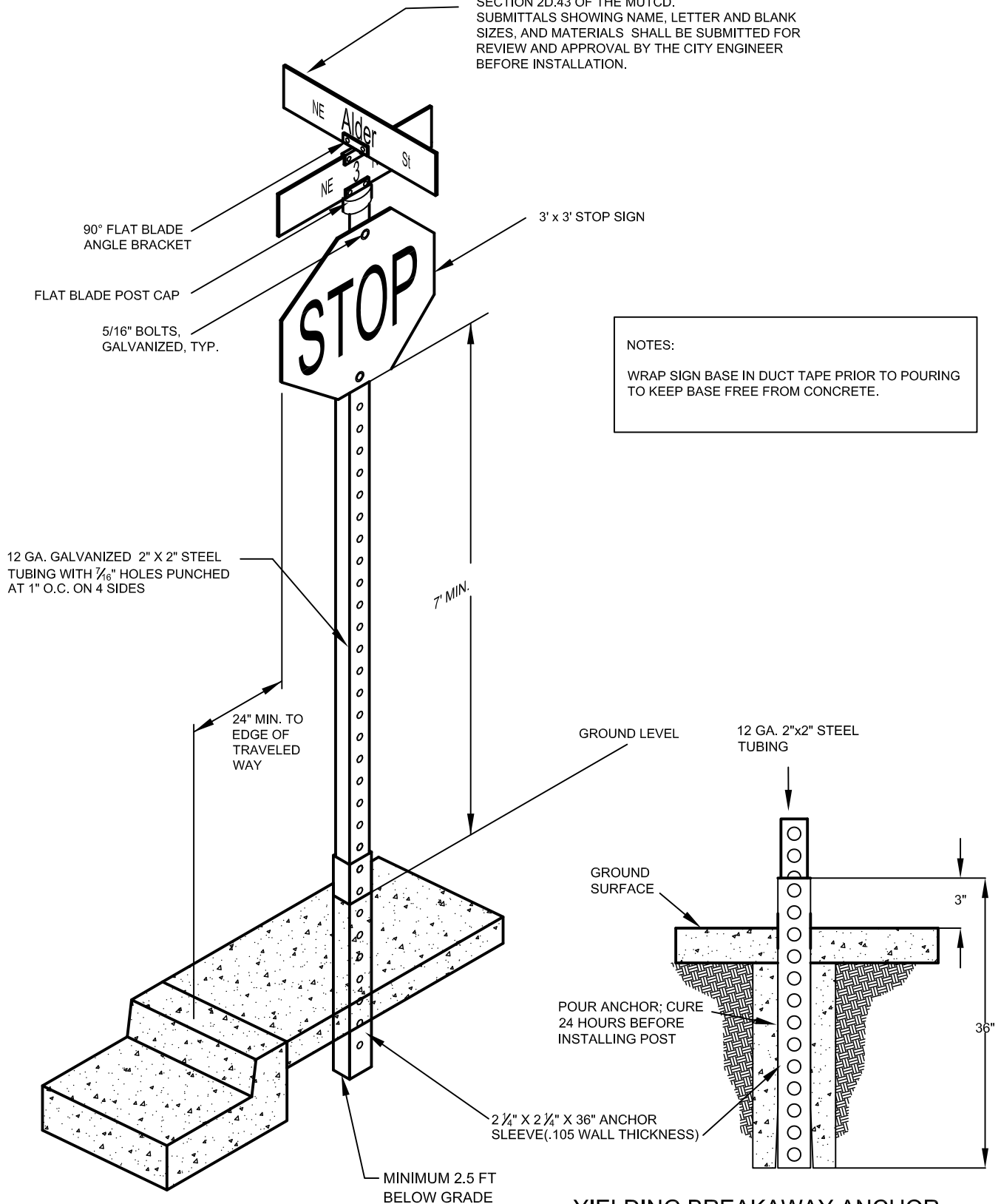
**EXTRUDED BONDED CURB  
 CONCRETE**

DETAIL NO.

**T-303**

12/13/2023

STREET SIGNS SHALL BE IN ACCORDANCE WITH SECTION 2D.43 OF THE MUTCD. SUBMITTALS SHOWING NAME, LETTER AND BLANK SIZES, AND MATERIALS SHALL BE SUBMITTED FOR REVIEW AND APPROVAL BY THE CITY ENGINEER BEFORE INSTALLATION.



NOTES:  
WRAP SIGN BASE IN DUCT TAPE PRIOR TO POURING TO KEEP BASE FREE FROM CONCRETE.

YIELDING BREAKAWAY ANCHOR BASE ASSEMBLY



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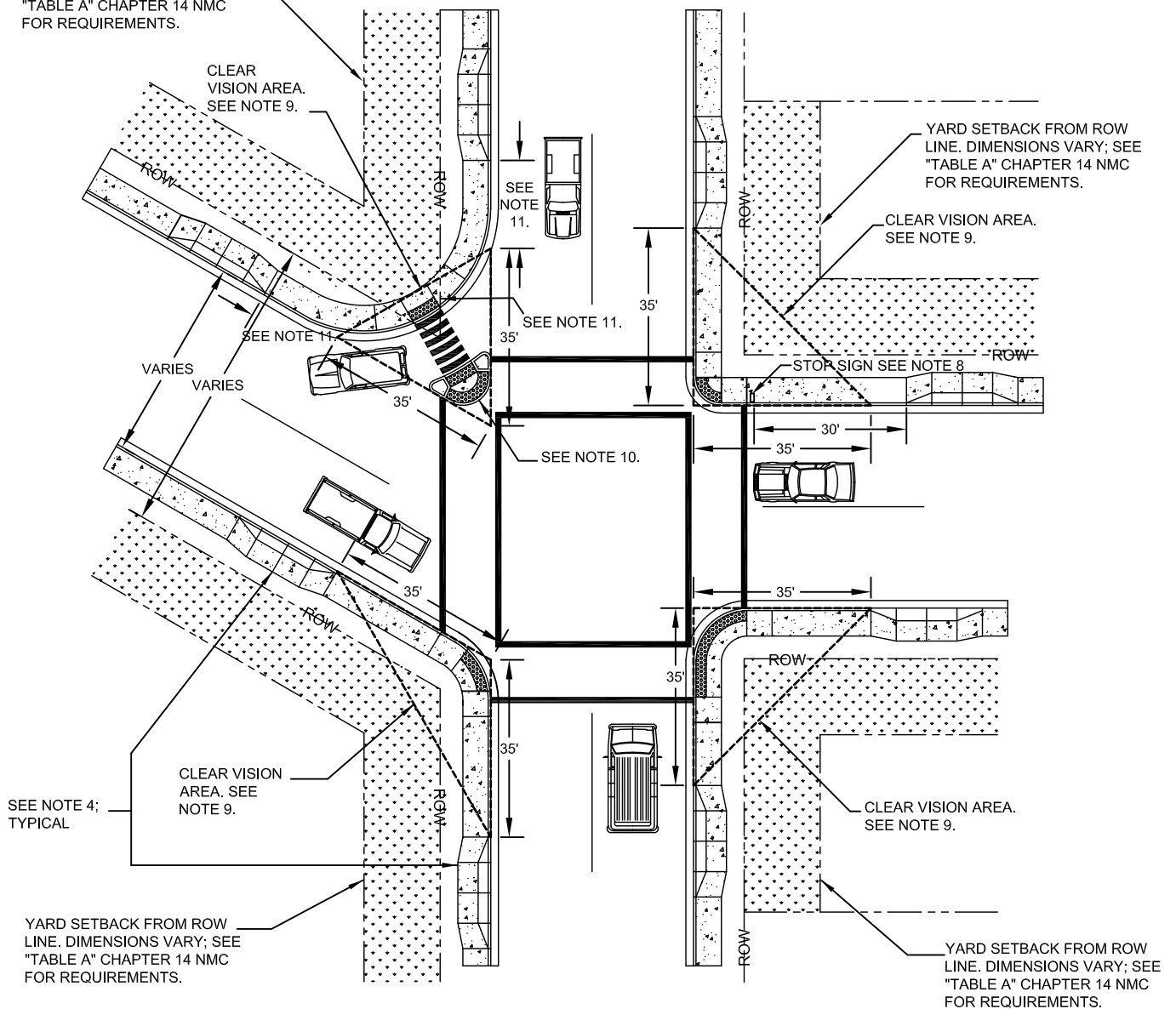
STANDARD SIGN DETAIL  
FOR LOCAL STREETS

DETAIL NO.

T-450

5/12/2020

YARD SETBACK FROM ROW LINE. DIMENSIONS VARY; SEE "TABLE A" CHAPTER 14 NMC FOR REQUIREMENTS.



**NOTES**

1. CLEAR VISION AREA BASED ON NEWPORT MUNICIPAL CODE (NMC 14.17) REQUIREMENTS. REFER TO NMC FOR FURTHER INFORMATION.
2. DRIVEWAYS SHALL BE LOCATED OUTSIDE CLEAR VISION AREA .
3. YARD SETBACKS ARE BASED ON NMC "TABLE A" CHAPTER 14; SEE NMC FOR FURTHER INFORMATION.
4. MULTIPLE DRIVEWAYS SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. CITY ENGINEER HAS FINAL APPROVAL OF DRIVEWAY LOCATION.
5. ACUTE ANGLE CORNERS SHALL BE DESIGNED FOR APPROPRIATE TURNING RADIUS.
6. PEDESTRIAN CROSSING SHALL NEED ADAPTATION DEPENDING ON TURNING RADIUS.
7. TURNING LANE SHALL BE 12 FT WIDE FROM FACE OF CURB TO FACE OF CURB.
8. DRIVEWAYS SHALL BE LOCATED NO CLOSER THAN 30 FT FROM STOP SIGNS. CITY ENGINEER HAS FINAL APPROVAL ON DRIVEWAY LOCATION.
9. CLEAR VISION AREA SHALL REMAIN CLEAR OF ALL NATURAL OR MAN MADE STRUCTURES 3 FT OR HIGHER BLOCKING LINE OF SIGHT EXCEPT FOR THOSE ALLOWED BY NMC 14.17.
10. UNUSUAL CURB RADIUS MAY REQUIRE PEDESTRIAN SAFETY ISLAND.
11. DISTANCE VARIES. CURB RADIUS MAY REQUIRE DRIVEWAY SET BACK FURTHER FROM 35 FT LINE OF CLEAR VISION AREA . CITY ENGINEER HAS FINAL APPROVAL ON DRIVEWAY LOCATION.



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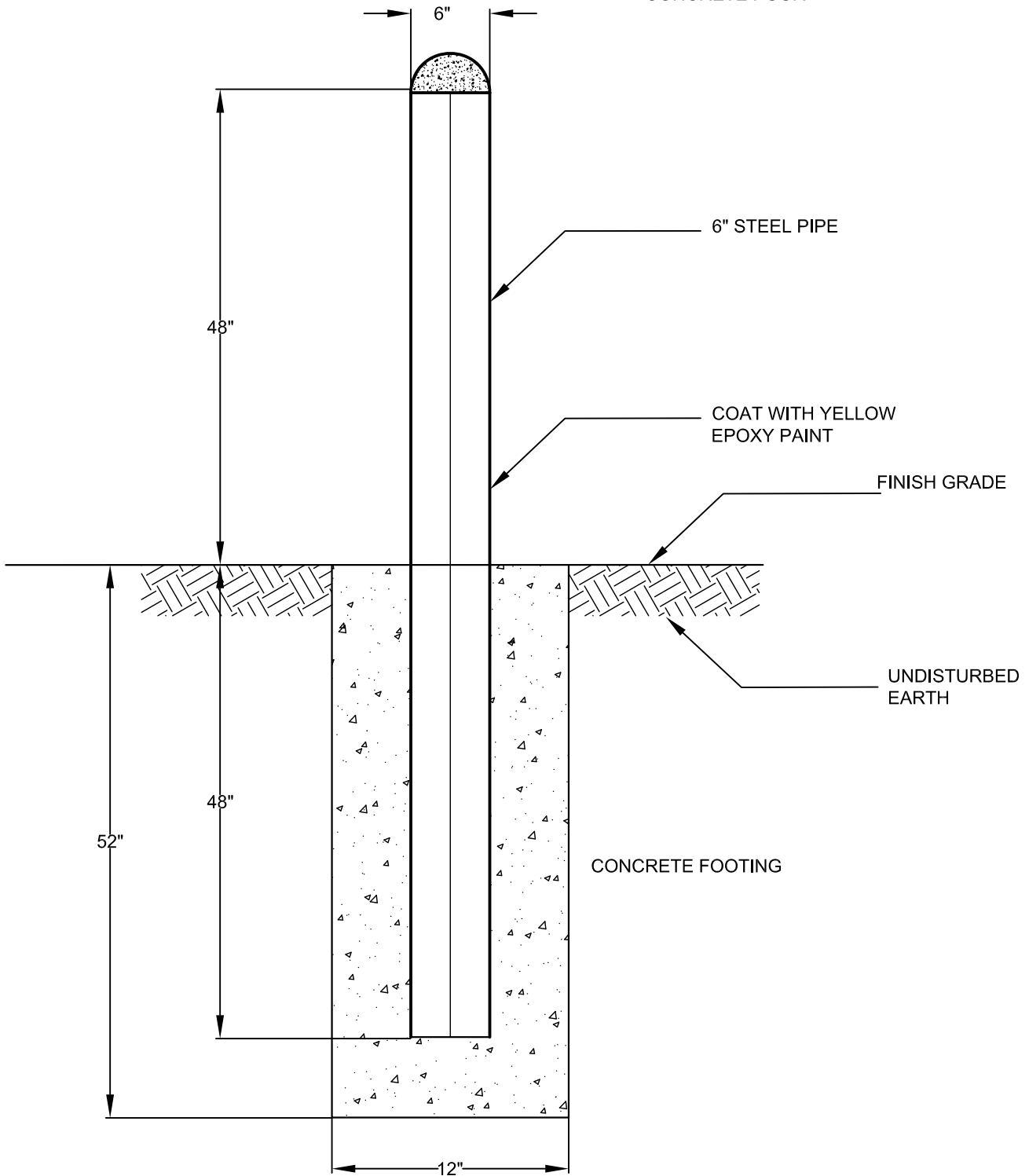
**CLEAR VISION AREA AT INTERSECTION**

DETAIL NO.

T-600

5/12/2020

FILL PIPE WITH CONCRETE; CONE TOP OF CONCRETE POUR



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## BOLLARD DETAILS

DETAIL NO.

T-604

11/22/2022